

Harvest Landing Specific Plan Amendment No. 3



City of Perris, California

March 2026



ACKNOWLEDGEMENTS



Harvest Landing Specific Plan Amendment No. 3

City of Perris

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This document reflects all amendments to January 2026

Amendment No.	Case No.	Details of Amendment	Approval Date
1	12-04-0010	The purpose of Amendment No. 1 is to modify Table 12.0-1, Land Use Restrictions to clarify allowable industrial land uses particularly related to storage in Airport Potential Zone 1 (APZ-1).	9/25/2012 Ordinance #1288
2	11-12-0005	The purpose of Amendment No. 2 is to update all graphics to reflect the street vacation of Nance and Markham Streets between Redlands Avenue and the Perris Valley Storm Channel. This amendment also reflects the street vacation and general plan amendment (GPA 12-02-0001) to the circulation element for the removal of Harley Knox Blvd. from Redland Avenue to Perris Valley Storm Channel.	11/27/2012 Resolution #4538
3	22-05250	The purpose of Amendment No. 3 is to update the land use designations to reflect a unified land use design and viable mixture of uses. This Amendment updates the former mix of Residential, Multiple Business Use (MBU), Commercial, Recreation, Park, and Drainage/Detention to a revised arrangement of MBU, Commercial, and Water Quality Management Plan (WQMP) Drainage/Detention, as well as adds an MBU Overlay in the northwest portion of the Specific Plan area. Additionally, the boundaries of the Specific Plan area are adjusted to remove a portion from the southern part of the Specific Plan area and add a portion to the northern part of the Specific Plan area.	2/10/2026 Resolution TBD

CONTENTS



SECTION	PAGE NUMBER
1.0 EXECUTIVE SUMMARY	1.0-1
1.1 Introduction	1.0-1
1.2 Specific Plan Vision and Purpose	1.0-1
1.3 Specific Plan Objectives	1.0-2
1.4 Specific Plan Format	1.0-4
1.5 Action Expectations	1.0-5
1.6 Existing Setting	1.0-5
1.6.1 Physical/Geographical	1.0-5
1.6.2 General Plan Land Use	1.0-10
1.6.3 Zoning	1.0-10
1.6.4 Opportunities and Constraints	1.0-10
1.6.5 Required Governmental Actions	1.0-11
1.7 Consistency Assessment	1.0-13
1.7.1 General Plan	1.0-13
1.7.2 Airport Influence Areas	1.0-24
1.8 Specific Plan EIR	1.0-25
2.0 LAND USE PLAN	2.0-1
2.1 Introduction	2.0-1
2.2 Low Density Residential (LDR)	2.0-2
2.3 Medium Density Residential (MDR)	2.0-2
2.4 High Density Residential (HDR)	2.0-2
2.5 Harvest Landing Sports Park (HLSP)	2.0-2
2.6 Multiple Business Uses (MBU)	2.0-4
2.7 Commercial/Retail Uses	2.0-4
2.8 Water Quality Management Plan (WQMP) Uses	2.0-4
2.9 Permitted Uses	2.0-5
2.10 Definitions	2.0-9
3.0 CIRCULATION AND INFRASTRUCTURE PLAN	3.0-1
3.1 Introduction	3.0-1
3.2 Circulation	3.0-1
3.2.1 Vehicular Roadway Network	3.0-3
3.2.2 Truck Routes	3.0-13
3.2.3 Public Transit	3.0-17
3.2.4 Bicycle Circulation	3.0-17
3.2.5 Pedestrian Circulation	3.0-18
3.3 Utilities	3.0-20
3.3.1 Water System	3.0-20
3.3.2 Recycled Water	3.0-22
3.3.3 Sewer	3.0-22

CONTENTS



SECTION	PAGE NUMBER
3.3.4	Stormwater Drainage 3.0-23
3.3.5	Dry Utilities Standards 3.0-35
3.4	WQMP Basin 3.0-36
3.4.1	WQMP/Drainage Detention 3.0-36
3.4.2	WQMP/Drainage Detention Site Design 3.0-36
4.0	DEVELOPMENT STANDARDS 4.0-1
4.1	Introduction 4.0-1
4.2	Community-Wide Development Standards 4.0-1
4.2.1	Residential Density 4.0-5
4.2.2	Parking 4.0-6
4.2.3	General Compliance 4.0-8
4.2.4	Crime Prevention Measures 4.0-9
4.2.5	Trash and Recyclable Materials 4.0-9
4.2.6	Screening 4.0-10
4.2.7	Lighting 4.0-11
4.2.8	Landscaping 4.0-11
4.2.9	Hazardous Materials 4.0-12
5.0	DESIGN GUIDELINES 5.0-1
5.1	Introduction 5.0-1
5.2	Community-Wide Design Guidelines 5.0-1
5.2.1	Architecture 5.0-3
5.2.2	Landscaping 5.0-10
5.2.3	Outdoor Furnishings 5.0-16
5.2.4	Walls and Fencing 5.0-18
5.2.5	Lighting 5.0-20
5.2.6	Color Palette 5.0-25
5.2.7	Access/Parking 5.0-26
5.2.8	Public Art 5.0-29
5.3	Multiple Business Use (MBU) 5.0-29
5.3.1	Architecture 5.0-29
5.3.2	Landscaping 5.0-31
5.3.3	Outdoor Furnishings 5.0-33
5.3.4	Walls and Fencing 5.0-34
5.3.5	Lighting 5.0-35
5.3.6	Color Palette 5.0-35
5.3.7	Access/Parking 5.0-36
5.3.8	Outdoor Storage 5.0-37
5.4	Commercial/Retail 5.0-38
5.4.1	Architecture 5.0-38
5.4.2	Landscaping 5.0-43

CONTENTS



SECTION	PAGE NUMBER
5.4.3	Outdoor Furnishings 5.0-44
5.4.4	Walls and Fencing 5.0-45
5.4.5	Lighting..... 5.0-45
5.4.6	Color Palette 5.0-46
5.4.7	Access/Parking 5.0-46
5.5	Residential 5.0-47
5.5.1	Architecture 5.0-48
5.5.2	Architectural Styles 5.0-50
5.5.3	Residential Home Types 5.0-55
5.5.4	Landscaping 5.0-63
5.5.5	Walls and Fencing 5.0-63
5.5.6	Lighting..... 5.0-63
5.5.7	Low Income and Senior Housing 5.0-63
5.5.8	Access/Parking 5.0-64
6.0	SIGNAGE 6.0-1
6.1	Introduction 6.0-1
6.2	Location 6.0-1
6.3	Design..... 6.0-1
6.3	Prohibited Signs 6.0-6
7.0	IMPLEMENTATION AND ADMINISTRATION 7.0-1
7.1	Specific Plan Implementation 7.0-1
7.1.1	Development Phasing..... 7.0-1
7.1.2	Specific Plan Financing..... 7.0-3
7.1.3	Project Maintenance 7.0-3
7.2	Specific Plan Entitlement Process 7.0-5
7.2.1	Agency Responsibilities 7.0-5
7.2.2	Permit Types and Processes..... 7.0-6
7.3	Specific Plan Amendment Process 7.0-10
7.3.1	Specific Plan Amendments 7.0-10
7.3.2	Substantial Conformance Determinations 7.0-10

FIGURE NUMBER AND TITLE	PAGE NUMBER
Figure 1.0-1 Regional Location	1.0-7
Figure 1.0-2 Local Vicinity	1.0-8
Figure 1.0-3 Project Site with Annexation Area	1.0-9
Figure 2.0-1 Land Use Plan	2.0-3
Figure 3.0-1 Circulation Plan	3.0-2
Figure 3.0-2a Standard Street Sections – Primary Arterial (Perris Boulevard)	3.0-3
Figure 3.0-2b Standard Street Sections – Secondary Arterial (Orange Avenue)	3.0-4
Figure 3.0-2c Standard Street Sections – Secondary Arterial (Frontage Road)	3.0-4
Figure 3.0-2d Standard Street Sections – Secondary Arterial (Indian Avenue)	3.0-5
Figure 3.0-2e Standard Street Sections – Major Collector (Barrett Avenue)	3.0-5
Figure 3.0-2f Standard Street Sections – Modified Collector (Harvest Landing Way)	3.0-6
Figure 3.0-2g Standard Street Sections – Private (Private Drive A)	3.0-6
Figure 3.0-3a Expanded Intersection – Arterial	3.0-10
Figure 3.0-3b Expanded Intersection – Secondary Arterial (Raised Median)	3.0-10
Figure 3.0-4 Gateway Entries	3.0-11
Figure 3.0-5 Separated Driveways	3.0-12
Figure 3.0-6 Truck Route Plan	3.0-15
Figure 3.0-7 Retail Delivery Truck Route Plan	3.0-16
Figure 3.0-8 Bicycle Circulation Plan	3.0-19
Figure 3.0-9 Existing and Proposed Water Infrastructure	3.0-30
Figure 3.0-10 Proposed Non-Potable Water Infrastructure	3.0-31
Figure 3.0-11 Existing and Proposed Sewer Infrastructure	3.0-32
Figure 3.0-12 Existing and Proposed Storm Drain Infrastructure	3.0-33
Figure 4.0-1 Screening Methods	4.0-10
Figure 5.0-1 Example Building Designs	5.0-6
Figure 5.0-2 Example Building Designs	5.0-8
Figure 5.0-3 Varied Roof Lines	5.0-9
Figure 5.0-4 Streetscape Landscaping	5.0-14
Figure 5.0-5 Bench Examples	5.0-17
Figure 5.0-6 Planter Examples	5.0-17
Figure 5.0-7 Screen Wall with Berming	5.0-20
Figure 5.0-8 Complimentary Lighting Fixture Examples	5.0-21
Figure 5.0-9 Lighting Examples	5.0-22
Figure 5.0-10 Down-Lighting Examples	5.0-23
Figure 5.0-11 Lighting Fixture Examples	5.0-23
Figure 5.0-12 Post Lighting Examples	5.0-24
Figure 5.0-13 Façade Examples	5.0-25
Figure 5.0-14 Façade Example	5.0-25
Figure 5.0-15 Enhanced Driveway Features	5.0-27
Figure 5.0-16 Pedestrian Access and Drop-Off	5.0-28
Figure 5.0-17 MBU Architecture Illustration	5.0-30



FIGURE NUMBER AND TITLE	PAGE NUMBER
Figure 5.0-18 MBU Architecture Illustration.....	5.0-31
Figure 5.0-19 MBU Lighting Example.....	5.0-35
Figure 5.0-20 MBU Façade Examples	5.0-36
Figure 5.0-21 Commercial/Retail Building Entrance Example.....	5.0-38
Figure 5.0-22 Commercial Façade Example.....	5.0-39
Figure 5.0-23 Example Building Designs	5.0-40
Figure 5.0-24 Example Building Designs	5.0-41
Figure 5.0-25 Commercial Plaza Examples.....	5.0-42
Figure 5.0-26 Commercial Lighting Example.....	5.0-46
Figure 5.0-27 Product Variation Example.....	5.0-49
Figure 5.0-28 Garage Variation Example	5.0-50
Figure 6.0-1 Harvest Landing Logo	6.0-1
Figure 6.0-2 City of Perris Logo	6.0-2
Figure 6.0-3 Pylon Monument Example.....	6.0-3
Figure 6.0-4 Pylon Monument Examples	6.0-4
Figure 6.0-5 Tenant Monument Signs Examples.....	6.0-4
Figure 6.0-6 Pylon and Monumentation Signage Locations	6.0-5



TABLE NUMBER AND TITLE	PAGE NUMBER
Table 1.0-1 General Plan Consistency Analysis.....	1.0-13
Table 2.0-1 Land Use Statistical Data	2.0-1
Table 2.0-2 Allowed Land Uses.....	2.0-5
Table 3.0-1 Roadway Design Requirements and Intersection Spacing.....	3.0-9
Table 3.0-2 Lane Requirements.....	3.0-9
Table 3.0-3 Driveway Spacing.....	3.0-12
Table 3.0-4 Perris Valley Area Drainage Plan Storm Drain Facilities.....	3.0-24
Table 3.0-5 Harvest Landing Specific Plan Proposed Storm Drain Facilities	3.0-26
Table 4.0-1 On-Site Dev Standards by Land Use (C, MBU, WQMP and HLSP)	4.0-1
Table 4.0-2 On-Site Dev Standards by Density Category (Detached Residential) ..	4.0-2
Table 4.0-3 On-Site Dev Standards by Density Category (Attached Residential)	4.0-4
Table 4.0-4 Minimum Building Separation for Attached Residential Uses.....	4.0-5
Table 4.0-5 Residential Density	4.0-6
Table 4.0-6 Parking Requirements	4.0-7
Table 5.0-1 Approved Streetscape Planting	5.0-14
Table 5.0-2 On-Site Plant Palette – MBU Land Use.....	5.0-32
Table 5.0-3 On-Site Plant Palette – Commercial Land Use	5.0-43
Table 5.0-4 Residential Architectural Style - Tuscan	5.0-50
Table 5.0-5 Residential Architectural Style - Cottage	5.0-51
Table 5.0-6 Residential Architectural Style - Monterey.....	5.0-52
Table 5.0-7 Residential Architectural Style - Farmhouse	5.0-53
Table 5.0-8 Residential Architectural Style - Craftsman	5.0-54
Table 5.0-9 Residential Home Types – Single Family Detached.....	5.0-56
Table 5.0-10 Residential Home Types – 2-pack Single Family Detached.....	5.0-57
Table 5.0-11 Residential Home Types – 4-pack Courtyard SF Detached	5.0-58
Table 5.0-12 Residential Home Types – 6-pack Courtyard SF Detached	5.0-59
Table 5.0-13 Residential Home Types – Motorcourt Townhome	5.0-60
Table 5.0-14 Residential Home Types – Row Townhome	5.0-61
Table 5.0-15 Residential Home Types – Greencourt Townhome	5.0-62
Table 7.0-1 Development Phasing.....	7.0-2
Table 7.0-2 Maintenance Plan	7.0-4
Table 7.0-3 Permit Types and Processes.....	7.0-6



1.0 EXECUTIVE SUMMARY

1.1 Introduction

The Harvest Landing Specific Plan's primary purpose is to facilitate transforming the current undeveloped Project Site into a contemporary retail and business park and provide a successful mix of professional office, business park, research and development, retail commercial, related storage and support service uses, residential, and pedestrian linkages. Harvest Landing encompasses 358.28 acres in western Perris and will contain a variety of residential uses, commercial uses, business park uses, businesses, and amenities.

The Harvest Landing Specific Plan text and exhibits serve as the long-range plan for the Specific Plan Project Site and the guide for all future development within the Specific Plan area. This document will guide the build-out of the property in a manner that is consistent with City and State policies, standards, and regulations, and that ensures the various land uses identified in the Specific Plan will be developed in a coordinated manner.

1.2 Specific Plan Vision and Purpose

As provided in California Government Code Section 65450, a Specific Plan has been prepared for the systematic implementation of the updated General Plan. As provided for in State law, a Specific Plan is a regulatory document that provides standards and criteria for the development of a particular geographic area. The Harvest Landing Specific Plan establishes the planning concept, design and development guidelines, administrative procedures, and implementation measures necessary to achieve the orderly and compatible development of the Project area. It is also intended to maintain consistency with and carry out the goals, objectives, and policies of the City of Perris General Plan.

The City of Perris Municipal Code, Section 19.49, specifies the purpose, requirements, regulations, and procedures for preparation of a Specific Plan in the City. California Government Code, Sections 65450 through 65454, establish the authority to adopt a Specific Plan, identify the required contents of a Specific Plan, and mandate consistency with the General Plan.

The California Government Code states that a "Specific Plan shall include a statement of the relationship of the Specific Plan to the General Plan, and further, that it may not be adopted or amended unless found to be consistent with the General Plan." The consistency of the Harvest Landing Specific Plan with the City of Perris General Plan is addressed in Section 1.7, *Consistency Assessment*, below.

The goal of the Harvest Landing Specific Plan is to incorporate the various land uses described in the 2016 General Plan into a unified commercial and business area that capitalizes on its proximity to I-215 and is grounded in market demands.



Specific Plan Amendment

This Specific Plan is the third amendment to the originally approved Harvest Landing Specific Plan, and includes the following:

- Reconfiguring the Specific Plan Land Use Plan by;
- Increasing the commercial land use area from the originally-approved 7.6 acres to 46.24 acres;
- Increasing the Multiple Business Use (MBU) area from the originally-approved 80.9 acres to 189.21 acres;
- Increasing the Water Quality Management Plan (WQMP) acreage from the originally-approved 4.9 acres to 12.91 acres for the WQMP;
- Increasing the roadway acreage from the originally-approved 34.1 acres to 43.62 acres;
- Permitting drive-through operations within the Commercial land use designation with an approved Conditional Use Permit.
- Permitting business park buildings of varied sizes within the MBU land use designation with an approved Development Plan Review;
- Permitting alcohol sales at sit down restaurants within the commercial land use designation with an approved Administrative Determination;
- Allowing non-residential uses within the MBU designation above 0.35 FAR, up to 0.75 FAR;
- Removing 7.26 acres of commercial from the overall Project; and,
- Annexing approximately 16.2 acres of property north of the existing Specific Plan site currently zoned Light Agricultural (A1) and Public (P) with a General Plan land use designation of Business Park (BP).

1.3 Specific Plan Objectives

General Plan Consistency

Base the future development and use of Harvest Landing on the Vision and concepts of the City's General Plan and change the Specific Plan land use designations based on market demands.

Promote Compatible Land Uses for the Area

This Specific Plan will provide land use designations and development standards to promote smart growth principles by:

- Ensuring a balance of commercial and business land uses that maintain and enhance the City's financial stability, economic variety, and environmental integrity.
- Encouraging the development of joint-use and dual-use facilities such as drainage basins and green space with employee amenities.
- Creating connected multi-use areas in close proximity to major roadways that will complement surrounding land uses.

See Section 2.0, *Land Use*, for specific land use requirements for the Specific Plan area.



Streamline the Development Process

The Specific Plan is structured to streamline the entitlement development process, fostering an environment conducive to swift and efficient development. This streamlined approach not only facilitates the realization of development projects but also stimulates economic vitality, laying a solid foundation for sustained financial growth and prosperity in the region. Sections 2.0 through 7.0 of this Specific Plan summarize the allowed land uses, standards, and guidelines for the Specific Plan area as well as the circulation and infrastructure plan and the implementation and administration process.

Promote Sustainable Development

In line with environmental stewardship, the Specific Plan champions sustainable practices through:

- **San Jacinto River Preservation:** Mandating measures to mitigate any adverse impacts on the San Jacinto River, safeguarding its health and vitality.
- **Water-Wise Landscaping:** Advocating for drought-tolerant landscaping to conserve water in Southern California's arid climate.
- **Energy Efficiency:** Prioritizing energy-efficient building design to achieve LEED certification, reducing energy consumption and greenhouse gas emissions. Business park buildings exceeding 400,000 SF will be required to be LEED Certified.

See Sections 3.0, 4.0, and 5.0 for standards and guidelines related to sustainable development as they relate to infrastructure, development, and design, respectively.

Unique Identity and Strong Sense of Place

The Specific Plan will provide a strong sense of place by establishing an identity for the area by:

- Promoting high level development standards to ensure aesthetic cohesiveness throughout the Specific Plan area.
- Using various methods such as logos, streetscapes, banners, public art, unique entry monumentation, and enhanced architecture.
- Beautifying the community by the use of native and appropriate non-native, drought-tolerant species.
- Establishing a strong identity by integrating design and architectural standards in the Specific Plan with a rich pattern of landscaping, streetscaping, signage, and architecture to create attractive and distinctive commercial and business zones.

See Sections 4.0 and 5.0 for standards and guidelines related to aesthetics and identity of the Specific Plan area.

Flexible Business Options

Harvest Landing is rooted in market realities. The business uses that are accommodated must be based on careful consideration of the surrounding uses and market. A unique blend of offices, business park, research and development, business park, retail commercial uses, food services, and residential which take advantage of the adjacent I-215 and complement the surrounding



retail uses, will help build a robust, employment-based economy in Perris. Harvest Landing will ensure the economic viability by creating a modulated site design that can:

- Have phased development;
- Allow for flexibility to best meet market demand at the time of development;
- Enhance the City's existing job base through the creation of a broad range of employment and career opportunities within Harvest Landing; and
- Provide a successful mix of professional office, business park, research and development, business park, retail commercial, related storage and support service uses, and pedestrian linkages.

See Section 2.0, *Land Use*, for allowed land uses in the Specific Plan area that will contribute towards flexible business options while maintaining complementary and compatible uses to the surrounding area.

Identify Infrastructure

The Specific Plan will identify infrastructure utility needs and provide circulation plans for various vehicular (passenger/truck, bus, and transit) and non-vehicular circulation (bikes, pedestrian routes). See Section 3.0, *Circulation and Infrastructure Plan*.

1.4 Specific Plan Format

The Harvest Landing Specific Plan is divided into the following sections:

Section 1: Executive Summary—This section describes the purpose and vision of the Specific Plan, Specific Plan objectives, the Specific Plan area, and the Specific Plan's relationship to other plans including the General Plan.

Section 2: Land Use Plan—This section describes the proposed development concept based on land use and includes the allowed land uses, build-out statistics and parameters.

Section 3: Circulation and Infrastructure Plan—This section provides detailed descriptions of the circulation and infrastructure for the Specific Plan area. Circulation includes the vehicular roadway network, truck routes, public transit, bicycle circulation, and pedestrian circulation. Utilities described in this section includes the water system, recycled water, sewer system, stormwater drainage, dry utilities, and the Water Quality Management Plan Basin land use area.

Section 4: Development Standards—This section provides the required development standards for the Specific Plan area based on the included land uses, and also includes development standards for low-impact design and parking.

Section 5: Design Guidelines—This section provides design guidelines for the land uses within the Specific Plan area in order to create an aesthetically pleasing and cohesive space. Guidelines



include those related to architecture, landscaping, screening, lighting, access, outdoor storage, etc.

Section 6: Signage—This section provides details related to signage for and within Harvest Landing.

Section 7: Implementation and Administration—This section contains the provisions for development processing, the development administration system, and the phasing program for the Harvest Landing Specific Plan.

1.5 Action Expectations

Numerous statements occur in this Specific Plan in the form of standards and guidelines that define action expectations to successfully implement the plan. The following terms clarify the level of commitment intended in the Specific Plan and reflect the expectation or desired outcome. The application of each term to a particular policy or action is a deliberate application of these definitions.

Shall—This type of development standard will always be followed. Shall represents an absolute commitment to the guidance expressed in the policy. (Similar action words: require, enforce, must, ensure.)

Should—This type of design guideline will be followed in most cases and exceptions or degrees of implementation are acceptable with valid reasons. (Similar action word: may.)

Allow—Permit someone else's initiative and support it unless there is a very good reason not to. (Similar action word: permit.)

Restrict—This type of standard or guideline sets specified limits within which action and/or implementation will occur. (Similar action words: control, limit, contain.)

Prohibit—This type of standard requires steps to actively prevent a specified condition or decision from occurring. (Similar action words: forbid, ban.)

Other terminology may appear in certain policy statements. These terms are to be interpreted according to their similarity to the appropriate term described above.

1.6 Existing Setting

1.6.1 Physical/Geographical

Project Location

As shown in Figure 1.0-1, *Regional Location*, Harvest Landing is situated on 358.28 acres in the City of Perris on the western edge of a broad valley formed by the San Jacinto River. Surrounding major physical and topographical features include Lake Perris and the Bernasconi Hills to the northeast,

HARVEST LANDING SPECIFIC PLAN EXECUTIVE SUMMARY



the San Jacinto River to the east and south, the Lakeview Mountains to the southeast, and the Motte Rimrock Reserve to the west.

As shown in Figure 1.0-2, *Local Vicinity*, Harvest Landing is generally bounded on the north by Placentia Avenue, on the south by Nuevo Road, on the east by Perris Boulevard, and on the west by Interstate 215 (I-215) and East Frontage Road. The revised Specific Plan boundary includes the removal of 7.26 acres of commercial land to the south that was divided by the City of Perris in the Pers de Plaza Project, a 5.54-acre property to the north west that was not previously part of the original Specific Plan, and will be designated as MBU and roads, as well as a 10.66-acre Multiple Business Use (MBU) Overlay area on the northwest side of the Specific Plan area (reference Figure 1.0-3, *Project Site with Annexation Area*).

Harvest Landing is ideally situated near transportation routes, including I-215, which runs along the Project Site's western edge, and California State Highway 74, roughly 1.4 miles south of the site. March Air Reserve Base is located to the north of the Project area. Perris Valley Airport, located to the south of the Project Site, provides for smaller aircraft traffic.

Primary access to Harvest Landing shall be from Perris Boulevard, Indian Avenue, Barrett Avenue, Orange Avenue, Frontage Road, and Nuevo Road. Frontage Road traverses the site from north to south and Orange Avenue traverses the site from east to west.

HARVEST LANDING SPECIFIC PLAN EXECUTIVE SUMMARY



Figure 1.0-1, Regional Location

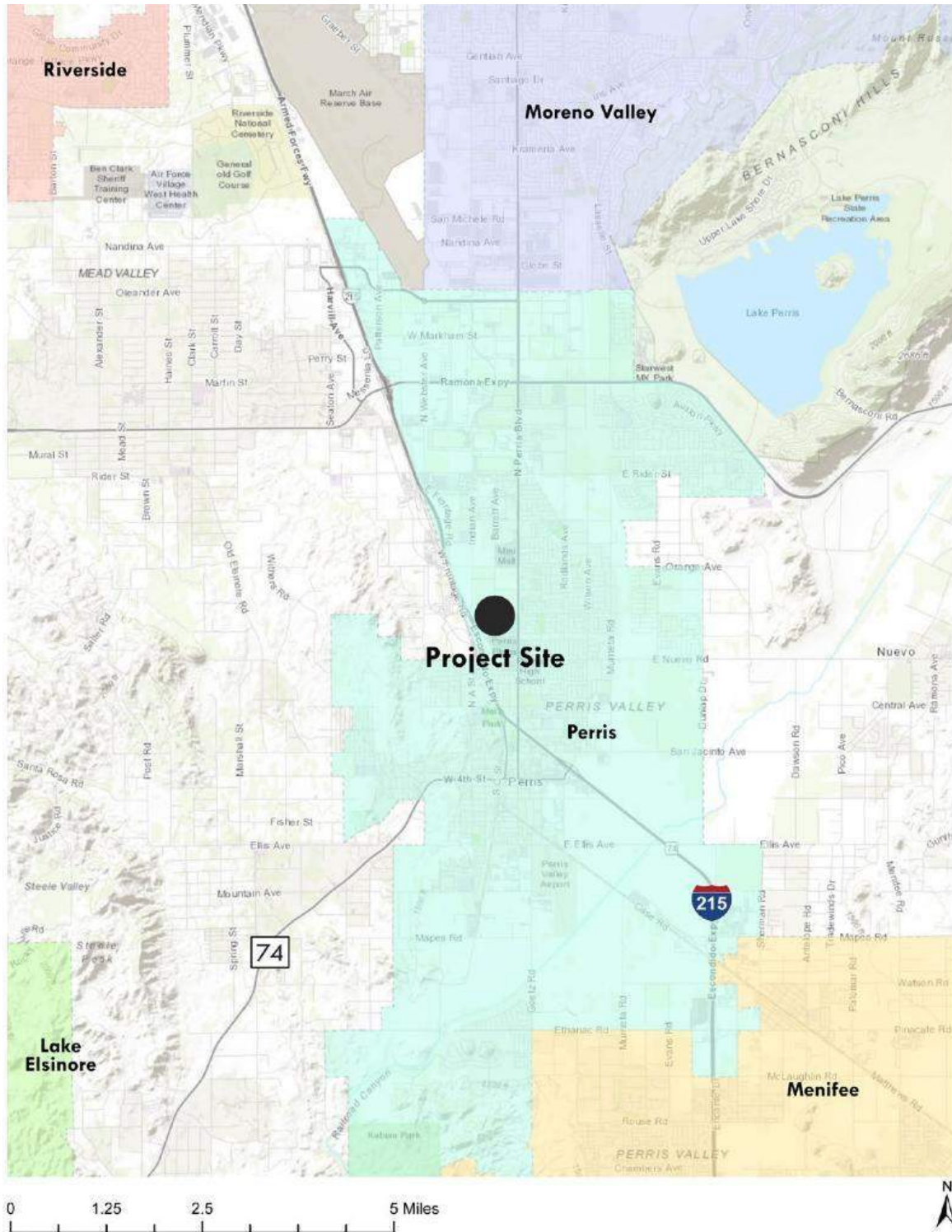




Figure 1.0-2, Local Vicinity





Figure 1.0-3, Project Site with Annexation Area





Existing Topography and Surrounding Uses

The Harvest Landing Specific Plan Project Site is relatively flat, sloping downwards in an easterly direction with elevations ranging from approximately 1,460 to 1,480 feet above mean sea level. The Specific Plan area is currently primarily undeveloped, with approximately 5.1 acres of limited residential use in the center of the site at the intersection of Indian Avenue and Orange Avenue, and an elementary school on a 10.66-acre area at the northwest corner of the site (reference Figure 1.0-2).

Surrounding land uses include the following:

- **North:** Commercial and business park uses.
- **South:** I-215 and commercial uses.
- **East:** Commercial and residential uses, and some vacant land.
- **West:** I-215, commercial uses, and some vacant land.

1.6.2 General Plan Land Use

On April 26, 2005, the City of Perris approved the Land Use Element of the City of Perris General Plan. The Land Use Map was updated on January 3, 2013. The Land Use Element incorporates land use policies and maps to guide the future development of the City of Perris. The Harvest Landing Specific Plan site is located within Planning Area 4 of the General Plan.

Planning Area 4 is designated as a "Freeway Business Park." Although this area was also primarily used for agriculture when the General Plan was adopted, more appropriate land uses for this planning area include Business Park, Commercial, and Business park uses due to the Specific Plan's close proximity to Interstate-215, which allows for greater access and visibility.

1.6.3 Zoning

The Harvest Landing Specific Plan has been adopted by City Council Ordinance and serves as the zoning for the Project Site and is therefore being updated to reflect a new and dynamic mix of residential, commercial, and business park uses. The Specific Plan provides the standards and development criteria to guide future development of the site. The text and diagrams of the Specific Plan address the planning of necessary infrastructure, facilities, and land uses.

1.6.4 Opportunities and Constraints

The opportunities and constraints that exist on the Harvest Landing site are the keys to determining the design, layout, and appropriate uses for the development of the property. The following is a list of the opportunities and constraints that were present at the time of the writing of this document.



Opportunities

Interstate 215

Interstate 215 runs along the western edge of the Project Site. The freeway provides clear visibility of the site as well as direct access via Nuevo Road and Placentia Avenue. The land adjacent to the freeway is an ideal location for business park, office, business park and retail uses, which will benefit from the proximity to this major transportation corridor. The placement of business uses along the freeway provide for the creation of a significant number of jobs in the City.

Surrounding Uses

Harvest Landing is surrounded by several uses that influenced the design of the Project. Critically, there are significant commercial retail uses either adjacent or in close proximity to the site.

The existing Perris Plaza with theaters, restaurants, and retail uses bounds the Project on the south. In addition, the Perris Town Center is located to the south across Nuevo Road. There are several other smaller retail centers along Perris Boulevard. Given these uses and as detailed below, the convenience retail market is well served in the area. Therefore, the Harvest Landing Specific Plan is focused on providing locations for office, light business park, and freeway-dependent retail uses which will allow for workers to utilize the retail amenities on their lunch breaks and after work hours.

Constraints

Airport Influence Areas

As explained in greater detail under Section 1.7.2, *Airport Influence Areas*, Harvest Landing is located near the March Air Reserve Base/Inland Port Airport and the Perris Valley Airport and falls within their influence areas. Development within these areas must adhere to specific requirements, such as those pertaining to density, building height, noise, glare, etc., so as not to conflict with airport operations and safety considerations.

Noise

The site is located between major noise sources. Vehicular noise from I-215, Perris Boulevard, and Placentia Avenue are critical factors to consider in the location, buffering, and/or mitigation of noise sensitive uses. The Specific Plan Environmental Impact Report (EIR) will contain an analysis of potential impacts related to noise in compliance with the California Environmental Quality Act (CEQA).

1.6.5 Required Governmental Actions

This Specific Plan will implement applicable elements of the City of Perris General Plan and include detailed information about the area's need for infrastructure improvements such as roads, water, sewer, and flood control facilities.

HARVEST LANDING SPECIFIC PLAN EXECUTIVE SUMMARY



The Harvest Landing Specific Plan has been prepared pursuant to the State of California Government Code, Title 7, Division 1, Article 8, Section 65450 which grants authority to cities to adopt Specific Plans for purposes of implementing the goals and policies of their General Plans. The Government Code states that Specific Plans may be adopted either by Resolution or by Ordinance and that the Specific Plan is required to be consistent with the General Plan. The Government Code sets forth the minimum requirements and review procedures for Specific Plans including the provision of a land use plan, infrastructure and public services plan, criteria and standards for development, and implementation measures. The Government Code also states that Specific Plans may address any other subjects, which in the judgment of the city are necessary or desirable for implementation of the General Plan.

The Harvest Landing Specific Plan requires action by the Planning Commission and City Council on the following:

- **Specific Plan Amendment No. 22-05250.** To revise land use designations, establish a plan for public facilities, design guidelines, and a development incentive program.
- **General Plan Amendment No. 24-05175.** To amend the Land Use Element and Circulation Element of the City of Perris General Plan to designate all properties within the revised Project area as Specific Plan.
- **Zone Change No. 24-05176.** To establish Specific Plan zoning for the properties within the Project Site.

Development Plan Reviews, Conditional Use Permits, Tentative Tract Maps, and a Development Agreement will be required to implement the Specific Plan buildout.

In addition to City of Perris discretionary approvals, the following Approvals or permits would be required:

- City of Perris Construction Permits (demolition, grading, building, etc.);
- County of Riverside Flood Control and Water Conservation;
- District approval of hydrology/drainage and storm drain facility plans;
- California State Water Resources Control Board Notice of Intent to permit coverage (General Construction Permit regulates stormwater and no stormwater discharges associated within construction activities);
- Regional Water Quality Control Board MS-4 Permit for Municipal Stormwater Permitting – Water Quality Management Plan; and,
- Eastern Municipal Water District approval of potable and non-potable water and sewer conveyance systems.



1.7 Consistency Assessment

1.7.1 General Plan

State law requires that a General Plan and zoning be consistent. The Harvest Landing Specific Plan process includes an amendment to the City's General Plan and Municipal Code to rectify inconsistencies between these two documents.

As described above, the General Plan had previously placed the Harvest Landing site within Planning Area 4, Freeway Business Park, and designated the Project Site for a combination of Specific Plan, business park, and community commercial land uses. The logic for these land use designations is clearly described in the General Plan: "The proximity of Planning Area 4 to I-215 makes it ideal for freeway-oriented land uses. New Business Park and freeway-oriented business can create additional employment opportunities, resulting in a larger customer base to support retail outlets, restaurants, and other service-oriented businesses that serve the before-during-after work needs of workers and professionals" (Perris General Plan, 2005, page 4).

The Harvest Landing Specific Plan implements the goals and policies of the General Plan, and also maintains the spirit and intentions of the General Plan by utilizing the mixture of MBU and Commercial land uses (as well as Water Quality Management Plan land use for stormwater management) as called for in the General Plan. However, the location and mixture of the General Plan land use designations are modified to reflect a unified land use design, viable mixture of uses, and market realities.

Goals/Policies

Future building permits and public works projects must be consistent with the Specific Plan (Government Code Sections 65455, 66473.5, 65860, and 65401). All projects that are found to be consistent with this Specific Plan will likewise be deemed consistent with the City's General Plan. Table 1.0-1 lists goals and policies from the General Plan that are applicable to the Harvest Landing Specific Plan, and describes the Specific Plan's consistency with these goals and policies.

Table 1.0-1, General Plan Consistency Analysis

City of Perris General Plan Goal/Policy	Harvest Landing Specific Plan Consistency
Land Use Element	
Policy II.A Require new development to pay its full, fair-share of infrastructure costs	Consistent. Development within the Specific Plan will pay development impact fees that contribute to infrastructure improvements pursuant to the Project Development Agreement.
Policy II.B Require new development to include school facilities or pay school impact fees, where appropriate.	Consistent. Pursuant to Development Agreement for the Project the Project will pay development impact fees to mitigate the cost of public facilities including schools needed to offset the impact of new development

HARVEST LANDING SPECIFIC PLAN EXECUTIVE SUMMARY



City of Perris General Plan Goal/Policy	Harvest Landing Specific Plan Consistency
<p>Policy V.A. Restrict development in areas at risk of damage due to disasters.</p>	<p>Consistent. The Specific Plan area is not within a high or very high fire hazard severity zone, and is in an area of minimal flood hazard.</p>
<p>Policy V.B. Ensure land use compatibility near March Air Reserve Base/Inland Port (ARB/IP) by implementing the policies of the 2014 March ARB/IP Airport Land use Compatibility Plan (ALUCP).</p>	<p>Consistent. The Project is consistent with the Basic Compatibility Criteria table as outlined in Municipal Code Section 19.51.060. The Project does not propose any potentially hazardous activities or the bulk storage of hazardous materials which would impact aircraft safety. Therefore, the Project would not pose any hazards to MARB/IPA. The project has been determined to be consistent with the Airport Land Use Compatibility Plan as reviewed and confirmed by the Riverside County Airport Land Use Commission.</p>
<p>Circulation Element</p>	
<p>Policy I.B Support development of a variety of transportation options for major employment and activity centers including direct access to commuter facilities, primary arterial highways, bikeways, park-n-ride facilities, and pedestrian facilities.</p>	<p>Consistent. The existing transit service will continue to serve its ridership in the area and may also serve employees of the commercial and business park components of the Project as well as visitors of the commercial component of the Project. The Project includes the construction of a Class II bike lane on Indian Avenue, Orange Avenue, Perris Boulevard, and Barrett Avenue, as well as a 10-foot-wide shared use trail on Frontage Road; and the Project includes refreshing striping on the adjacent streets, thereby improving bicycle facilities and network. Construction of a 10-foot-wide shared use trail along the Project frontage with Frontage Road and Perris Boulevard and construction of a 6-foot-wide sidewalk along the Project frontage along Indian Avenue, Orange Avenue, Barrett Avenue, Harvest Landing Way, and Private Drive will improve pedestrian facilities and the sidewalk network. The Project will also improve Barrett Avenue, Frontage Road, and Orange Avenue west of Barrett Avenue to full widths, and improve Perris Boulevard and Orange Avenue east of Barrett Avenue to half width. On Indian Avenue, the Project will improve the right-of-way to its ultimate width between Orange Avenue and the southern point of the Val Verde Elementary School frontage and half width on northbound Frontage Road along the Val Verde Elementary School frontage. In addition, the Project will construct a bus stop along Perris Boulevard adjacent to the proposed commercial area.</p>
<p>Policy I.D Encourage and support the development of projects that facilitate and enhance the use of alternative modes of transportation.</p>	<p>Consistent. The Project vicinity is served by RTA Route 19, 27, and 30. This existing transit service will continue to serve its ridership in the area and may also serve employees of the commercial and</p>

HARVEST LANDING SPECIFIC PLAN EXECUTIVE SUMMARY



City of Perris General Plan Goal/Policy	Harvest Landing Specific Plan Consistency
	<p>business park components of the Project as well as visitors of the commercial component of the Project. In addition, the Project includes the addition of a bus stop along Perris Boulevard adjacent to the proposed commercial area. The Project also includes construction of a sidewalk along Perris Boulevard to provide additional pedestrian access to the bus stop from the Project's commercial and business park uses.</p>
<p>Policy II.A Maintain the following target Levels of Service:</p> <ul style="list-style-type: none"> • LOS "D" along all City maintained roads (including intersections) and LOS "D" along I-215 and SR 74 (including intersections with local streets and roads). An exception to the local road standard is LOS "E", at intersections of any Arterials and Expressways with SR 74, the Ramona-Cajalco Expressway or at I-215 freeway ramps. 	<p>Consistent. Development within the Specific Plan will pay fair share for road improvements to achieve a satisfactory intersection LOS D.</p>
<p>Policy II.B Maintain the existing transportation network while providing for future expansion and improvement based on travel demand, and the development of alternative travel modes.</p>	<p>Consistent. The existing transit service will continue to serve its ridership in the area and may also serve employees of the commercial and business park components of the Project as well as visitors of the commercial component of the Project. The Project includes construction of a bus stop along Perris Boulevard adjacent to the proposed commercial area. The Project also includes the construction of a Class II bike lane on Indian Avenue, Orange Avenue, Perris Boulevard, and Barrett Avenue, as well as a 10-foot-wide shared use trail on Frontage Road, and the Project includes refreshing of striping on the adjacent streets to improve bicycle facilities and network. Construction of a 10-foot-wide shared use trail along the Project frontage with Frontage Road and Perris Boulevard and construction of a 6-foot-wide sidewalk along the Project frontage along Indian Avenue, Orange Avenue, Barrett Avenue, Harvest Landing Way, and Private Drive will improve pedestrian facilities and the sidewalk network. The Project will also improve Barrett Avenue, Frontage Road, and Orange Avenue west of Barrett Avenue to full widths, and improve Perris Boulevard and Orange Avenue east of Barrett Avenue to half width. On Indian Avenue, the Project will improve the right-of-way to its ultimate width between Orange Avenue and the southern point of the Val Verde Elementary School frontage and half width on northbound Frontage Road along the Val Verde Elementary School frontage.</p>

HARVEST LANDING SPECIFIC PLAN EXECUTIVE SUMMARY



City of Perris General Plan Goal/Policy	Harvest Landing Specific Plan Consistency
<p>Policy III.A Implement a transportation system that accommodates and is integrated with new and existing development and is consistent with financing capabilities.</p>	<p>Consistent. Development within the Specific Plan will pay development impact fees to contribute to infrastructure improvements pursuant to the approved Development Agreement for the Harvest Landing Specific Plan</p>
<p>Policy IV.A Provide non-motorized alternatives for commuter travel as well as recreational opportunities that maximize safety and minimize potential conflicts with pedestrians and motor vehicles.</p>	<p>Consistent. The Project includes the construction of bikeways and a 12.91-acre water quality management basin with walking paths and exercise equipment. In addition, the Project includes the construction of a Class II bike lane on Indian Avenue, Orange Avenue, Perris Boulevard, and Barrett Avenue, as well as a 10-foot-wide shared use trail on Frontage Road, and the Project will refresh striping on the adjacent streets to improve the bicycle facilities and network. The Project also includes construction of a 10-foot-wide shared use trail along the Project frontage with Frontage Road and Perris Boulevard and construction of a 6-foot-wide sidewalk along the Project frontage along Indian Avenue, Orange Avenue, Barrett Avenue, Harvest Landing Way, and Private Drive to improve pedestrian facilities and the sidewalk network.</p>
<p>Policy V.A Provide for safe movement of goods along the street and highway system.</p>	<p>Consistent. The Project includes five truck driveways along Frontage Road and installation of Private Drive A for the business park portion of the Phase 1 development. The commercial component of the Phase 1 development requires one multi-purpose driveway on Orange Avenue, one multi-purpose driveway on Harvest Landing Way, and one multi-purpose driveway on Barrett Avenue. Phase 2 development without the Overlay will require at least one multi-purpose driveway on Frontage Road and at least two multi-purpose driveways along Indian Avenue south of Val Verde Elementary School. Development of the Overlay Area will require an additional multi-purpose driveway along Indian Avenue, should the site be developed. The Project prohibits trucks from the business park buildings from utilizing Barrett Avenue, which will be prevented through installation of signage as required by City of Perris Good Neighbor Guideline Policy 1.16.</p>
<p>Policy VIII.A Encourage the use of Transportation Demand Management (TDM)/Transportation Control Measure (TCM) strategies and programs that provide attractive, competitive alternatives to the single-occupant vehicle.</p>	<p>Consistent. Facility operators will establish a Voluntary Commute Trip Reduction Program. In addition, the Project includes bicycle parking for Project employees.</p>
<p>Policy VIII.B Identify Transportation Systems Management (TSM) strategies that will assist in</p>	<p>Consistent. Facility operators will establish a Voluntary Commute Trip Reduction Program. In</p>

HARVEST LANDING SPECIFIC PLAN EXECUTIVE SUMMARY



City of Perris General Plan Goal/Policy	Harvest Landing Specific Plan Consistency
mitigating traffic impacts and that will maintain the desired level of service along the street and highway system.	addition, the Project includes bicycle parking for Project employees.
Conservation Element	
Policy II.A Comply with state and federal regulations to ensure protection and preservation of significant biological resources.	Consistent. The Project will not conflict with any State and federal regulations.
Policy III.A Review all public and private development and construction projects and any other land use plans or activities within the MSHCP area, in accordance with the conservation criteria procedures and mitigation requirements set forth in the MSHCP.	Consistent. Development of the Specific Plan will be in compliance with the MSHCP.
Policy IV.A Comply with state and federal regulations and ensure preservation of the significant historical, archaeological and paleontological resources.	Consistent. The Project will comply with State and federal regulations and ensure preservation of the significant historical, archaeological and paleontological resources.
Policy V.A Coordinate land-planning efforts with local water purveyors.	Consistent. As detailed in the Environmental Impact Report prepared for the Project, the demands of the Specific Plan are within the projections of Eastern Municipal Water District's 2020 Urban Water Management Plan.
Policy VI.A Comply with requirements of the National Pollutant Discharge Elimination System (NPDES).	Consistent. Stormwater detention facilities will be sized to meet the required design capture volume for pollutant control requirements.
Goal VII Protection of significant landforms.	Consistent. The Specific Plan area is disturbed from previous agricultural activities and is vegetated with non-native grasses. The site is relatively flat with a gentle slope. There are no significant landforms present onsite that would be removed as a part of the Project.
Policy VII.A Preserve significant hillsides and rock outcroppings in the planning areas.	Consistent. The Specific Plan area is relatively flat and does not contain any hillsides or rock outcroppings that would be removed/graded during the development of the Specific Plan Area.
Policy VIII.A Adopt and maintain development regulations that encourage water and resource conservation.	Consistent. Development of the Specific Plan will be in compliance with CALGreen and adhere to a Stormwater Pollution Prevention Plan (SWPPP) and Water Quality Management Plan.
Policy VIII.B Adopt and maintain development regulations that encourage recycling and reduced waste generation by construction projects	Consistent. The Project includes implementation of recycling programs.
Policy VIII.C Adopt and maintain development regulations which encourage increased energy efficiency in buildings, and the design of durable buildings that are efficient and economical to own	Consistent. The Project will comply with CALGreen/Title 24 requirements to implement energy conservation measures and water efficient

HARVEST LANDING SPECIFIC PLAN EXECUTIVE SUMMARY



City of Perris General Plan Goal/Policy	Harvest Landing Specific Plan Consistency
and operate. Encourage green building development by establishing density bonuses, expedited permitting, and possible tax deduction incentives to be made available for developers who meet LEED building standards for new and refurbished developments (U.S. Green Building Council's Leadership in Energy and Environmental Design green building programs).	plumbing. Buildings over 400,000 SF will be LEED compliant.
Policy IX.A Encourage land uses and new development that support alternatives to the single occupant vehicle.	Consistent. The Project includes EV van accessible, clean air van carpool, Voluntary Commute Trip Reduction Program and bicycle parking.
Policy X.B Encourage the use of trees within project design to lessen energy needs, reduce the urban heat island effect, and improve air quality throughout the region.	Consistent. A total of 1,239,079 square feet, or 20.2 percent of the business park site, will be covered in drought tolerant landscaping, primarily along the boundaries of each proposed parcel and throughout parking areas. A variety of 24-inch and 36-inch box trees, 15 gallon trees, shrubs, accents, and groundcover would be planted. Proposed tree species would include Blue Palo Verde, Desert Willow, Chitalpa, Camphor Tree, Olive, Canary Island Pine, Afghan Pine, London Plane, Chilean Mesquite, African Sumac, California Pepper, and Brisbane Box.
Policy X.C Encourage strategic shape and placement of new structures within new commercial and business park projects.	Consistent. The Project is designed to orient the business park activities and truck access to the west near I-215 with the commercial uses acting as a buffer between the MBU uses to the west and the residences to the east of Perris Boulevard.
Housing Element	
Policy 2.1 Promote development within specific plans that provide a variety of housing types and densities based on the suitability of the land, including the availability of infrastructure, the provision of adequate services and recognition of environmental constraints.	Consistent. The Project allows for the future development of residential uses within the Low Residential, Medium Residential, High Residential, MBU and Commercial land use designations to ensure compatibility with adjacent land uses and to achieve the City's long-term housing goals. Residential development is permitted throughout the Specific Plan consistent with existing density requirements and must comply with the Specific Plan's Development Standards. The Specific Plan allows for a total residential capacity of 1,860 dwelling units allocated across three density categories as noted in Section 4.2 of the Specific Plan.
Policy 2.4 Promote construction of units consistent with the new construction needs identified in the Regional Housing Needs Assessment (RHNA).	Consistent. The City's RHNA buffer is able to accommodate the housing capacity reduction in the Specific Plan area elsewhere in the City. The

HARVEST LANDING SPECIFIC PLAN EXECUTIVE SUMMARY



City of Perris General Plan Goal/Policy	Harvest Landing Specific Plan Consistency
	Housing Element will be updated in alignment of state law.
Noise Element	
Policy II.A Appropriate measures shall be taken in the design phase of future roadway widening projects to minimize impacts on existing sensitive noise receptors.	Consistent. Noise and vibration impacts are analyzed in the Environmental Impact Report for the Project, which concluded development of the Specific Plan, including proposed roadway widening activities, would not result in significant noise related impacts. Additionally, development of the Specific Plan will comply with relevant noise regulations.
Policy IV.A Reduce or avoid the existing and potential future impacts from air traffic on new sensitive noise land uses in areas where air traffic noise is 60 dBA CNEL or higher.	Consistent. A portion of the Specific Plan Area from the southeast corner of the intersection of Indian Avenue and Orange Avenue up to the northeast corner of the site is within the 60 dBA CNEL noise contour, which is considered a moderate noise impact per MARB/IPA ALUCP standards. The proposed Commercial and MBU land uses within the 60 dBA CNEL noise contour are consistent with the uses permitted by the MARB/IPA ALUCP.
Policy V.A New large scale commercial or business park facilities located within 160 feet of sensitive land uses shall mitigate noise impacts to attain an acceptable level as required by the State of California Noise/Land Use Compatibility Criteria.	Consistent. As described in the Environmental Impact Report prepared for the Project, development of the Specific Plan would not generate noise levels in excess of 60 dBA CNEL at noise sensitive land uses.
Safety Element	
Policy S-2.1 Require road upgrades as part of new developments/major remodels to ensure adequate evacuation and emergency vehicle access. Limit improvements for existing building sites to property frontages.	Consistent. Development within the Specific Plan area includes the design and construction of internal access and provide fire suppression facilities (e.g., hydrants and sprinklers) in conformance with the City's Municipal Code. The Riverside County Fire Department reviews development plans as part of the construction permitting process to ensure that emergency access is provided pursuant to the requirements of the Uniform Fire Code and Section 503 of the California Fire Code (Title 24, California Code of Regulations, Part 9).
Policy S-2.2 Require new development or major remodels include backbone infrastructure master plans substantially consistent with the provisions of "Infrastructure Concept Plans" in the Land Use Element.	Consistent. Development of the Phase 1 area includes construction of a 12.91-acre water quality management basin, which includes a shared bioretention basin for flows from the Community Shopping Center and Commercial Big Box Retail sites, an underground detention system to store treatment flows, and a lift station. Phase 1 development includes the construction of a new 12-foot by 7-foot reinforced concrete box storm drain line in Perris Boulevard to Harvest Landing Way, where it will transition to a 102-inch diameter

HARVEST LANDING SPECIFIC PLAN EXECUTIVE SUMMARY



City of Perris General Plan Goal/Policy	Harvest Landing Specific Plan Consistency
	<p>storm drain line and will continue north on Barrett Avenue and connect to the proposed storm drain line within Orange Avenue. The Project will construct an 72-inch diameter storm drain line heading west on Orange Avenue, which will transition to a 54-inch diameter storm drain line west of Indian Avenue, and transition to a 42-inch diameter storm drain line prior to the intersection with Frontage Road. South of Harvest Landing Way, the Project includes construction of a new 48-inch diameter storm drain line. The Project includes installation of a 36-inch storm drain line in the proposed 12-foot-wide EMWD maintenance road in the vacated portion of Indian Avenue and a 24-inch storm drain line in Private Drive A. In addition, the Project includes improvements to approximately 1,400 linear feet of off-site flood control channel Perris Valley Master Drainage Plan Line K.</p> <p>All Business Park site buildings and the Commercial Big Box Retail building will be served by 8-inch sewer lines which would connect to a new proposed 15-inch sewer main in Perris Boulevard. The new 15-inch sewer main in Perris Boulevard would extend the existing 15-inch sewer main in Perris Road. The new extension would travel south on Perris Boulevard and east on Nuevo Road to Murrieta Road for approximately 8,344 linear feet. Phase 1 development would require the construction of a new 8-inch diameter waterline along Barrett Avenue and an 8-inch waterline in Orange Avenue. In addition, the Project would include construction of an 8-inch waterline in Frontage Road which would connect to a new 8-inch waterline in Walmart Supercenter Drive.</p>
<p>Policy S-2.3 Primary access routes shall be completed prior to the first certificate of occupancy in developments located in outlying areas of the City.</p>	<p>Consistent. Primary access driveways are reviewed by the City through the Plan Check process.</p>
<p>Policy S-2.4 Provide adequate emergency facilities to serve existing and future residents, ensuring that all new essential facilities are located outside of hazard prone areas</p>	<p>Consistent. The Project is required to adhere to City Ordinance No. 1182, which sets forth policies, regulations, and fees related to the funding and construction of facilities (Community Facilities, Police, Fire, Parks and Recreation, Library, and Streets) necessary to address direct and cumulative environmental effects generated by new development.</p>
<p>Policy S-2.5 Require all new developments, redevelopments, and major remodels to provide adequate ingress/egress, including at least two</p>	<p>Consistent. The Project includes multiple points of access for each of the respective site plans to be in compliance with the City of Perris's Municipal Code and Riverside County Fire Department.</p>

HARVEST LANDING SPECIFIC PLAN EXECUTIVE SUMMARY



City of Perris General Plan Goal/Policy	Harvest Landing Specific Plan Consistency
points of access for sites, neighborhoods, and/or subdivisions.	
Policy S-3.3 Ensure businesses in Perris are prepared for emergency and disaster situations.	Consistent. The Project will be built in compliance with the California Building Code and include signage for emergency situations.
Policy S-4.3 Require new development projects and major remodels to control stormwater run-off on site.	Consistent. Stormwater drainage facilities at site will be adequately sized to meet minimum retention volume requirements of the MS4 Permit.
Policy S-5.3 Promote new development and redevelopment in areas of the City outside the VHFHSZ and allow for the transfer of development rights into lower-risk areas, if feasible.	Consistent. The Specific Plan is outside of the VHFHSZ.
Policy S-5.6 All developments throughout the City Zones are required to provide adequate circulation capacity, including connections to at least two roadways for evacuation.	Consistent. The Project is consistent with the City's circulation requirements. Each proposed phase and overall development has been designed to provide adequate circulation capacity, including connections to at least two roadways to ensure safe and efficient evacuation. Accordingly, this standard has been met throughout the project.
Policy S-5.8 Adopt State Fire Safe Regulations as necessary for new development and require verification of adequate water supply, adequate ingress/egress for evacuation purposes, proper use of building design and materials, and proper treatment of fuels to reduce fire vulnerability.	Consistent. The Environmental Impact Report for the Project concluded that it would require less water than what was estimated by the Eastern Municipal Water District's 2020 Urban Water Management Plan for the site. Thus, the Project will not require additional water supplies. Ingress and egress will be designed according to the City's Municipal Code. The Riverside County Fire Department will review development plans as part of the construction permitting process to ensure that emergency access is provided pursuant to the requirements of the Uniform Fire Code and Section 503 of the California Fire Code (Title 24, California Code of Regulations, Part 9).
Policy S-5.9 Ensure that the City maintains adequate facilities and fire service personnel in conformance with the Riverside County Fire Department's Fire Strategic Plan.	Consistent. Development under the Specific Plan will pay applicable Development Impact Fees pursuant to the approved Development Agreement for the Harvest Landing Specific Plan (Community Facilities, Police, Fire, Parks and Recreation, Library, and Streets) necessary to address direct and cumulative environmental effects generated by new development. Development impact fees collected ensure that the level of fire protection services is maintained and response times are improved and can be applied to the purchase of equipment, maintenance of existing facilities, and the construction of new facilities.
Policy S-5.10 Ensure that existing and new developments have adequate water supplies and	Consistent. The Environmental Impact Report for the Project concluded that it would require less

HARVEST LANDING SPECIFIC PLAN EXECUTIVE SUMMARY



City of Perris General Plan Goal/Policy	Harvest Landing Specific Plan Consistency
conveyance capacity to meet daily demands and firefighting requirements.	water than what was estimated by the Eastern Municipal Water District's 2020 Urban Water Management Plan for the site. Thus, the Project will not require additional water supplies.
Policy S-6.1 Ensure new development and redevelopments comply with the development requirements of the AICUZ Land Use Compatibility Guidelines and ALUP Airport Influence Area for March Air Reserve Base.	Consistent. The Project is consistent with the Basic Compatibility Criteria table as outlined in Municipal Code Section 19.51.060, and would not pose any hazards to MARB/IPA.
Policy S-6.2 Effectively coordinate with March Air Reserve Base, Perris Valley Airport, and the March Inland Port Airport Authority on development within its influence areas.	Consistent. The Project is consistent with the Basic Compatibility Criteria table as outlined in Municipal Code Section 19.51.060.
Policy S-6.3 Effectively coordinate with March Air Reserve Base and Perris Valley Airport on development within its influence areas.	Consistent. The Project is consistent with the Basic Compatibility Criteria table as outlined in Municipal Code Section 19.51.060.
Policy S-7.1 Require all development to provide adequate protection from damage associated with seismic incidents.	Consistent. Approval of Project development plans is contingent on compliance with the California Building Code, which would ensure that buildings provide adequate protection from damage associated with seismic incidents.
Policy S-7.2 Require geological and geotechnical investigations by State-licensed professionals in areas with potential for seismic and geologic hazards as part of the environmental and development review and approval process.	Consistent. A Preliminary Geotechnical Evaluation was prepared for the Project as part of the Environmental Impact Report.
Policy S-7.3 Ensure slope stability issues are effectively addressed in both developed and developing areas within the City.	Consistent. The Specific Plan area and the adjacent parcels are relatively flat, with a slight slope in the southwestern direction, and do not contain any hills or steep slopes.
Policy S-8.1 Coordinate with the Riverside County Fire Department to ensure commercial and business park activities comply with all federal, state, county, and local laws regulating hazardous materials waste.	Consistent. Routine use and transport of hazardous materials would comply with applicable laws and regulations.
Policy S-8.2 Ensure that the transport, use, storage, and disposal of hazardous materials occur in a responsible manner that protects public health and safety.	Consistent. Routine use and transport of hazardous materials would comply with applicable laws and regulations.
Open Space Element	
Policy I.A Develop more active recreational parks.	Consistent. The Project includes the development of an employee walking path and fitness areas within the 12.91-acre water basin on the west side of the retail center. In addition, employee amenity areas such as basketball and pickleball courts for MBU buildings exceeding 100,000 square feet are included as a part of the Specific Plan.
Healthy Community Element	

HARVEST LANDING SPECIFIC PLAN EXECUTIVE SUMMARY



City of Perris General Plan Goal/Policy	Harvest Landing Specific Plan Consistency
<p>Policy HC 1.3 Improve safety and the perception of safety by requiring adequate lighting, street visibility, and defensible space</p>	<p>Consistent. The Project will provide lighting around the Specific Plan area consistent with Section 19.02.110 of the City's Municipal Code.</p>
<p>Policy HC 3.5 Promote job growth within Perris to reduce the substantial out-of-Perris job commutes that exist today.</p>	<p>Consistent. Full build out of the Specific Plan area would result in an estimated 6,427 new jobs within the City.</p>
<p>Policy HC 6.1 Support regional efforts to improve air quality through energy efficient technology, use of alternative fuels, and land use and transportation planning.</p>	<p>Consistent. Development in the Specific Plan area will be developed to achieve LEED Silver certification and comply with Title 24 building efficiency requirements. The Project will also provide EV charging stations.</p>
<p>Policy HC 6.2 Support regional water quality efforts that balance water conservation, use of recycled water, and best practices in watershed management.</p>	<p>Consistent. The Project includes installation of a recycled water line for landscape irrigation, which will be designed consistent with Municipal Code Section 19.70.030, which outlines water conservation requirements for new developments.</p>
<p>Policy HC 6.3 Promote measures that will be effective in reducing emissions during construction activities.</p> <ul style="list-style-type: none"> • Perris will ensure that construction activities follow existing South Coast Air Quality Management District (SCAQMD) rules and regulations • All construction equipment for public and private projects will also comply with California Air Resources Board's vehicle standards. For Projects that may exceed daily construction emissions established by the SCAQMD, Best Available Control Measures will be incorporated to reduce construction emissions to below daily emission standards established by the SCAQMD • Project proponents will be required to prepare and implement a Construction Management Plan which will include Best Available Control Measures among others. Appropriate control measures will be determined on a project by project basis, and should be specific to the pollutant for which the daily threshold is exceeded. 	<p>Consistent. The Project will comply with applicable SCAQMD rules to reduce construction-related air quality emissions.</p>
Environmental Justice Element	
<p>Continue to ensure new development is compatible with the surrounding uses by co-locating compatible uses and using physical barriers, geographic features, roadways or other infrastructure to separate less compatible uses. When this is not possible, impacts may be mitigated using: noise barriers, building insulation, sound barriers, traffic diversion.</p>	<p>Consistent. The Specific Plan Area includes business park uses adjacent to I-215 and other business park uses to the west, and includes commercial uses to the east of the MBU area to buffer between the business park uses onsite and the residences to the east.</p>
<p>As part of the development review process, require conditions that promote Good Neighbor Policies for Business park Development for business park</p>	<p>Consistent. The Project application was submitted prior to the Good Neighbor Guidelines policies.</p>



City of Perris General Plan Goal/Policy	Harvest Landing Specific Plan Consistency
buildings larger than 100,000 square feet. The conditions shall be aimed at protecting nearby homes, churches, parks, day-care centers, schools, and nursing homes from air pollution, noise lighting, and traffic associated with large warehouses, making them a "good neighbor."	
<p>A community that actively works to reduce the impacts of poor air quality.</p> <ul style="list-style-type: none"> Participate in air quality planning efforts with local, regional, and State agencies that improve local air quality to protect human health, minimize the disproportionate impacts on sensitive population groups, and ensure that City concerns are resolved early in the process. Inform existing industries of the state 5-minute maximum idling limitation and condition new business park projects to enforce the state's 5-minute maximum idling limitation for stationary diesel trucks. 	<p>Consistent. The Project is designed to achieve LEED Silver certification and comply with Title 24 building efficiency requirements in order to minimize greenhouse gas emissions.</p>
Require developers to provide pedestrian and bike friendly infrastructure in alignment with the vision set in the City's Active Transportation plan or active transportation in-lieu fee to fund active mobility projects.	<p>Consistent. An employee walking path is included within the 12.91-acre water basin area on the west side of the community retail site.</p>

1.7.2 Airport Influence Areas

The Harvest Landing Specific Plan area overlaps with airport land use compatibility zones for the March Air Reserve Base/Inland Port Airport. The airports are within the Riverside County Airport Land Use Compatibility Plan (ALUCP) Policy Document, which includes individual plans for each airport.

March Air Reserve Base/Inland Port Airport

All of the Harvest Landing Specific Plan area, except for the southernmost tip, is located within Compatibility Zone C2 for the March Air Reserve Base/Inland Port Airport, as designated in Map MA-1, *Compatibility Map*, of the March Air Reserve Base/Inland Port ALUCP, most recently updated and adopted in 2014.

According to the March Air Reserve Base/Inland Port ALUCP, Zone C2 is a "flight corridor zone" and limits residential uses to no more than 6.0 dwelling units per acre, and other uses to an average of 200 people per acre. The C2 Zone has no requirement for amount of open land. Development conditions include discouragement of children's schools, airspace review required for objects over 70 feet tall, notification to the March Air Reserve Base of any land use having an electromagnetic radiation component to assess whether a potential conflict with radio



communications could result, and deed notice and disclosure of real estate transactions within airport compatibility zones. Prohibited uses include highly noise-sensitive outdoor nonresidential uses (e.g., major spectator-oriented sports stadiums, amphitheaters, concert halls, and drive-in theaters), and hazards to flight including physical (e.g., tall objects), visual, and electronic forms of interference with the safety of aircraft operations. Land use development that may cause attraction of birds to increase is also prohibited. These types of land uses are not proposed within the Harvest Landing Specific Plan.

Additionally, the southern tip of the Harvest Landing Specific Plan area (approximately 7.6 acres) is within Zone D ("flight corridor buffer"), which does not have restrictions on land use intensities, and development considerations in this area require disclosure during real estate transactions only. The original project approval overrode the Airport Land Use Commission (ALUC) determination, permitting higher-density residential development beyond what is allowed in the C2 zone. This amendment does not propose a change to the previously approved residential densities identified in the original Harvest Landing Specific Plan.

1.8 Specific Plan EIR

The California Environmental Quality Act (CEQA) was adopted to maintain the quality of California's environment. Its purpose is to inform decision makers, staff, and the public about the potential environmental impacts of development. Compliance with CEQA requires that a project be evaluated for potential impacts before being approved. Further, the State adopted a policy "that the public agencies should not approve projects as proposed if there are feasible alternatives or feasible mitigation measures available that would substantially lessen the significant environmental effects of such projects."

Adoption of a Specific Plan is a project subject to CEQA. As such, an Environmental Impact Report (EIR) has been prepared to analyze and disclose the significant environmental impacts of the Harvest Landing Specific Plan Amendment, as well as plan alternatives and the means by which possible environmental effects may be reduced or avoided. The EIR is incorporated into this Specific Plan by reference and is attached separately. At the December 17, 2025 City of Perris Planning Commission public hearing and the February 10, 2026 City of Perris City Council public hearing, identified Alternative 4 as the preferred development option.



HARVEST LANDING SPECIFIC PLAN LAND USE PLAN

2.0 LAND USE PLAN

2.1 Introduction

This section describes the proposed development concept for Harvest Landing Specific Plan. The Harvest Landing Specific Plan outlines a thoughtfully integrated mix of land uses, fostering diverse commercial and business opportunities. It establishes a cohesive development framework characterized by a unified design, seamless connectivity, and enhanced landscaping. The Harvest Landing land use designations include Low Density Residential, Medium Density Residential, High Density Residential, Harvest Landing Sports Park, Multiple Business Use (MBU), Commercial (C), and Water Quality Management Plan/Drainage Detention (WQMP). There is one MBU overlay area, which allows MBU land uses in addition to the existing land use.

The land use plan is shown on Figure 2.0-1, *Land Use Plan*, and the build-out potential is described in Table 2.0-1, *Land Use Statistical Data*, below.

The following are the proposed land use designations and acreages in this Specific Plan:

- Low Density Residential (LDR): 18.0 acres
- Medium Density Residential (MDR): 16.4 acres
- High Density Residential (HDR): 15.4 acres
- Harvest Landing Sports Park (HLSP): 16.5 acres
- Commercial (C): 46.24 acres
- Multiple Business Use (MBU): 189.21 acres
- Water Quality Management Plan/Drainage Detention: 12.91 acres

Total Harvest Landing Specific Plan Area: 358.28 acres (includes 43.62 acres of roadways)

Buildout Potential

Table 2.0-1, *Land Use Statistical Data*, provides a detailed breakdown of the build-out levels of Harvest Landing.

Table 2.0-1, Land Use Statistical Data

Land Use	Acres	Density/FAR	Units/Building Sq. Ft.	Jobs ¹
Low Density Residential	18.00	0.5-7.0	110	N/A
Medium Density Residential	16.40	7.1-12.0	160	N/A
High Density Residential	15.40	15.1-22.0	345	N/A
Harvest Landing Sports Park	16.50	N/A	N/A	N/A
Multiple Business Use	189.21	0.75	6,181,491	8,570
Commercial	46.24	0.75	1,510,661	3,021
WQMP Drainage/Detention	12.91	N/A	N/A	N/A
Subtotal	314.66	--	7,692,152	11,591



HARVEST LANDING SPECIFIC PLAN LAND USE PLAN

Land Use	Acres	Density/FAR	Units/Building Sq. Ft.	Jobs ¹
Roads	43.62	N/A	N/A	N/A
Grand Total	358.28	--	7,692,152	11,591

1. Job Assumption: 500 square feet per employee in Commercial and 1,000 square feet per employee in Multiple Business Use area. (Source: County of Riverside General Plan).
2. For the purposes of CEQA the accompanying EIR for the HLSP Amendment assumes a reasonably foreseeable buildout of 5,735,535 SF of MBU and 428,507 SF of Commercial.

2.2 Low Density Residential (LDR)

Low Density Residential (LDR): This designation provides for low density detached single-family residential uses with densities ranging from 0.5-7.0 units per acre.

2.3 Medium Density Residential (MDR)

Medium Density Residential (MDR): This designation provides for small-lot single-family detached residential uses and medium density single-family attached dwellings (E.G. duplex, triplex, fourplex, cluster/row townhome) with densities ranging from 7.1-12.0 units per acre.

2.4 High Density Residential (HDR)

High Density Residential (HDR): This designation provides for high density multifamily residential uses (E.G. fourplex, cluster/row townhome, and stacked flats) with densities ranging from 15.1-22.0 units per acre.

2.5 Harvest Landing Sports Park (HLSP)

Harvest Landing Sports Park (HLSP): This designation provides for active and passive open space and recreation opportunities, including sports parks, athletic fields, playgrounds, picnic areas, and neighborhood parks. Also allows for the detention of storm water.



HARVEST LANDING SPECIFIC PLAN LAND USE PLAN

Figure 2.0-1 Land Use Plan





HARVEST LANDING SPECIFIC PLAN LAND USE PLAN

2.6 Multiple Business Use (MBU)

Multiple Business Use (MBU): This designation provides for a mix of professional office, business park, research and development, business park, related storage and support service uses, and pedestrian linkages. The MBU land use allows for a mix of Business/Professional Office and business park land uses, which are described below, as well as single and multi-family residential uses at the existing densities and locations as depicted in Figure 2.1 in the original approved Harvest Landing Specific Plan. Please see Appendix A for reference.

Allowed Business/Professional Office Uses Under MBU Land Use: Allowed uses are those associated with business, professional, or administrative services located in areas of high visibility from major roadways. Manufacturing is also allowed in this designation.

Allowed Business Park and Manufacturing Uses Under MBU Land Use: Business park uses encompass a broad range of manufacturing and non-manufacturing activities. Within the MBU zoning designation, business park uses are permitted, including manufacturing, research and development, materials testing, fabrication, and assembly of non-hazardous products, as well as retail activities associated with manufacturing operations. A maximum of one (1) Parcel Hub is permitted within this zone.

Public facilities are allowed within the MBU Zoning with the approval of the Community Development Director.

MBU Overlay: This overlay allows for MBU land uses in addition to the underlying land use designations within the existing lot(s).

2.7 Commercial/Retail Use (C)

Commercial (C): This land use designation allows retail, professional office, and service-oriented business activities which serve the entire City, as well as single and multi-family residential uses at the existing densities and locations as depicted in Figure 2.1 in the original approved Harvest Landing Specific Plan. Please see Appendix A for reference.

2.8 Water Quality Management Plan (WQMP) Uses

Water Quality Management Plan (WQMP): This designation provides for stormwater drainage and detention uses, including drainage swales and detention basins. This area will also serve as an outdoor amenity for employees of the Specific Plan area to utilize with features such as, but not limited to, walking paths, benches, and/or exercise equipment, but is not designated as Open Space or Park land use. Residential uses may be allowed per section 4.2.1 of this Specific Plan.



HARVEST LANDING SPECIFIC PLAN LAND USE PLAN

2.9 Permitted Uses

The allowable land uses and permit requirements are indicated in Table 2.0-2, *Allowed Land Uses*.

Permitted Uses (P) are allowed, subject to compliance with all applicable provisions of the Zoning Ordinance, and to obtaining any other permit required by the Municipal Code, including a business license. Proposed projects comprised of a permitted use are not granted immediate approval as they must undergo a review process and are subject to public hearing and final approval determined by the City.

Administrative Development Plan Review/Development Plan Review (ADPR/DPR) would be required for any new development regardless of whether the use is permitted or permitted with an approved Conditional Use Permit.

Conditional Use Permit (CUP) is required, pursuant to Chapter 19.61 of the City of Perris Zoning Ordinance, for any use that may need to be conditioned in order to operate in a manner so as not to generate impacts on the community.

Prohibited Uses (PRO) are land uses that are not allowed in the specified land use designation.

For a full description of the approval process, refer to Section 7.0, *Implementation and Administration*.

Table 2.0-2, Allowed Land Uses

Land Use	MBU	C	LDR	MDR/HDR
Commercial Uses				
Administrative and Professional Offices/Services	P	P	PRO	PRO
Adult Entertainment	PRO	PRO	PRO	PRO
Alcohol Sales for Off-site Consumption	CUP	CUP	PRO	PRO
Alcohol Sales for On-site Consumption in Restaurant	P	P	PRO	PRO
Animal Services	P	CUP	PRO	PRO
Animal Boarding/Kennel (see Title 8 of the Municipal Code)	CUP	PRO	PRO	PRO
Auditoriums, Convention Halls, and Theaters	PRO	CUP	PRO	PRO
Auto Parts Sales	P	P	PRO	PRO
Bakeries (Commercial)	P	P	PRO	PRO
Barber/Beauty/Nail Shops	P	P	PRO	PRO
Cabinet and woodworking shops	P	PRO	PRO	PRO
Cable Companies	P	PRO	PRO	PRO



HARVEST LANDING SPECIFIC PLAN LAND USE PLAN

Land Use	MBU	C	LDR	MDR/HDR
Catering Establishments	P	CUP	PRO	PRO
Car, Sales	CUP	CUP	PRO	PRO
Car Washes	CUP	CUP	PRO	PRO
Bank	PRO	P	PRO	PRO
Cleaning/Janitorial	P	PRO	PRO	PRO
Clinics including dental, medical, and optical	P	P	PRO	PRO
Convenience Stores	P	P	PRO	PRO
Copy Centers/Postal Service Centers/Blueprinting	P	P	PRO	PRO
Dance Schools/Karate Studios	P	P	PRO	PRO
Deli/Sandwich Shops/Donut Shops (less than 12 fixed seats)	P	P	PRO	PRO
Distributors and showrooms	P	P	PRO	PRO
Drive-Thru Services	CUP	CUP	PRO	PRO
Drug Stores	P	P	PRO	PRO
Dry Cleaners	P	P	PRO	PRO
Flower/Gift Shops	P	P	PRO	PRO
Fueling stations and services	CUP	CUP	PRO	PRO
Funeral Homes	PRO	PRO	PRO	PRO
General Retail / General Merchandise	P	P	PRO	PRO
Grocery Store	P	P	PRO	PRO
Health/Athletic Clubs	P	P	PRO	PRO
Home Improvements with outdoor display of lumber, garden, and nursery items	P	P	PRO	PRO
Hotels	P	P	PRO	PRO
Kennels, veterinarians, and animal services	P	P	PRO	PRO
Laboratories (e.g., film, medical, dental, R&D, etc.)	P	P	PRO	PRO
Landscape Nurseries	P	P	PRO	PRO
Laundromats (self-serve)	P	P	PRO	PRO
Laundry and dry cleaning	CUP	P	PRO	PRO
Massage Parlor	PRO	PRO	PRO	PRO
Misc. Repair/Service, indoor only (exclusive of auto repair)*	CUP	P	PRO	PRO
Nurseries/Garden Supplies	P	P	PRO	PRO
Night Clubs/Bars/Lounges	CUP	CUP	PRO	PRO
Offices	P	P	PRO	PRO
Office Equipment/Supplies	P	P	PRO	PRO
Thrift Shops	PRO	PRO	PRO	PRO



HARVEST LANDING SPECIFIC PLAN LAND USE PLAN

Land Use	MBU	C	LDR	MDR/HDR
Personal Services	P	P	PRO	PRO
Pest Control	CUP	CUP	PRO	PRO
Publishing/Printing Plants	P	PRO	PRO	PRO
Restaurants (No Drive Thru)	P	P	PRO	PRO
Restaurants (With Drive Thru)	CUP	CUP	PRO	PRO
Restaurants With Alcohol Sales	P	P	PRO	PRO
Service Stations	CUP	CUP	PRO	PRO
Smoke Shops	P	P	PRO	PRO
Specialty Food Stores (no alcohol)	P	P	PRO	PRO
Storage (Indoor Ancillary Uses)	P	P	PRO	PRO
Swap Meets (Indoor)	PRO	PRO	PRO	PRO
Swap Meets (Outdoor)	PRO	PRO	PRO	PRO
Tattoo Parlors, Body-Piercing Studios	P	P	PRO	PRO
Vehicle Leasing/Rental	CUP	CUP	PRO	PRO
Vehicle-Related Routine Service and Maintenance (minor repair)*	CUP	CUP	PRO	PRO
Veterinary Services	P	P	PRO	PRO
Video Arcades	CUP	CUP	PRO	PRO
Welding and Related Uses	P	PRO	PRO	PRO
Wholesale Business	CUP	P	PRO	PRO
Residential				
Residential (Multi-Family)	P	P	P	P
Residential (Single-Family)	P	P	P	PRO
Communication Towers (Additional FAA review may be required)				
Stealth Monopoles/Wireless Communication Facilities	CUP	CUP	PRO	PRO
Educational and Care Facilities				
Blood Banks	PRO	PRO	PRO	PRO
Child Care Center / Nursery School, Private	CUP	CUP	PRO	PRO
Day Care for Employee Children Only*	P	P	PRO	PRO
Health Clinics/Outpatient Surgery	CUP	CUP	PRO	PRO
Hospitals Urgent Care Centers	CUP	CUP	PRO	PRO
Medical Care Clinics and Offices	P	P	PRO	PRO
Medical Equipment and Supplies	P	P	PRO	PRO
Outpatient Treatment Programs	P	P	PRO	PRO
Senior/Congregate Care Facilities	P	CUP	PRO	PRO
Schools, Technical, Trade and Vocational	CUP	CUP	PRO	PRO



HARVEST LANDING SPECIFIC PLAN LAND USE PLAN

Land Use	MBU	C	LDR	MDR/HDR
Business Park and Manufacturing Uses				
Manufacturing (Indoor)	CUP	PRO	PRO	PRO
Recycling Facilities	PRO	PRO	PRO	PRO
Recycling Facilities, reverse vending only	PRO	PRO	PRO	PRO
Truck Storage Outdoor	PRO	PRO	PRO	PRO
Parcel Hub**	P	PRO	PRO	PRO
Business Park/Office	P	PRO	PRO	PRO
Warehouse and Distribution Centers	PRO	PRO	PRO	PRO
Recreation Uses				
Recreational Areas and Facilities (Outdoor)	CUP	CUP	PRO	PRO
Recreational Areas and Facilities (Indoor)	CUP	CUP	PRO	PRO
Storage Uses				
Mini-storage/Wholesale Facilities	CUP	PRO	PRO	PRO
Non-Profits				
Government Facilities	P	P	PRO	PRO
Public/Semi-Public Institutions	P	P	PRO	PRO
Public Infrastructure Facilities	P	PRO	PRO	PRO
Public or Semi-Public Education Facilities	CUP	CUP	PRO	PRO
Religious Institutions	CUP	CUP	PRO	PRO
Professional Office				
Business/Professional Office	P	P	PRO	PRO
Other Uses				
Drainage/Detention Basins	P	P	PRO	PRO
Libraries	CUP	P	PRO	PRO
Museums	CUP	P	PRO	PRO
Radio/TV Broadcasting	CUP	PRO	PRO	PRO
Private/Public Utility Facilities	P	CUP	PRO	PRO
Transportation	P	PRO	PRO	PRO

* Permitted by right when incidental and accessory to a primary permitted use; a Conditional Use Permit shall only be required when proposed as a standalone use.

** A maximum of one (1) Parcel Hub is permitted within the Harvest Landing Specific Plan.

LAND USE TABLE NOTES:

The interpretation or inclusion of any uses not specifically listed in this table shall be subject to the discretion of the Director of Development Services and/or Planning Commission upon appeal using this Specific Plan as a guide. See Section 6, Implementation, for the accommodation of minor amendments to this table.



HARVEST LANDING SPECIFIC PLAN LAND USE PLAN

2.10 Definitions

Specific uses that are not defined in the section please refer to the City of Perris' Municipal code, Chapter 19.08.

Adult Entertainment: Any establishment providing adult entertainment as defined by City of Perris Municipal Code, Title 5, Chapter 5.50 including, but not limited to, adult arcade, adult bookstore, adult novelty store, adult video store, adult motion picture theater, and exotic dance studio.

Alcohol Sales for Off-site Consumption: The act of selling any type of alcohol for off-site consumption such as convenience stores, service stations with alcohol related sales. Refer to City of Perris Zoning Ordinance, Chapter 19.65.

Alcohol Sales for On-site Consumption: The act of selling any type of alcohol for on-site consumption such as restaurants serving alcoholic beverages, bars and cocktail lounges. Refer to City of Perris Zoning Ordinance, Chapter 19.65.

Animal Services: As defined by Municipal Code Section 8.01.010 Definitions, *mean the city-funded program that provides staffing, facilities and resources to operate the city animal control and animal shelter operations.*

Business/Professional Office: A place of business where professional or clerical duties are performed such as business support services, legal services, collection agencies, development services office and property management services.

Child Care Center/Nursery School Private: Facilities for the temporary care of children on a regular, recurring basis for pay or other valuable consideration as outlined in City of Perris Zoning Ordinance, Chapter 19.83, such as childcare facilities, private daycare and private nursery schools.

Drive-Thru Services: A type of service provided by a business that allows customers to purchase products without leaving their cars such as banks, pharmacies, and restaurants.

Food and Food Service (no alcohol): Establishments that serve and prepare food as the primary function without the sale of alcoholic beverages including coffee shops, delicatessens, bakeries, candy, ice cream and yogurt shops.

Funeral Homes: A funeral home or parlor is a business that provides burial and funeral services and merchandise such as caskets for the deceased and their families.

General Retail: The sale of goods or merchandise from a fixed location, including, but not limited to, a department store, boutique or kiosk such as pet and pet supply, book stores, craft stores, department stores, discount stores, drug store/pharmacies, florists, grocery stores, jewelry stores,



HARVEST LANDING SPECIFIC PLAN LAND USE PLAN

furniture sales, garden and farm supplies, office equipment sales, apparel stores, appliance stores, gift and card stores.

Government Facilities: Establishments owned by the system from which they are regulated over including, but not limited to, Municipal, County, State or Federal governmental administrative offices and facilities, libraries, courthouses, fire stations, and fleet or maintenance yards.

Hospitals and Urgent Care Centers: Any building or portion thereof, used for the treatment or accommodation of injured or ill persons, includes convalescent and rest homes. It shall not include asylums, detention or similar buildings where human beings are housed or detained under legal restraint.

Hotels: Buildings designed for or occupied by the temporary lodging of individuals in which there are 6 or more guest rooms for which there is no provision for cooking in any individual room or suite. Said use may also contain such ancillary facilities as conference facilities, personal services or food preparation and dispensing.

Laboratories: Facilities used for scientific, technical, medical, dental, or research and development (R&D) purposes, including testing, analysis, and experimentation. This may include medical and dental labs, film processing labs, and R&D facilities supporting various industries. Laboratories may involve the use of specialized equipment, controlled environments, and chemical or biological materials, subject to applicable safety and regulatory standards.

Landscape Nurseries: Reproduction and growing of plants to usable size for retail or wholesale.

Manufacturing/Business Park: The fabrication or storage of goods and services for sale including, but not limited to, cabinet and woodworking shops, distributors and showrooms, food products, manufacturing, light manufacturing, Business Park uses, research and development, research centers and wholesale (with on-site merchandise).

Manufacturing: Pharmaceutical, Hazardous Materials, Chemicals: The fabrication or storage of goods and services for sale including, but not limited to, pharmaceuticals, hazardous materials, or chemicals as allowed in the above land use table.

Medical Care Clinics and Offices: A facility, office or clinic used for the provision of health, prevention of illness and treatment of illness or injury under the care of a physician including, but not limited to, chiropractic, dental, vision, acupuncture and orthodontic offices, excluding urgent care facilities, hospitals and clinics requiring a state permit.

Monopole/Wireless Communication Facilities: Radio antenna or structure situated on legal lot that is the local point of interface between a wireless phone device and a wireless network consisting



HARVEST LANDING SPECIFIC PLAN LAND USE PLAN

of a support structure including, but not limited to, a tower, pole or stealth structure (monopine, monopalm, water tower, etc.) and accessory equipment such as antenna array, microwave dishes, GPS antenna, equipment shelter and cabinet that incorporates stealth design. (Refer to City of Perris Zoning Ordinance, Chapter 19.85)

Mortuary: A mortuary is a building or room used for the storage of human cadavers awaiting identification or removal for autopsy, burial or cremation.

Parcel Hub: A parcel hub typically serves as a regional and local freight-forwarding facility for time-sensitive shipments transported by air and ground carriers, such as FedEx, UPS, and USPS. Packages are received, immediately sorted, and dispatched on vehicles for direct delivery. From the parcel hub, items are routed to delivery routes or carriers responsible for the final segment of the journey from the hub to the recipient. Unlike a warehouse, a parcel hub is designed for rapid movement and short on-site handling periods, with minimal or no long-term storage. No warehouse storage or distribution activities occur on site; however, limited assembly and repackaging may take place.

Personal Services: A business whose principal activity may include weight loss centers, nail salons, barber shops, health clubs, spas, studios for art, exercise, dance and similar services.

Pest Control: Business or facilities that provide services to control the spread of pests including, but not limited to, termites, rodents and insects.

Public/Semi-Public Institutions: An institution that is the responsibility of a governmental unit or over which a governmental unit exercises administrative control including, but not limited to, city hall, government offices, community centers or a facility having some features of a public institution such a fraternal lodge or utility.

Public or Semi-Public Educational Facilities: An educational organization that public officials (elected, appointed, or both) operate and that public funds support including, but not limited to, schools and administrative offices.

Public Infrastructure Facilities: The basic facilities, services, and installations needed for the functioning of a community including, but not limited to, transportation and communications systems, public utilities, detention basin and drainage facilities. Infrastructure necessary for essential public services, including water, wastewater, electricity, gas, and telecommunications. This includes EMWD facilities such as water treatment facilities, water wells, pumping stations, storage tanks, and conveyance systems. These facilities may include associated buildings, pipelines, and equipment necessary for operation and service reliability.

Recreational Areas and Facilities (Indoor): Amusement or entertainment provided in an enclosed space designed to provide pleasure or relaxation including, but not limited to, billiard halls, amusement centers, social/fraternal organizations, indoor commercial recreation, restaurants



HARVEST LANDING SPECIFIC PLAN LAND USE PLAN

with entertainment (exclusive of adult entertainment), bowling alleys, movie theaters, climbing walls, batting cages, go-cart racing, billiards, arcades and laser tag.

Recreational Areas and Facilities (Outdoor): Amusement or entertainment provided in any open space area designed to provide pleasure or relaxation including, but not limited to, outdoor commercial recreation, paintball facilities, public parks, trails and community centers.

Religious Institutions: An establishment, organization or association instituted to advance or promote religious purposes or beliefs including, but not limited to, churches, Sunday school, mosques, temples, synagogues including recreational facilities and residential quarters for incidental use. Does not include private schools or daycare.

Residential: Dwelling units for individuals or households, including single-family homes, multi-family apartments or condominiums, townhouses, duplexes, and other types of housing. This does not include transient lodging (e.g. hotels or motels), institutional housing, or hotels or other short-term housing. Please see Section 4.2.1 for permitted residential density.

Schools, Technical and Trade: An educational institution designed to allow and encourage students to learn under the supervision of teaching instructors including vocational facilities that train students in a variety of skills needed to perform a certain job or career such as private, technical or trade schools.

Storage Uses (Ancillary): Facilities directly associated with and incidental to the primary use occupying less than 10% of the site or floor area used for the keeping of materials or products in an open, uncovered yard or in an unwallled building.

Vehicle - Related Outdoor Storage and Other Facilities: Facility used to store vehicles including, but not limited to, towing yards, vehicle auctions and establishments where major body repair and painting occurs, excluding outdoor dismantling and salvage yards.

Vehicle - Related Routine Service and Maintenance: Facilities that provide routine vehicle-related services and maintenance for minor vehicle repairs including, but not limited to, incidental body or fender work, painting, upholstery, oil changes, engine tune-up, adjusting lights or brakes, or supplying and installing replacement parts of or for passenger vehicles and trucks. Also includes any building or lot having pumps and storage tanks where fuels, oils, or accessories for motor vehicles are dispensed, sold, or offered for sale at retail only as well as car washing facilities.

Wholesale Facilities: An establishment where the sale of goods in large quantities, as for resale by a retailer, takes place. Refer to City of Perris Zoning Ordinance, Chapter 19.08, 19.44.090.A.



HARVEST LANDING SPECIFIC PLAN CIRCULATION AND INFRASTRUCTURE

3.0 CIRCULATION AND INFRASTRUCTURE PLAN

3.1 Introduction

The Harvest Landing Specific Plan will transform a mostly undeveloped area into a commercial, business park, and business center with convenient access to major transportation routes. Presently, the area lacks sufficient infrastructure to effectively support this type of project development, and existing infrastructure is generally limited to supporting the limited uses on and around the area. Infrastructure improvements will be required to address the service needs of the Specific Plan area.

This Circulation and Infrastructure Plan shall serve as a guide for the development of more detailed plans for circulation, domestic water distribution, wastewater collection, stormwater collection, and other utilities that are deemed necessary as development occurs within this Specific Plan area. The conceptual Infrastructure Plans provide a schematic approach to identify the route and location of public facilities within the Specific Plan area. Future development proposals will determine the exact size and location of facilities; however, the design of the infrastructure improvements shall closely adhere to the facilities depicted in the Circulation and Infrastructure Plan.

3.2 Circulation

The Circulation Plan provides Standards and Guidelines intended to ensure the safe and efficient movement of people and goods within the Harvest Landing Specific Plan area meet future transportation needs city-wide. The Circulation Plan addresses several aspects of circulation throughout the Harvest Landing Specific Plan including vehicle, truck and transit circulation, and non-vehicular circulation (pedestrian and bike). The Circulation Plan is depicted in Figure 3.0-1, *Circulation Plan*.



HARVEST LANDING SPECIFIC PLAN CIRCULATION AND INFRASTRUCTURE

Figure 3.0-1, Circulation Plan



Harvest Landing
City of Perris



HARVEST LANDING SPECIFIC PLAN CIRCULATION AND INFRASTRUCTURE

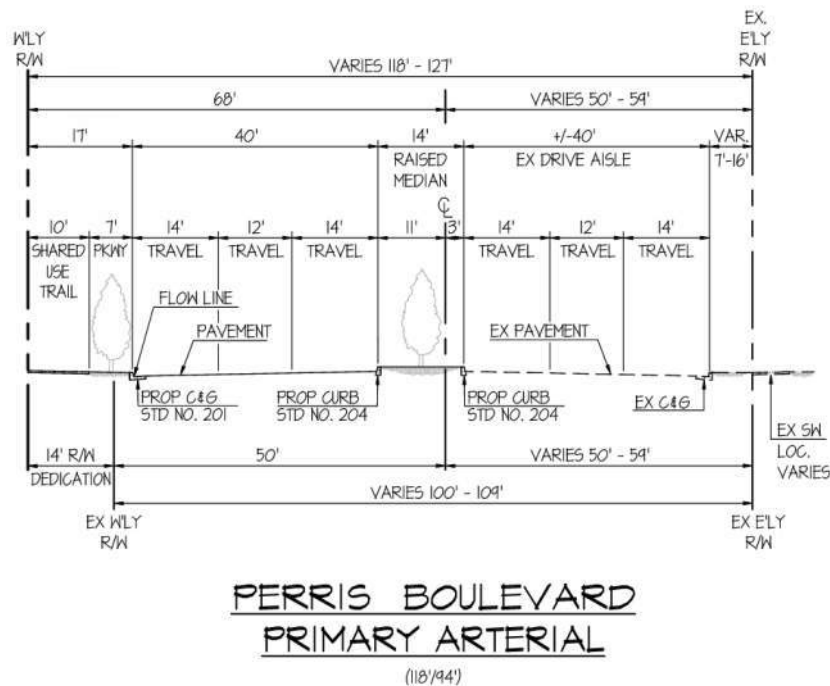
3.2.1 Vehicular Roadway Network

Harvest Landing is well positioned to take advantage of existing regional transportation corridors. I-215 runs along the western edge of the project, providing access to Riverside and other areas within southern California. Regional north-south access to the Harvest Landing Specific Plan area is provided via Interstate-215, Perris Boulevard, and Indian Avenue. Two existing interchanges located just south of the Specific Plan Project site at Nuevo Road and north of the Specific Plan area at Placentia Avenue provide direct access from the Project to the freeway.

The Harvest Landing Specific Plan includes a vehicular circulation plan that fits into and complements the existing surrounding roadway system. Like the City's General Plan Circulation Element, the Project's roadway system consists of a hierarchy of streets including primary arterials, secondary arterials, collectors, and private streets.

All streets within Harvest Landing meet or exceed the City's General Plan future right-of-way standards for each roadway designation. Typical street sections are found on Figures 3.0-2a – g, *Standard Street Sections*, and described below.

Figure 3.0-2a, Standard Street Sections – Primary Arterial (Perris Boulevard)





HARVEST LANDING SPECIFIC PLAN CIRCULATION AND INFRASTRUCTURE

Figure 3.0-2b, Standard Street Sections – Secondary Arterial (Orange Avenue)

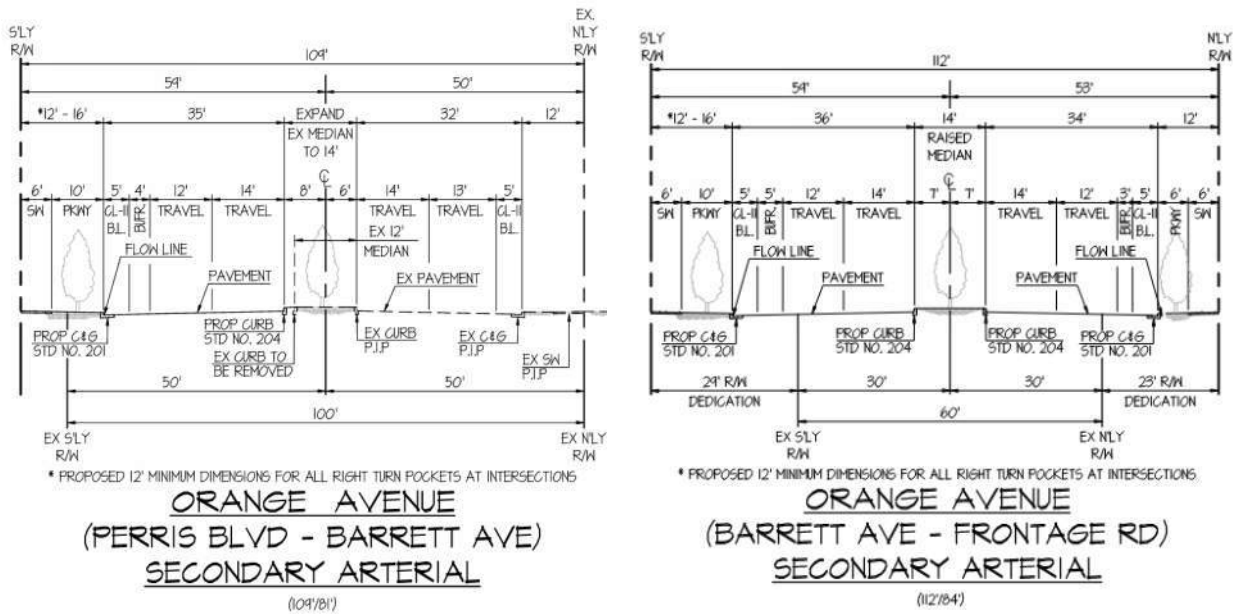
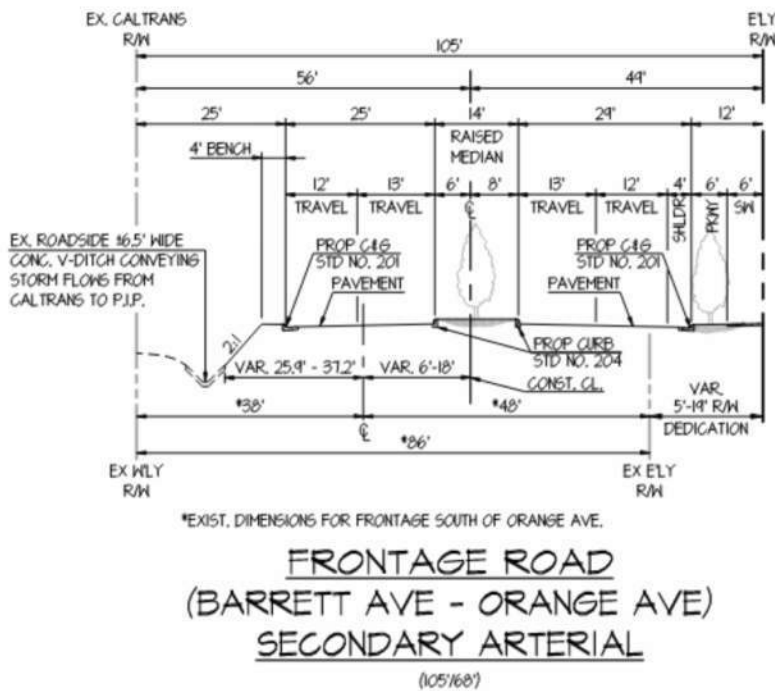


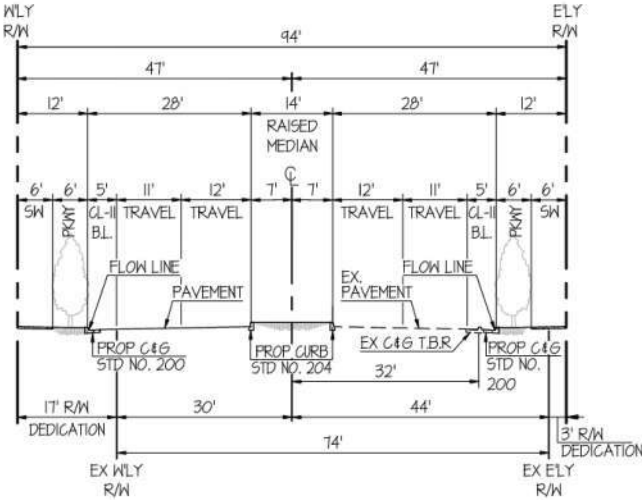
Figure 3.0-2c, Standard Street Sections – Secondary Arterial (Frontage Road)





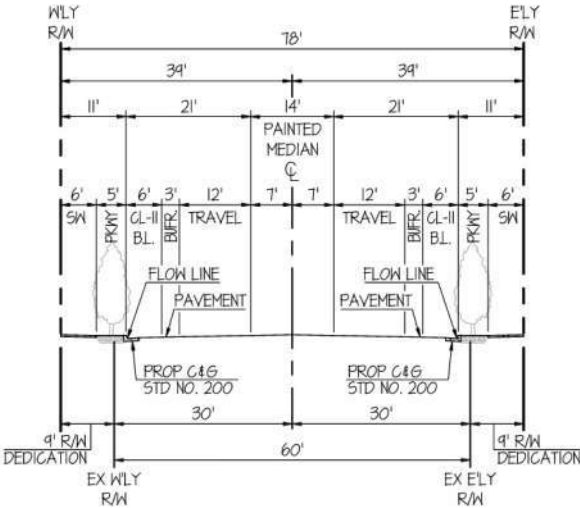
HARVEST LANDING SPECIFIC PLAN CIRCULATION AND INFRASTRUCTURE

Figure 3.0-2d, Standard Street Sections – Secondary Arterial (Indian Avenue)



INDIAN AVE
 (ORANGE - VAL VERDE SCHOOL)
SECONDARY ARTERIAL
 (94'/10')

Figure 3.0-2e, Standard Street Sections – Major Collector (Barrett Avenue)



BARRETT AVENUE
 (N'LY WALMART DWY - ORANGE AVE)
MAJOR COLLECTOR
 (78'/56')



HARVEST LANDING SPECIFIC PLAN CIRCULATION AND INFRASTRUCTURE

Figure 3.0-2f, Standard Street Sections – Modified Collector (Harvest Landing Way)

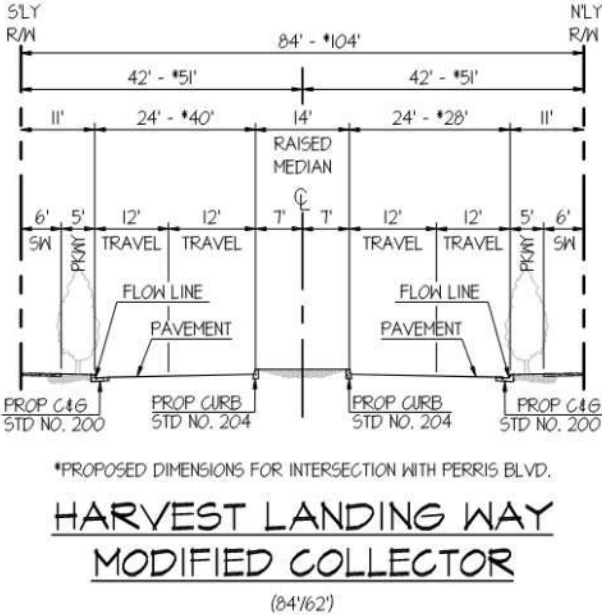
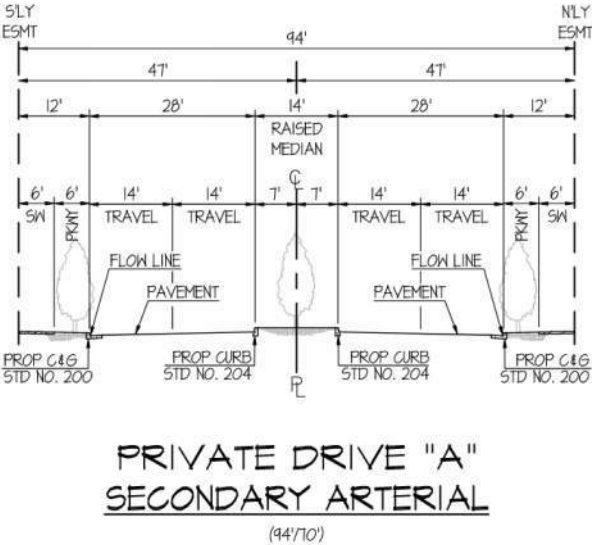


Figure 3.0-2g, Standard Street Sections – Private (Private Drive A)





HARVEST LANDING SPECIFIC PLAN CIRCULATION AND INFRASTRUCTURE

Freeway

Interstate-215: Interstate-215 (I-215) runs north-south along the western boundary of Harvest Landing. Existing freeway on and off-ramps are located at Nuevo Road and Placentia Avenue.

Primary Arterials

Primary arterials serve major traffic movements or major traffic corridors within a 118-foot to 127-foot right-of-way. While they may provide access to abutting land, their primary function is to serve traffic moving through the area. Primary arterial streets generally have a curb-to-curb width of 90-feet to 94-feet.

Primary arterial streets serving the Harvest Landing Specific Plan area include:

- **W Placentia Avenue:** Placentia Avenue runs east-west along the northernmost boundary of the site. An interchange at Placentia Avenue and I-215 has provided improved access from the northern portion of the Specific Plan area to surrounding areas and I-215.
- **N Perris Boulevard:** Perris Boulevard serves as a main corridor within the City. It runs north-south along the eastern edge of the Project, providing direct access between the Specific Plan area and the downtown area.

Secondary Arterials

A secondary arterial is intended to carry local traffic between the local street system and the primary arterial system. Secondary arterial streets generally vary from a curb-to-curb width of 81 feet to 86 feet and may have one or two lanes in each direction with a 12-foot to 14-foot raised median. Median requirements of section 19.70.050B shall also include Orange Avenue and Indian Avenue. (See Figure 3.0-1, *Circulation Plan*)

Secondary arterial streets serving the Harvest Landing Specific Plan area include:

- **Orange Avenue:** Orange Avenue is an east-west roadway which traverses through the center of Harvest Landing. It will act as the primary east-west corridor in Harvest Landing.
- **Indian Avenue:** Indian Avenue is a north-south roadway on the western side of Harvest Landing. It will serve as a direct link from Placentia to Orange Avenue.
- **Frontage Road:** Frontage Road is a north-south roadway on the most western edge of Harvest Landing. It serves as a main north-south corridor adjacent for Harvest Landing.

Collectors

A major collector road is a low- or moderate-capacity road that tends to lead traffic from local roads or sections of neighborhoods to activity areas within communities, arterial roads, or occasionally directly to expressways or freeways within a 66-foot to 84-foot right of way within the Harvest Landing Specific Plan. Collector streets typically range from 44 feet to 62 feet wide curb



HARVEST LANDING SPECIFIC PLAN CIRCULATION AND INFRASTRUCTURE

to curb with 6 feet of sidewalk on both sides depending on the design and traffic volumes to be served.

Major collectors in Harvest Landing include:

- **Barrett Avenue:** Barrett Avenue is a public, local north-south roadway throughout Harvest Landing intersecting with Nuevo Road at the south and Placentia Avenue to the north.
- **Harvest Landing Way:** Harvest Landing Way is a modified collector with a short east-west roadway connecting Barrett Avenue and Perris Boulevard. Harvest Landing Way is tapered at the intersection of Perris Boulevard to allow for turning in and out of the commercial center.

Private Drives

Harvest Landing has one private drive servicing the site:

- **Private Drive "A":** Private Drive "A" will be a short east-west roadway connecting MBU parcel to Frontage Road to allow for trucks to enter and exit the site directly onto Frontage Road and avoid entry onto Orange or Barrett Avenues. The Private Drive will have a right-of-way of 94 feet and an 70-foot width curb to curb.

Roadway Development Standards

The circulation plan of Harvest Landing establishes the general alignments and right-of-way sections to safely meet the transportation needs of its businesses and visitors. The improvements required for development of individual projects along segments of roadways will be confirmed at the development stage.

Roadway Design Requirements

All intersection spacing and/or access openings shall be in compliance with Table 3.0-1, *Roadway Design Requirements and Intersection Spacing*, below, or as otherwise approved by the City Engineer.

Cross-Sections

All Specific Plan roads shall be constructed per the standard cross-sections shown in Figures 3.0-2a – g, above.

Lane Requirements/Expanded Intersections

All Specific Plan roads shall be constructed per the lane requirements outlined in Table 3.0-2, *Lane Requirements*, below and provide expanded intersections as depicted in Figures 3.0-3a to 3.0-3b.

No Textured Pavement Within City Right-of-Way

Textured pavement accents shall not be permitted within the City maintained rights-of-way, unless otherwise approved by the City Engineer.



HARVEST LANDING SPECIFIC PLAN CIRCULATION AND INFRASTRUCTURE

Gateway Entries

The gateway entries as shown in Figure 3.0-4, have been strategically located at the entries of Harvest Landing at key intersections. The design for these gateways shall include a consistent application of elements, all within the street rights-of-way, such as landscaping, signage on one or both sides of the street, walls/fencing and lighting within the designated monumentation area as depicted on Figure 3.0-4. For signage, please see Section 6.

Nuisance Storm Flows

Roadway intersections shall be free of nuisance water by providing storm drains for nuisance flows.

Table 3.0-1, Roadway Design Requirements and Intersection Spacing

		Road Type				
		Private Drive	Collector	Major Collector	Secondary Arterial (Raised Median)	Primary Arterial
Traffic Index		5.5	7.0	8.0	9.5	10.0
Right-of-Way (b)		112' ¹	66' ³	78'	94'	1278'
Curb to Curb		68' ²	44' ³	56'	70'	94'
Minimum Radii (d) (Horizontal)	Predominantly Flat					
Minimum Grade (%) (d)	Predominantly Flat					
Preferred Design Speed	Predominantly Flat	30	35	40	50	60
Intersection Intervals (c) (D)		200'	330' (a)	330' (a)	660' (a)	1200' (a)

- a) Commercial/Business park driveway access as determined by City Engineer.
- b) Additional right-of-way required at intersections to accommodate dual left turns. Additional right-of-way may be required on opposite side of intersection to align through lanes.
- c) All intersections and approaches along designated truck routes shall be concrete section for a minimum of 150 feet on either side of centerline.
- d) Intersections do not include driveways, driveways spacing is provided in Table 3.0-3 below.
 - ¹ Harvest Landing Way tapers from 84 to 102 feet to support the intersection of Harvest Landing Way and Perris Blvd.
 - ² Harvest Landing Way tapers from 62-foot curb to curb to 82 feet to support the intersection of Harvest Landing Way and Perris Blvd.
 - ³ Per prior city standards.

Table 3.0-2, Lane Requirements

Classification	Number of Through Lanes Along Segment	Number of Intersection Turn Lanes Required for Intersection with Secondary Arterial and Greater	
		Left	Right
Primary Arterial*	3	2 or 1**	1
Secondary Arterial	2	2 or 1**	1
Collector	1	1	1

*Perris Blvd and Placentia are the only primary Arterial. These streets only boarder the Specific Plan.

**Dependent on traffic study.



HARVEST LANDING SPECIFIC PLAN CIRCULATION AND INFRASTRUCTURE

Figure 3.0-3a, Expanded Intersection – Arterial

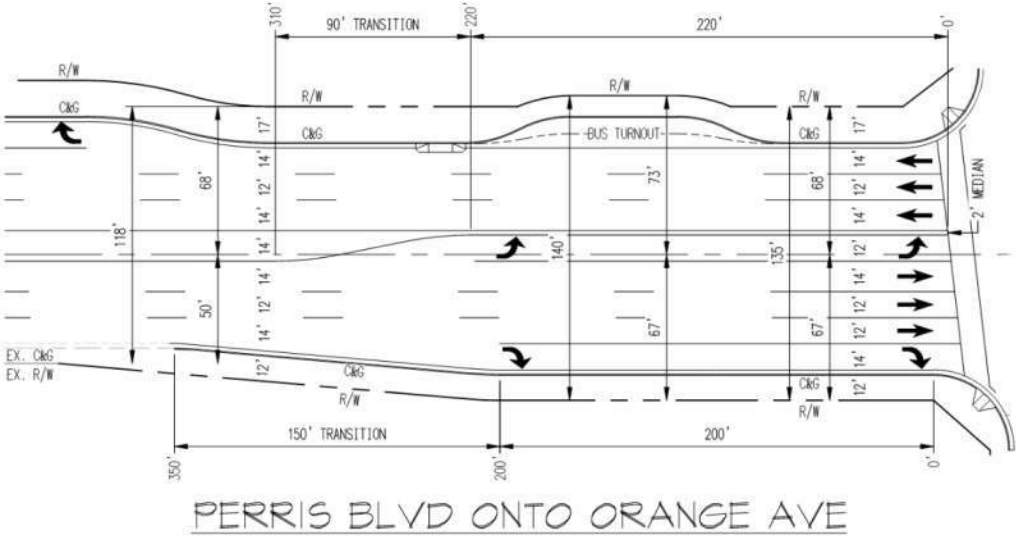


Figure 3.0-3b, Expanded Intersection – Secondary Arterial (Raised Median)

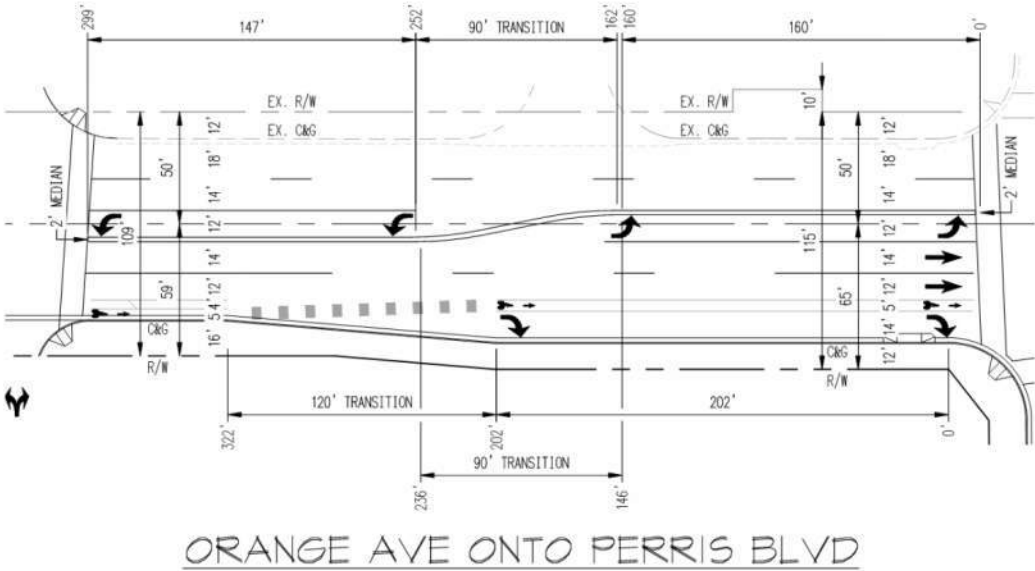




Figure 3.0-4, Gateway Entries





HARVEST LANDING SPECIFIC PLAN CIRCULATION AND INFRASTRUCTURE

Entrances and Driveway Spacing

Table 3.0-3, *Driveway Spacing*, lists appropriate driveway spacing based on roadway type within Harvest Landing.

Table 3.0-3, Driveway Spacing

	Road Type					
	Private	Collector	Major Collector	Secondary Arterial (Painted Median)	Secondary Arterial (Raised Median)	Primary Arterial
Intersection Intervals (feet)	200'	200'	200'	200'	200'	300'

Site access shall promote safety, efficiency, convenience, and minimize conflict between employee/customer vehicles and large trucks by creating separate access points as shown in Figure 3.0-5.

Figure 3.0-5, Separated Driveways





HARVEST LANDING SPECIFIC PLAN CIRCULATION AND INFRASTRUCTURE

Emergency Vehicle Access

Design of primary drive aisles shall allow for emergency vehicle access. Typically, this requirement is a minimum of 20 feet. However, applicants are encouraged to check with the City's Fire Marshall.

Adequate Vehicle Spacing For Drive-Thrus

In addition to the conditional use requirements set forth in Section 19.38.030 of the City Municipal Code, the Harvest Landing Specific Plan requires Businesses with drive-thru service(s) to provide adequate stacking to accommodate sufficient vehicles per the anticipated use prior to each pick-up window to avoid conflict with on-site circulation.

Avoid Back-up onto Public Streets

To avoid back-up onto public streets, entry drive approaches shall avoid conflict points such as parking stalls, internal drive aisles, or pedestrian crossings. Final determination of the driveway approach length shall be determined by the Planning Manager and the City Engineer.

Consideration of Large Truck Maneuverability

The design and location of loading facilities shall take into consideration the specific dimensions required for the maneuvering of large trucks and trailers into and out of loading positions at docks or in stalls and driveways.

3.2.2 Truck Routes

The Harvest Landing Specific Plan area accommodates commercial, business, and business park uses and requires a greater need for established truck routes to serve future businesses. Existing truck routes that have been designated by the City include I-215 and Placentia Avenue.

Inbound trucks to Harvest Landing will exit I-215 east onto Placentia Avenue, then utilize Frontage Road to access the MBU land uses in the western portions of Harvest Landing and return to Placentia Avenue to exit Harvest Landing.

To access Commercial land uses on the eastern portions of Harvest Landing, trucks will use Orange Avenue and enter Harvest Landing south from Orange Avenue to access retail buildings, exiting on Harvest Landing Way and continuing east to North Perris Boulevard to return to Orange Avenue to exit Harvest Landing along Frontage Road. Trucks exiting the site to Frontage Road will be limited to right-turns only onto Frontage Road.

The truck routes in Harvest Landing are shown on Figure 3.0-6, *Truck Route Plan*.

Truck Route Development Standards

Routes in which large trucks will travel shall be established in order to improve the flow of traffic through the City. I-215 and Placentia Avenue are designated as City truck routes. Inbound trucks to Harvest Landing will exit I-215 east onto Placentia Avenue, then utilize Frontage Road to access



HARVEST LANDING SPECIFIC PLAN CIRCULATION AND INFRASTRUCTURE

the MBU land uses in the western portions of Harvest Landing and return to Placentia Avenue to exit Harvest Landing. Trucks exiting the Specific Plan area onto Frontage Road will be limited to right-turns only.

Delivery Truck access to the Commercial land uses on the eastern portions of Harvest Landing, shall use Orange Avenue and Barrett Avenues access retail buildings. Delivery trucks servicing the retail uses will exit the Commercial Land use from Orange Avenue and turn southbound onto Perris Boulevard and directly right into Harvest Landing, followed with a right turn onto Harvest Landing Way to Barrett Avenue and exiting Harvest landing via Orange Avenue.

Large Turning Radius

A 50-foot turning radius shall be provided at intersections along truck routes. Entrances used primarily or solely by dual axle vehicles shall provide a minimum 50-foot radius curb returns. A minimum 40-foot turning radius shall be required for driveways with 40 feet being the preferred driveway turning radius. Excluding the commercial delivery truck route.

Concrete Intersections and Approaches

All intersections and approaches shall be paved the full width of the street with concrete for a minimum distance of 150 feet on either side of the curb return.

Increased Stacking

Typical stacking distance at turn pockets is 150 feet. Increased stacking distance in turn pockets along the truck routes shall be provided as deemed necessary by the City and City Engineer.

Acceleration/Deceleration Lanes

Acceleration, deceleration, as well as right turn lanes shall be required where deemed necessary by the City and/or City Engineer to prevent traffic congestion at entrances and exits.

Mitigation Measures

Each development project shall comply with the on-site and off-site street improvement recommendations and mitigation measures outlined in the subsequent traffic studies for each individual project, or as otherwise interpreted by the City Engineer.

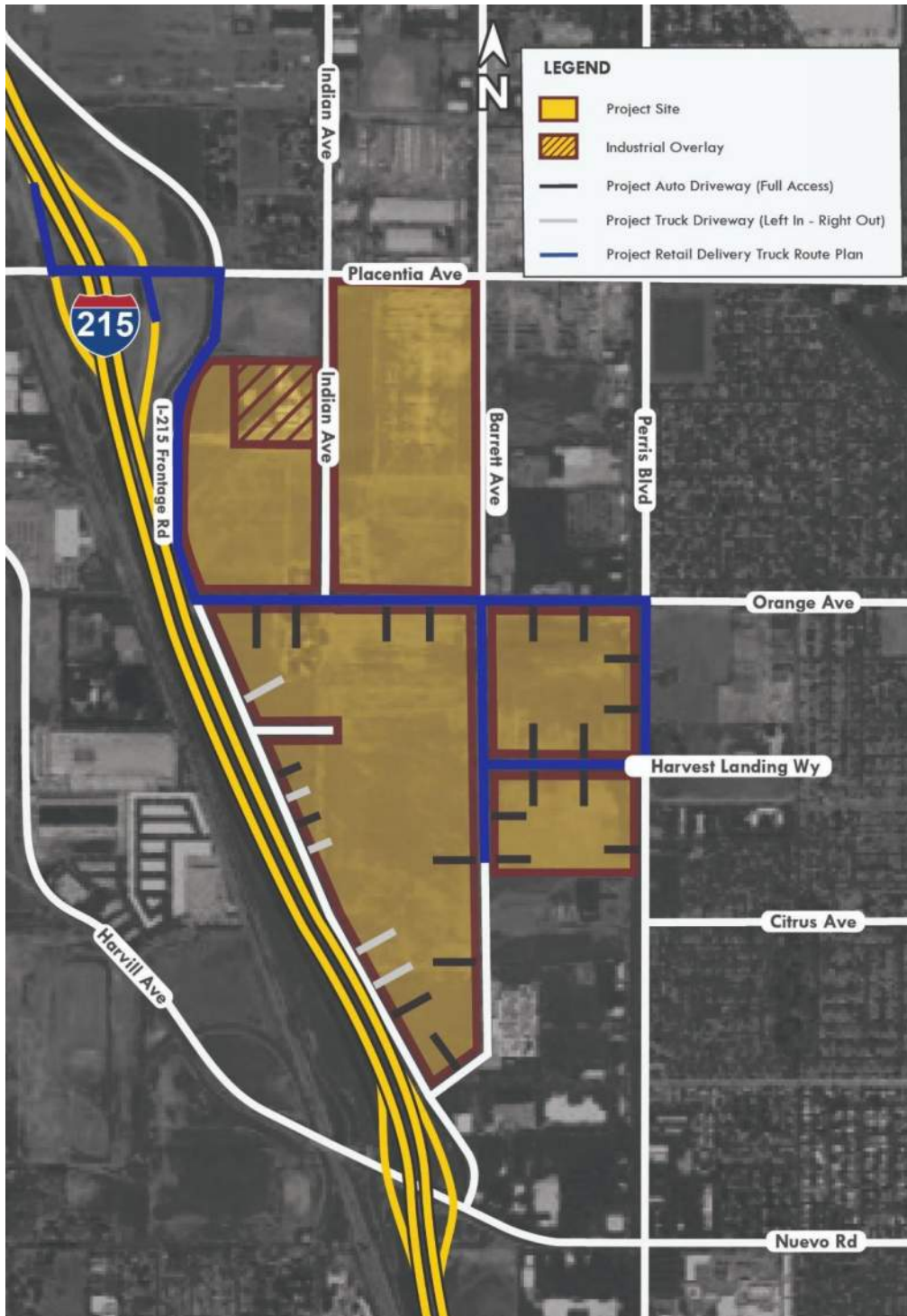


Figure 3.0-6, Truck Route Plan





Figure 3.0-7, Retail Delivery Truck Route Plan





HARVEST LANDING SPECIFIC PLAN CIRCULATION AND INFRASTRUCTURE

3.2.3 Public Transit

Currently, two primary Riverside Transportation Agency (RTA) bus lines provide service to the Harvest Landing Specific Plan area. These are Routes 19 and 30, which run north/south along Perris Boulevard, which is the eastern boundary of the Specific Plan area. Route 19 provides service from the Moreno Valley Station in the City of Moreno Valley to the Perris Station Transit Center at “C” Street and Perris Boulevard at its south terminus. Route 30 extends from the Perris Transit Station Center to/from May Ranch, near the Perris Boulevard/Ramona Expressway intersection. Bus stops are provided on Perris Boulevard along the Harvest Landing Specific Plan area boundary near the intersection of Perris Boulevard and Harvest Landing Way and the intersection of Perris Boulevard and Orange Avenue.

Bus Route Development Standards

Bus Stops at Commercial Centers

Harvest Landing Retail Center shall provide two turnout pockets for bus stops along North Perris Boulevard.

Bus Route Design Guidelines

Projects Along Identified Routes

Projects located along existing and/or future bus routes should coordinate with the Riverside Transit Authority (RTA) early in the process to determine transit requirements such as location, bus turnouts and seating and shelters.. Refer to RTA’s Design Guidelines for Bus Transit.

Additional Public Right-of-Way

Additional public right-of-way may be required, as determined by RTA, to accommodate the bus turnout and the minimum sidewalk requirement.

Bus stops should be designed to allow convenient access by transit, which includes a covered shelter, trash receptacle, and safety lighting in accordance with the City’s selected standard for the area.

3.2.4 Bicycle Circulation

The pedestrian and bicycle circulation system, shown in Figure 3.0-8, *Bicycle and Pedestrian Circulation Plan*, joins the northernmost Multiple Business Use area with the southern Multiple Business Area and the eastern commercial area.

The City’s climate makes bicycling a viable year-round recreational opportunity and alternative mode of transportation. Bicycle circulation in Harvest Landing occurs with a mix of Class II bike lanes and shared use trails as follows:

- 5-foot-wide Class II bike lanes along both sides of Indian Avenue (north of Orange Avenue).
- 10-foot-wide shared use trail along the east side of Frontage Road.



HARVEST LANDING SPECIFIC PLAN CIRCULATION AND INFRASTRUCTURE

- 10-foot-wide shared use trail along the west side of Perris Boulevard.
- 5-foot-wide Class II bike lanes along both sides of Orange Avenue (west of Perris Boulevard and west of Barrett Avenue).
- 6-foot-wide Class II bike lane along both sides of Barrett Avenue from Frontage Road to Placentia Avenue.

Bicycle Route Development Standards

The City of Perris bike trail design standards are based on Caltrans Highway Design Manual, Bikeway Planning and Design Standards. The minimum Class II bike lane width shall be 5 feet. Bike lane development within Harvest Landing will utilize design criteria found in the Perris General Plan Circulation Element.

3.2.5 Pedestrian Circulation

Pedestrian sidewalks (6 feet wide) are adjacent to all roadways to allow pedestrian access throughout Harvest Landing. Additionally, walking paths are located within the perimeter of the WQMP Detention/Drainage area to provide an outdoor amenity for employees of the area to utilize and take advantage of green space.

Primary walkways shall be 5 feet wide at a minimum and conform to ADA/Title 24 standards for surfacing, slope, and other requirements. A minimum 5-foot wide sidewalk or pathway, at or near the primary drive aisle, shall be provided as a connecting pedestrian link from the public street to the building(s), as well as to systems of mass transit, and other on-site building(s).

Pedestrian Connectivity Design Guidelines

Pedestrian connections should be created within the multiple business use area, residential, sports park and the commercial area. These connections should tie into the walkways to create a cohesive pedestrian and circulation system within the Project.

The pedestrian linkage promotes safe nonvehicular movement throughout the entire Project and between the Project and adjacent uses.



Figure 3.0-8, Bicycle Circulation Plan





HARVEST LANDING SPECIFIC PLAN CIRCULATION AND INFRASTRUCTURE

3.3 Utilities

3.3.1 Water System

The water plan for Harvest Landing is based on the availability of water and capacity of water systems surrounding the Project as well as the maximum build-out of the Project. The Harvest Landing Project site is within the service area of the Eastern Municipal Water District (EMWD) for water supply.

Water Supply

The Harvest Landing Retail Center & Business Park will be served by the Eastern Municipal Water District (EMWD), which has accounted for the Project's water demand in its 2020 Urban Water Management Plan (UWMP). The Project's estimated total water demand is 561.68 acre-feet per year (AFY), which remains within the demand projections previously considered for the site. Water supply for the Project will be met through a combination of imported water from the Metropolitan Water District and EMWD's local supply resources, including potable groundwater, desalinated brackish groundwater, and recycled water where applicable. The Project will be required to implement water-efficient devices and landscaping in accordance with EMWD's "Water Efficient Guidelines for New Development" to enhance conservation efforts.

Recycled water will be the preferred source for all non-potable uses, such as irrigation for landscaped areas, in alignment with EMWD's policies. Prior to construction, the Project developer must coordinate with EMWD to establish development design conditions, which may include the installation of on-site and off-site water infrastructure improvements. Any changes to the Project's land use or density will require updates to EMWD to ensure accurate tracking of water demand projections. Additionally, the Project will undergo periodic review every three years until construction begins to verify that no significant changes to EMWD's water supply or the Project's demand have occurred.

Water System Plan

Figure 3.0-9, *Existing and Proposed Water Infrastructure*, shows the existing and proposed water infrastructure system for Harvest Landing. Existing infrastructure is located along Indian Avenue, Barrett Avenue, Placentia Avenue, Orange Avenue, and Perris Boulevard. However, the water system will be expanded to accommodate buildout of Harvest Landing. Expanded water infrastructure is proposed to extend Orange Avenue infrastructure between Indian Avenue and Barrett Avenue; expand infrastructure along Barrett Avenue south of Orange Avenue, and along Frontage Road.

Non-Potable Water System

Figure 3.0-10, *Proposed Non-Potable Water Infrastructure*, illustrates the proposed non-potable water system for the Project. Recycled water will ultimately be available for future connections throughout the Specific Plan area. All landscaped areas will be irrigated with recycled water



HARVEST LANDING SPECIFIC PLAN CIRCULATION AND INFRASTRUCTURE

provided by EMWD once the Indian Avenue recycled water line is extended to the Project Site. In the interim, until that connection is established, landscaping will be irrigated with potable water. Potable water use will continue only until EMWD's recycled water facilities are fully extended to serve the site at ultimate buildout..

Water Development Standards

Design Standards

All waterlines shall be designed and located per Eastern Municipal Water District (EMWD) standards. All waterline facilities shall require the approval of both EMWD and the City of Perris.

Water Supply Assessment

Individual projects shall be required to comply with Senate Bills 610 and 221 for the preparation of a Water Supply Assessment as follows:

- Retail shopping centers or business establishments employing more than 1,000 persons or having more than 500,000 square feet of floor space.
- Commercial office building employing more than 1,000 persons or having more than 250,000 square feet of floor space.
- Business park, manufacturing or processing plants and business park parks housing more than 1,000 persons, occupying more than 40 acres of land or having more than 650,000 square feet of floor area.

Plan of Service

Developers shall coordinate with EMWD to determine water service requirements through EMWD's Plan of Service process.

Fire Protection

All water facilities shall be sized to provide adequate fire protection per the requirements of the City of Perris Building and Safety Department.

Irrigation Water Demand

Developers shall provide information that estimates a project's irrigation water demand, and submit conceptual landscape/irrigation conceptual plans to EMWD for review during the plan of service process.

Conservation Measures

The Harvest Landing Retail Center & Business Park will incorporate comprehensive water conservation measures to promote long-term sustainability and reduce overall water demand. These measures will align with the Eastern Municipal Water District (EMWD)'s Water Efficient Guidelines for New Development, as well as state and local water conservation policies.

The development will implement high-efficiency plumbing fixtures in all buildings, including low-flow toilets, urinals, faucets, and water-efficient dishwashers where applicable. Irrigation systems



HARVEST LANDING SPECIFIC PLAN CIRCULATION AND INFRASTRUCTURE

will utilize weather-based controllers, drip irrigation or equally water efficient water systems. All landscaping will be designed with drought-tolerant and native plant species, reducing the need for supplemental irrigation.

Additionally, non-potable water sources, such as recycled water, will be prioritized for irrigation and other permissible uses in coordination with EMWD's recycled water infrastructure. Indoor and outdoor conservation strategies will be continuously monitored, and future tenants may be required to adhere to EMWD's water efficiency programs, including ongoing conservation incentives and education initiatives.

By incorporating advanced water-saving technologies, sustainable landscaping, and efficient irrigation systems, the Harvest Landing Retail Center & Business Park will significantly reduce its overall water footprint while supporting regional water conservation efforts.

Existing Facility Relocation

Relocation of existing water facilities will require coordination with and approval by EMWD. All relocation costs shall be incurred by the development.

Inspection

All waterlines shall be placed underground and inspected by EMWD and the City of Perris.

3.3.2 Recycled Water

Recycled Water Candidates

Projects located within one mile of existing EMWD recycled water facilities and that require more than 3,000 square feet of landscape are potential recycled water candidates. EMWD shall be contacted early in the development process to determine if a recycled water connection will be required or if recycled water facilities need to be constructed.

On-Site Recycled Waterline

All projects within the Harvest Landing Specific Plan area will be required to install on-site recycled waterlines (purple pipe) and an irrigation meter for connection to existing or future recycled facilities.

3.3.3 Sewer

The Harvest Landing Project lies within the service area of EMWD and the sewer plan for Harvest Landing is based on their regional master plan, the projected demand generated at project build-out, and the capacity of the sewer systems surrounding the Project.

Figure 3.0-11, *Existing and Proposed Sewer Infrastructure*, shows the existing and proposed sewer infrastructure for Harvest Landing. Existing infrastructure is located primarily throughout the



HARVEST LANDING SPECIFIC PLAN CIRCULATION AND INFRASTRUCTURE

northern half of the Project site. Development of Harvest Landing would expand sewer infrastructure throughout the southern half of the Project site.

Sewer Development Standards

Design Standards

All sewer lines shall be designed and located per EMWD standards. All sewer facilities shall require the approval of both EMWD and the City of Perris.

Plan of Service

Developers shall coordinate with EMWD to determine sewer service requirements through EMWD's Plan of Service process.

Existing Sewer Lines May be Relocated to Facilitate Development

Relocation of existing sewer facilities will require coordination with and approval by EMWD. All relocation costs shall be incurred by the development.

On-Site Sewage Disposal Systems

On-site sewage disposal systems are prohibited for all non-residential land uses, unless otherwise approved by the City Engineer.

3.3.4 Stormwater Drainage

Perris Valley Area Drainage Plan

The Harvest Landing Specific Plan area is located within the Perris Valley Area Drainage Plan that was prepared by the Riverside County Flood Control and Water Conservation District (RCFC&WCD) and adopted by the City of Perris. The Perris Valley Area Drainage Plan provides the necessary storm drain infrastructure required to provide flood protection for the surrounding area. Moreover, the Perris Valley Area Drainage Plan has adopted a fee of \$8,875 per acre that is used to finance the required storm drain infrastructure identified within the Perris Valley Area Drainage Plan watershed area.

The adopted Perris Valley Area Drainage Plan has a total of 10 storm drain facilities within the Project boundary, listed in Table 3.0-4, *Perris Valley Area Drainage Plan Storm Drain Facilities*.



HARVEST LANDING SPECIFIC PLAN CIRCULATION AND INFRASTRUCTURE

Table 3.0-4, Perris Valley Area Drainage Plan Storm Drain Facilities

Proposed Storm Drain Facility	Storm Drain Size	Perris ADP Equivalency
Line K	12'x8' RCB 102" RCP 96" RCP	Line K
Lateral K-15	60" RCP	Line K-15, Line K-17 & K-18
Lateral K-19	48" RCP	Line K-16 & Line K-19
Lateral K-17	48" RCP	Line K-23
Line K-20	48-90" RCP	Line K-20
Lateral K-22	36" – 60" RCP	Line K-21 & Line K-22
Lateral K-24	36" RCP	Lateral K-24

Based on research conducted for the Project area, the Perris Valley Area Drainage Plan identified 75% of the area as single family one-quarter-acre dwelling units. Please note, the Perris Valley Area Drainage Plan was based on a land use map that pre-dates Perris Valley Area Drainage Plan adoption date of July 1987. The critical storm drain system is Line K, which is the mainline that collects all other storm drain systems. At the downstream end of the Project boundary, Line K has a design flow rate of 626 cubic feet per second (cfs). The proposed Harvest Landing development area has been planned to discharge storm flows into Line K. As a result, the Project will be required to ensure that the design flow rate of 626 cfs is not exceeded.

Existing Drainage

Prior to the development of Harvest Landing and the surrounding master planned drainage improvements, a total of 963.48 acres of drainage (327.48 on-site and 636 off-site from the west and north) and a peak flow rate of approximately 1,090 cfs impacted the site. Of the total existing drainage area and flow, 27 acres and 40 cfs drain to Line J and the remaining 950 acres and 1,050 cfs drain to Line K. Prior to implementation of Harvest Landing and Perris Valley Area Drainage Plan improvements, off-site drainage came from the following sources:

- **Culvert 1 (Orange Avenue):** a 4-foot by 3-foot reinforced concrete box that accommodated 240 cfs of off-site drainage from approximately 130 acres to the west. Water from Culvert 1 flowed into an existing lateral and headwall at the intersection of Orange and Barrett Avenues via a natural earthen channel.
- **Culvert 2 (approximately 1400 feet South of Orange Avenue):** a 6-foot by 3-foot reinforced concrete box that accommodated 360 cfs of off-site drainage from approximately 380 acres to the west.
- **Culvert 3: Line J,** which runs through the southern portion of the site and conveys off-site drainage from the west through Harvest Landing via an underground pipe.
- **Northwest:** Approximately 70 acres and 64 cfs of surface flows from the northwest.



HARVEST LANDING SPECIFIC PLAN CIRCULATION AND INFRASTRUCTURE

- **Line H**, an existing storm drain facility, begins at Indian Avenue along Placentia Avenue extending easterly and constructed as part of the I-215 interchange project. It provides a regional basin to collect and mitigate storm runoff from west of Indian and I-215. Captured Storm flows are metered down within basin and discharged into an underground 36-inch storm drain pipe system that is maintained and operated by the City. Line H storm flows are mitigated from what is shown in the Perris Valley MDP by the 215 interchange improvement project.
- **Frontage Road:** Existing concrete roadside channel running parallel to the Caltrans I-215 right-of-way from the intersection of Frontage Road and Orange south to the intersection of Frontage Road and Walmart Supercenter (Barrett Ave.), approximately 4,410 ft. Interstate 215 (Caltrans) is located to the west of the site; has multiple road inlets with underground storm drain lines that collect and convey storm water from the Caltrans right-of-way to the existing roadside concrete ditch along the west side of Frontage Road. Approximately six (6) underground drain lines are connected to the existing ditch that conveys flows to the south, to an existing inlet and 24" lateral connecting to the existing MDP Line J (11'x5' RCB).

Figure 3.0-12, *Existing and Proposed Storm Drain Infrastructure*, shows the existing and proposed storm drain infrastructure for Harvest Landing. Limited storm drain infrastructure exists around the perimeter of the Project site. Buildout of the Harvest Landing Specific Plan would expand storm drain infrastructure throughout the Project site.

Proposed Drainage System

The storm drain design of the Project is maintaining the connection to Line K downstream of Perris Boulevard. The total allowable flow rate for the Line K system cannot exceed 626 cfs. Due to the change in land use associated with the Project, each development site within the Specific Plan will incorporate subsurface storage basins or where feasible above ground basins to mitigate flow rates in order regulate outflow rates where the cumulative total will be at or less than 626 cfs. The Project is proposing to construct the Line K system from the westerly Project boundary to the east to Medical Center Drive. This will allow the Line K system to be constructed to the existing Line K Channel that is maintained and operated by RCFC&WCD. The existing Line K Channel downstream and east of Medical Center Drive is a 100 year facility that has been designed to meet the requirements of the Perris Valley Area Drainage Plan. Moreover, the existing Line K Channel conveys the flows to the Perris Valley Channel which is also maintained and operated by RCFC&WCD.

The Project is proposing to construct 6 storm drain infrastructure facilities. The design of the proposed planning area will eliminate the need for some of the existing storm drain facilities. The Specific Plan area will be designed as follows:

- Site 1, located between Indian Avenue and Barrett is a business park site that will eliminate the need for Line K-16, Line K-18 and Line K-19.
- Sites 2, 3, 4, and 5 will discharge into proposed Lateral K-16 and will eliminate the need for Line K-18 and Line K-19.



HARVEST LANDING SPECIFIC PLAN CIRCULATION AND INFRASTRUCTURE

- The Commercial Retail Center will include a large 12.91-acre water quality bio-retention basin with a subsurface storage basin to mitigate for water quality.
- The Big Box Commercial Retail site will implement onsite storm drain and eliminate the need for Line K-15 and Line K-17. However, proposed Lateral K-15 will be proposed to perpetuate the flow patterns identified in the Perris Valley Area Drainage Plan.
- Site 6 is a proposed business park site located south of Site 1. The Project proposes to realign the storm drain systems in Barrett Avenue with the proposed Lateral K-15 system. Site 6 is tributary to the proposed Line K system; therefore, the drainage area is being perpetuated as planned. The Line K-15 system defined in the Perris Valley Area Drainage Plan will be deleted.
- Site 7 is a proposed business park site located south of Site 6 and represents the most southerly limit of the Project. Based on the Master Drainage Plan hydrology, about 50% of the Site 7 project was proposed to drain into Line J. However, due to existing development that has occurred, a drainage path to Line J is not accessible. As a result, the proposed Project has been planned to discharge into Lateral K-15. It should be noted, the overall Project will ensure that the peak flow rate for Line K at the downstream end of the Project does not exceed the Master Drainage Plan flow rate, to allow the entire Site 7 to be re-routed into Line K.

Table 3.0-5, *Harvest Landing Specific Plan Proposed Storm Drain Facilities*, below summarizes the proposed storm drain systems and the equivalent system shown in the existing Perris Valley Area Drainage Plan.

Table 3.0-5, Harvest Landing Specific Plan Proposed Storm Drain Facilities

Proposed Storm Drain Facility	Storm Drain Size	Perris Valley Area Drainage Plan Equivalency
Line K	12'x8' RCB 102" RCP 96" RCP	Line K-16 Line K-18 & Line K-19
Line K-13	48" RCP	Line K-13
Link K-14	Exist. 3.5 x 11 RCB, 3.5x 8 RCB, & 36"	Line K-14
Lateral K-15	60" RCP	Line K-15 & Line K-17
Lateral K-16	48" RCP	Line K-16 & Line K-19
Lateral K-17	48" RCP	Line K-23
Line K-20	84" RCP	Line K-20
Lateral K-22	36" – 60" RCP	Line K-22 & Line K-24

The proposed solution provided by the Project is an alternative to the approved Perris Valley Area Drainage Plan. The alternative is required since the approved Perris Valley Area Drainage Plan never accounted for a development site or final street locations. The proposed design along with proposed onsite detention basins will ensure that all Project flows are mitigated cumulatively to be below the 626 cfs for Line K at Perris Boulevard. Moreover, the proposed solution provides storm



HARVEST LANDING SPECIFIC PLAN CIRCULATION AND INFRASTRUCTURE

drain alignments that perpetuate the drainage patterns as envisioned by the Perris Valley Area Drainage Plan.

On-Site Detention Basins

The Project will contain a 12.91-acre WQMP Drainage/Detention Facility extending south from the Perris Boulevard/Orange Avenue intersection adjacent to the Commercial Area. The WQMP Drainage/Detention Facility is comprised of two on-site bio-retention basins and one underground detention basin. The WQMP area will include landscaping and walking paths to simultaneously serve as a green and open space amenity for employees of Harvest Landing. Stored stormwater from the underground detention system will be pumped to sub-regional basins for treatment.

To mitigate the flow from the site to Line K down to the required 626 cfs, the on-site detention basins shall have a minimum capacity to store 92 acre-feet of storm water.

General Standards

Refer to NPDES Permit Board Order R8-2013-0024 for comprehensive and up-to-date information regarding water quality management standards, including specific guidelines for reducing pollutants in stormwater runoff. This order outlines the requirements for both construction and post-construction phases, focusing on maintaining compliance with State and Federal water quality laws. To further ensure adherence, detailed design strategies, such as Best Management Practices (BMPs), can be found in the Water Quality Management Plan (WQMP) Manual and the Low Impact Development (LID) Manual. Both documents are published by the Riverside County Flood Control and Water Conservation District, providing essential tools for managing stormwater, enhancing infiltration, and minimizing runoff impact in new developments.

Water Quality Management Plan

Most developments are required to implement a Water Quality Management Plan (WQMP) in accordance with the most recently adopted Riverside County MS4 NPDES Permit (Board Order R8-2013-0024). Approval by the City of a WQMP plan requires submittal of a complete document with supporting data which includes at a minimum, a site "Post-Construction BMP Plan," and treatment control facility sizing calculations. Site design based on Low Impact Design (LID) and source control best management practices (BMPs) shall be incorporated into the civil site design. If these two types of BMPs do not sufficiently manage hydromodification or treat expected pollutants, treatment control facilities shall be implemented in order to assure proper pollutant treatment. Treatment control BMPs are in accordance with Riverside County Storm Water Best Management Practice Hand Book. The Regional Water Quality Control Board (RWQCB) continuously updates impairments as studies are completed, the most current version of impairment data should be reviewed prior to preparation of Preliminary or Final WQMP document.



HARVEST LANDING SPECIFIC PLAN CIRCULATION AND INFRASTRUCTURE

Water Quality Standards

In accordance with the MS4 Permit, applicable new development and redevelopment projects shall adhere to the following:

- Design the site to minimize imperviousness, detain runoff, and infiltrate, reuse, or evapotranspire runoff where feasible.
- Cover or control sources of stormwater pollutants.
- Use LID to infiltrate, evapotranspire, harvest and use, or treat runoff from impervious surfaces.
- Ensure runoff does not create a hydrologic condition of concern.
- Maintain Stormwater BMPs.

Where feasible, roadways and parking aisles shall incorporate medians to enhance site functionality and support Low Impact Development (LID) principles. Instead of traditional raised or mounded landscaped medians, these depressed landscaped areas are designed to capture runoff from parking lots and streets. This helps reduce directly connected impervious surfaces, promotes infiltration, and provides initial treatment of runoff through a swale or trench before it reaches a treatment control facility. The design shall include a flow line slope or a grated inlet to ensure proper drainage of the depression within a maximum of 72 hours.

Sitewide Drainage Standards

The development of the drainage system in Harvest Landing will be subject to the following standards:

- The Riverside County Flood Control and Water Conservation District (RCFC&WCD) conducts a separate review and conditioning process independent of the City's review and approval procedures. Harvest Landing shall coordinate directly with RCFC&WCD to obtain any future Conditions of Approval required by the District. In the event of any conflict between RCFC&WCD requirements and the requirements herein, the most stringent requirement, as determined by the City of Perris, shall apply.
- All drainage facilities will be designed and constructed in accordance with the Riverside County Flood Control and Water Conservation District (RCFC&WCD) standards and specifications, the City of Perris standards, and the Standard Specifications for Public Works Construction.
- Storm drain facilities shall be designed to collect and discharge storm water runoff without damage to streets or adjacent properties.
- The Eastern Municipal Water District (EMWD) will review the design of drainage facilities in conjunction with their review of the sewer and water facilities.
- Drainage plans shall be submitted to EMWD for review and approval. This is to ensure that all proposed drainage facilities are compatible with the existing EMWD facilities.
- The capital cost of all on-site facilities will be the applicant's responsibility. Such facilities will be dedicated to RCFC&WCD, the City of Perris or County Service area for maintenance and operations. There may be Area Drainage Plan (ADP) fee credits and/or reimbursements from RCFC&WCD for the construction of ADP facilities in the area. Applicable ADP fee credits and/or reimbursement are for ADP facilities larger than 36" in diameter.
- All areas within the Specific Plan area will be required to prepare a Water Quality Management Plan (WQMP) and a Storm Water Pollutant Prevention Plan (SWPPP) in accordance with the requirements of the National Pollutant Discharge Elimination System



HARVEST LANDING SPECIFIC PLAN CIRCULATION AND INFRASTRUCTURE

(NPDES) standards. Any business park developments will require an additional SWPPP to operate.

- All projects proposing construction activities including clearing, grading, or excavation that results in the disturbance of at least one acre of total land area, or activity which is part of a larger common plan of development of one acre or greater shall obtain the appropriate NPDES construction permit and pay the appropriate fees. All development within the Specific Plan boundaries shall be subject to future requirements adopted by the County to implement the NPDES program. Mitigation measures may include, but not be limited to, on-site detention, water quality basins, covered storage of all outside facilities, vegetated swales, and monitoring programs, etc.
- In the case of phased development, existing on-site drainage and drainage facilities that are outside of the improvement area will need to be accounted for. The developer's engineer shall provide solutions that address the drainage from any unimproved areas during the permitting process.



Figure 3.0-9, Existing and Proposed Water Infrastructure

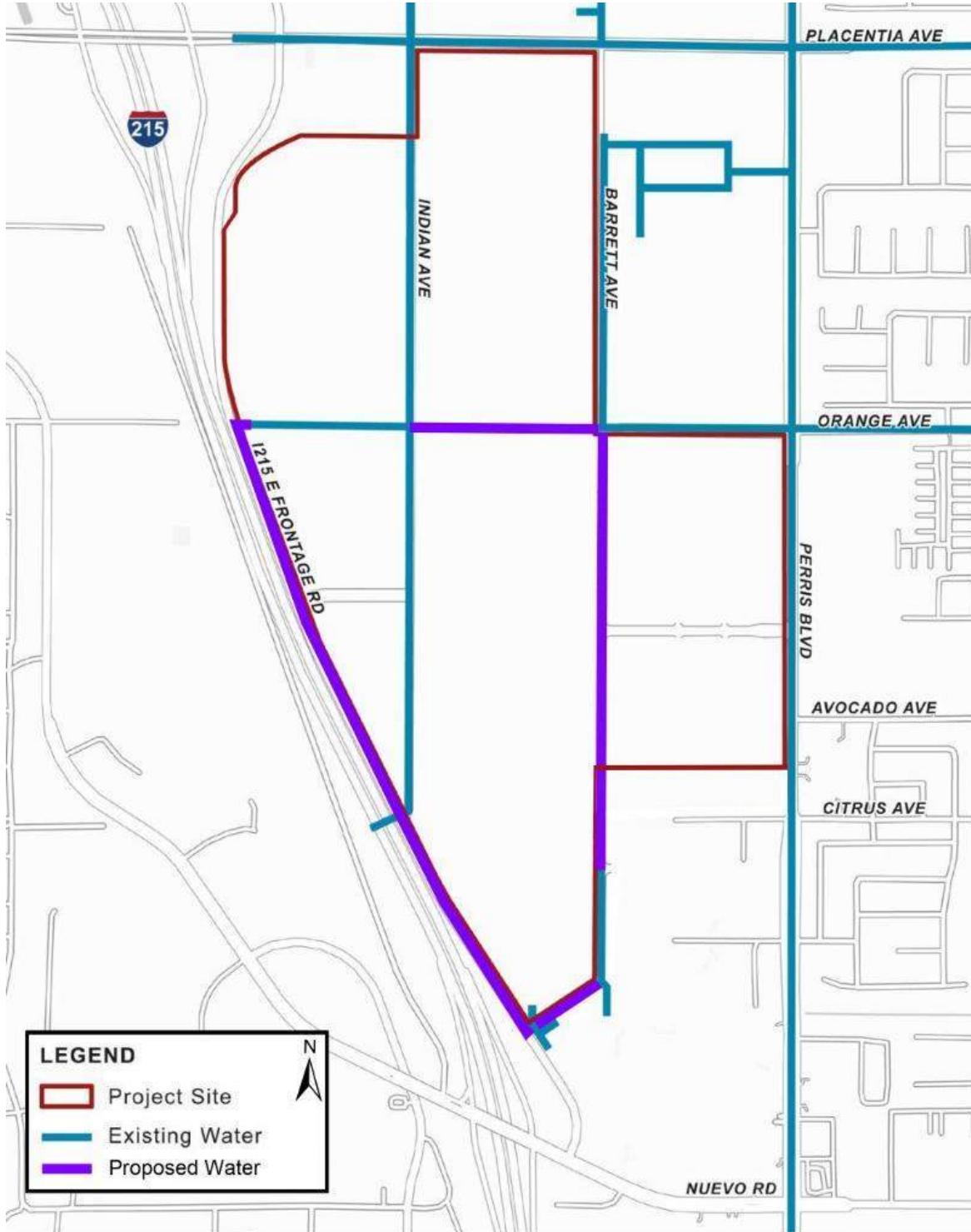




Figure 3.0-10, Proposed Non-Potable Water Infrastructure

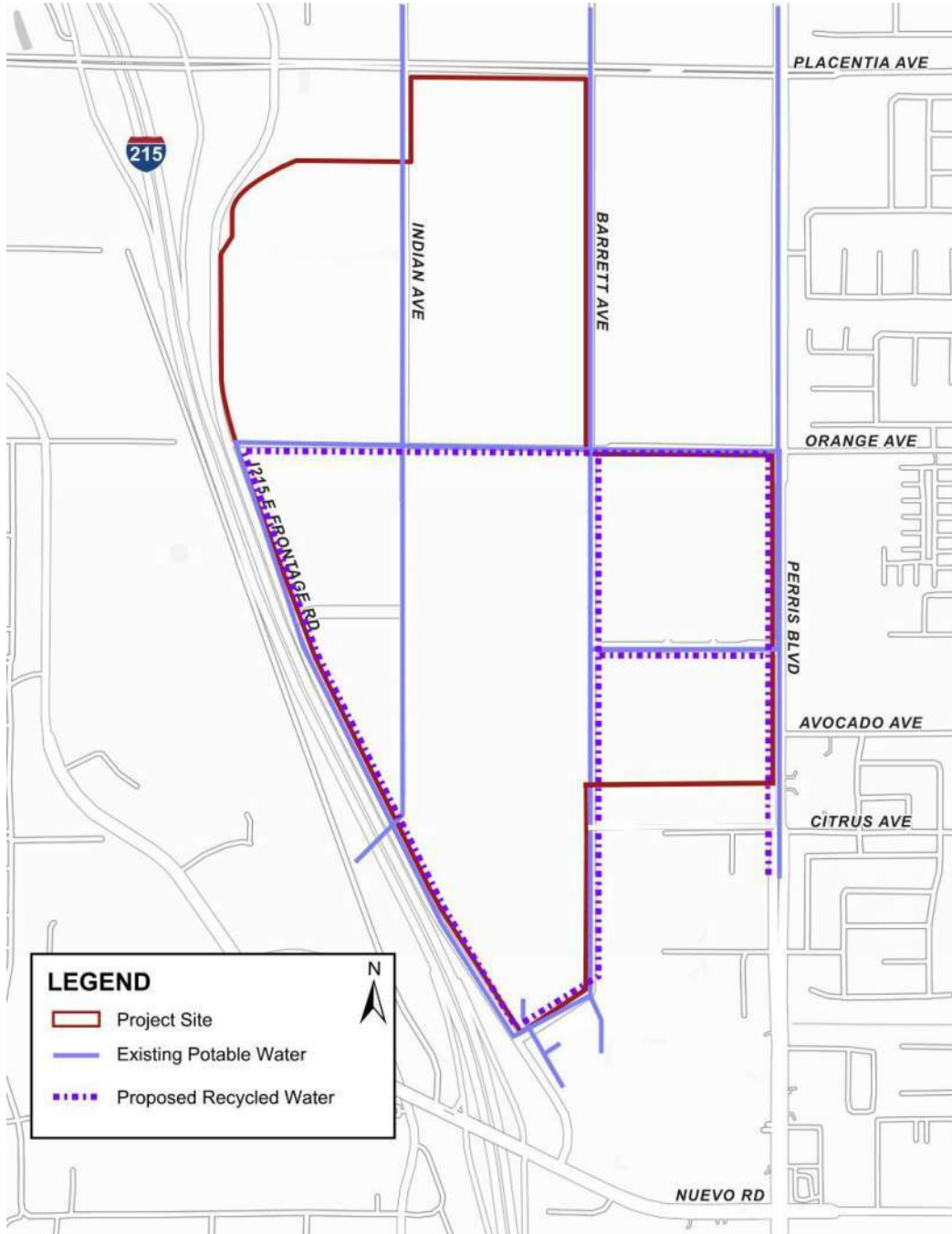




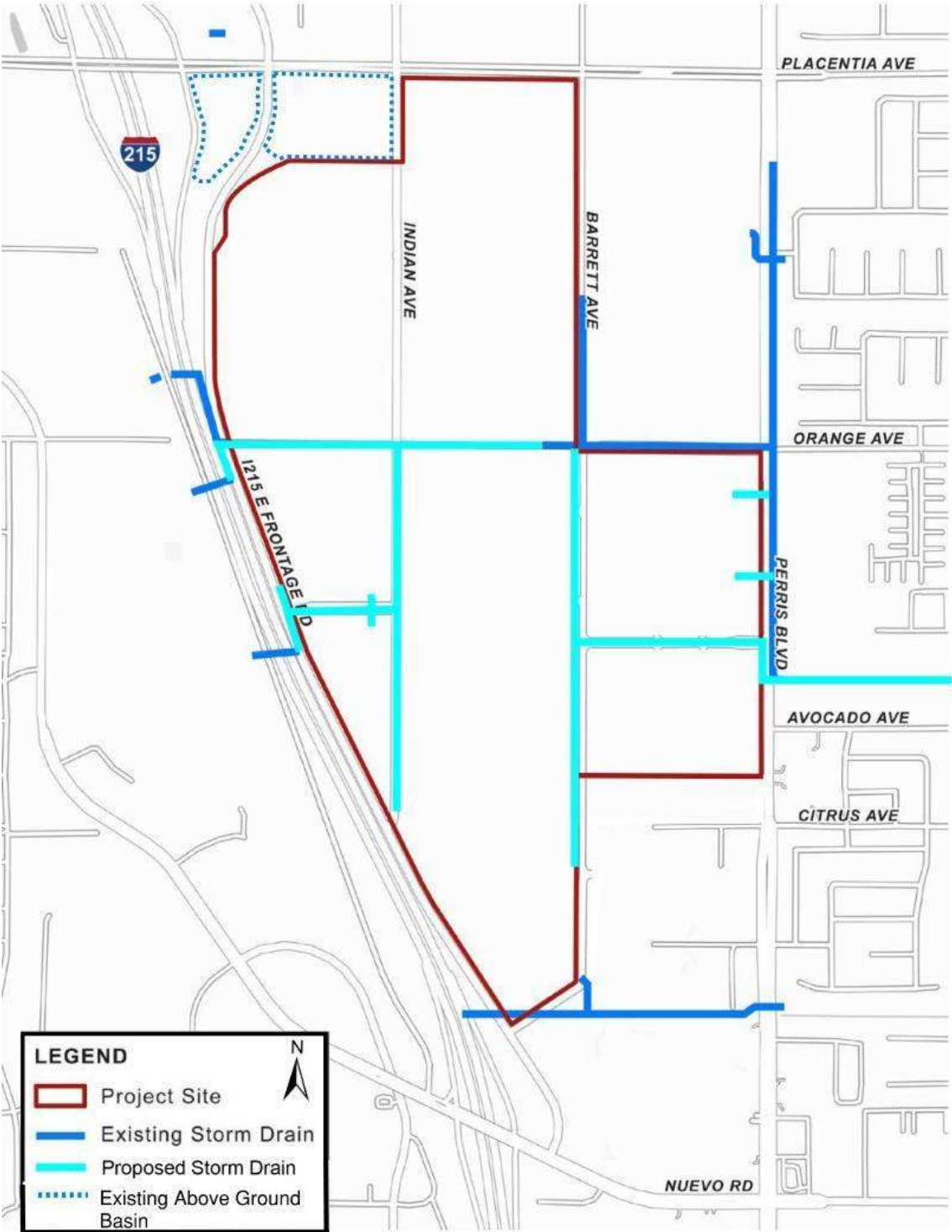
Figure 3.0-11, Existing and Proposed Sewer Infrastructure





HARVEST LANDING SPECIFIC PLAN CIRCULATION AND INFRASTRUCTURE

Figure 3.0-12, Existing and Proposed Storm Drain Infrastructure





HARVEST LANDING SPECIFIC PLAN CIRCULATION AND INFRASTRUCTURE

Water Quality Site Design

Low Impact Design

According to the State Water Resources Control Board, Low Impact Design (LID) is, “a sustainable practice that benefits water supply and contributes to water quality protection. Unlike traditional storm runoff BMPs, LID takes a different approach by using site design and storm water management to maintain the site’s pre-development runoff rates and volumes. The goal of LID is to mimic a site’s predevelopment hydrology by using design techniques that infiltrate, filter, store, evaporate and detain runoff close to the source of rainfall.

As stated in the Riverside County LID Manual, when LID is implemented correctly on a site, it provides two primary benefits: (1) hydromodification flows are managed across the site, and (2) expected pollutants are reduced in the remaining runoff. In order to meet RWQCB requirements in the Santa Ana Watershed, the design capture volume (VBMP) is based on capturing the volume of runoff generated from an 85th percentile, 24-hour storm event. There are eight mandatory BMP types to be implemented on project sites:

- Infiltration Basins
- Infiltration Trenches
- Permeable Pavement
- Harvest and Use
- Bioretention Facilities
- Extended Detention Basins
- Sand Filter Basins
- LID Biotreatment

The NPDES Permit requires that the design capture volume be first infiltrated, evapotranspired, or harvested and reused. When such retention methods are infeasible, the remainder of the volume can be biotreated. The steps to this approach include:

- Optimize the Site Layout
- Preserve Existing Drainage Patterns
- Protect Existing Vegetation and Sensitive Areas
- Preserve Natural Infiltration Capacity
- Minimize Impervious Area
- Disperse Runoff to Adjacent Pervious Areas
- Delineate Drainage Management Areas
- Classify and Tabulate DMAs, and Determine Runoff Factors
 - Self-treating areas
 - Self-retaining areas
 - Areas draining to self-retaining areas
 - Areas draining to BMPs

Where feasible, roadways and parking aisles shall incorporate medians to enhance site functionality and support Low Impact Development (LID) principles. Instead of traditional raised



HARVEST LANDING SPECIFIC PLAN CIRCULATION AND INFRASTRUCTURE

or mounded landscaped medians, these depressed landscaped areas are designed to capture runoff from parking lots and streets. This helps reduce directly connected impervious surfaces, promotes infiltration, and provides initial treatment of runoff through a swale or trench before it reaches a treatment control facility. The design shall include a flow line slope or a grated inlet to ensure proper drainage of the depression within a maximum of 72 hours.

3.3.5 Dry Utilities Standards

The Harvest Landing Specific Plan area is surrounded by existing development and utility infrastructure. Utilities for Harvest Landing will connect to existing infrastructure, with future projects coordinating with utility providers and the City to ensure adequate provision.

Natural Gas

The Southern California Gas Company (SoCalGas) is the natural gas provider for the area, and will provide natural gas service to Harvest Landing via an existing 6" gas main in Perris Boulevard. Individual projects will coordinate with the utility provider for connection to existing utilities and expansion of existing infrastructure, if needed.

Electricity

The Specific Plan area will be served by Southern California Edison for electric service. Developers of individual projects will coordinate with the utility provider for connection to existing utilities and expansion of existing infrastructure, if needed. All overhead lines under 33kVA will be undergrounded as directed by the City Engineer.

Telecommunications/Cable

There are multiple companies in the area that provide telecommunications and cable service. Developers of individual projects will coordinate with the utility provider for connection to existing utilities and expansion of existing infrastructure, if needed.

Utility Connections and Meters

All utility connections and meters shall be coordinated with the development of the site and shall not be exposed, except where deemed appropriate or necessary by the City of Perris Building Official. To the greatest extent possible, these utility connections should be integrated into the building or the architectural design.

Pad-Mounted Transformers and Meter Box Locations

Pad-mounted transformers and/or meter box locations shall be screened from view from surrounding properties and public rights-of-way. Utilities shall be located underground, unless waived by the City Engineer.



HARVEST LANDING SPECIFIC PLAN CIRCULATION AND INFRASTRUCTURE

Electrical, Telephone, CATV and Similar Service Wires and Cables

All electrical, telephone, CATV and similar service wires and cables which provide direct service to the property being developed, within the exterior boundary lines of such property, shall be installed underground.

Electrical Transmission Lines

Electrical transmission lines 66kv and less shall be installed underground.

All Equipment Shall be Internalized

All equipment shall be internalized into the building design to the greatest extent possible. When infeasible, they shall be screened and not prominently visible from public rights-of-way.

Solid Waste

Solid waste collection service in the City of Perris and Harvest Landing is provided by CR&R Disposal. Solid waste is first transported to Perris Materials Recovery Facility where recyclable materials are separated from solid wastes. Non-recyclable solid wastes are then transported to either the El Sobrante Landfill or to the Badlands Landfill in Moreno Valley. Construction and other waste disposal shall be hauled to a City approved facility.

3.4 WQMP Basin

3.4.1 WQMP/Drainage Detention

The Water Quality Management Plan (WQMP)/Drainage Detention land use is intended to be utilized for drainage/detention basin areas that double as green spaces with walking trails and/or light recreation equipment (e.g., benches, picnic tables, exercise equipment along walkways), but is not designated as Open Space or Park land use. Land uses other than for stormwater drainage and detention are not permitted.

3.4.2 WQMP/Drainage Detention Site Design

The WQMP land use area in Harvest Landing is intended to be incorporated as a major part of the Specific Plan area's stormwater treatment and drainage. Site design shall incorporate detention basins, retention basins, underground stormwater storage, designed to a capacity to accommodate stormwater drainage from the Harvest Landing Specific Plan area.

Site-wide stormwater drainage requirements are included above under Section 3.3.4, *Stormwater Drainage*.

On-Site Circulation

On-site circulation is limited to pedestrian and bicycle access via walking paths. Vehicular access is not permitted. Internal walkways shall provide connection through and/or around the site.



HARVEST LANDING SPECIFIC PLAN CIRCULATION AND INFRASTRUCTURE

Paving and walkways in the WQMP/Drainage Detention area shall consist of, but are not limited to, concrete pedestrian walkways and decomposed granite trails. Paved walkways shall be a minimum of 8 feet in width for passing and two-way use. Unpaved paths shall be no less than 4 feet in width minimum. Both shall be free of any debris or obstructions, including but not limited to: brush, stumps, logs, and large rocks. The trail surface should be kept free of rocks and debris greater than 1.5 inches in diameter.

Site design will consider pedestrian access when adjacent to trails or other community amenities.

Furnishings

To enhance versatility, specifically for the amenity of Phase 1's Buildings, the WQMP land use area, passive recreational furnishings such as benches, covered areas, picnic tables, and exercise equipment are allowed along walkways and in a manner not interfering with or in the way of stormwater drainage/detention.

Landscaping

Landscaping shall be limited to minimal ground cover, to be compliant with California Drought Tolerant measures, as well as reduce impact of roots on the drainage facilities for the Project.

When possible, these areas shall be designed to blend in with the surrounding development, be landscaped, and be designed to accommodate uses that can be flooded, such as active/passive recreation and natural open space.



HARVEST LANDING SPECIFIC PLAN DEVELOPMENT STANDARDS

4.0 DEVELOPMENT STANDARDS

4.1 Introduction

In order to ensure the orderly, consistent, and sensible development of the Harvest Landing Specific Plan, development standards and design criteria have been created for each land use category. The following sections outline development standards for the Harvest Landing Project site, which are complemented by design guidelines listed in Section 5.0, *Design Guidelines*, of this Specific Plan.

4.2 Community-Wide Development Standards

Project-wide development standards, such as requirements for lot sizes, setbacks, etc., are outlined in Table 4.0-1, *On-Site Development Standards by Land Use*.

Table 4.0-1, On-Site Development Standards by Land Use (C, MBU, WQMP and HLSP)

Development Standards	C	MBU	WQMP	HLSP	Notes
Minimum Lot Size	10,000 SF	15,000 SF	N/A	N/A	SF = square feet
Minimum Lot Frontage	100 feet	100 feet	N/A	N/A	45' on cul-de-sacs and street knuckles at ROW
Minimum Lot Width	100 feet	100 feet	N/A	N/A	
Minimum Lot Depth	100 feet	150 feet	N/A	N/A	90' on cul-de-sacs and street knuckles
Maximum Structure Size/ Floor Area Ratio (FAR)	0.75 FAR	0.75 FAR	N/A	0.025 FAR	Note 3 and 9
Minimum Structure Separation	None	None	N/A	10 feet for one-story buildings; 15 feet for two-story buildings	
Accessory Structures Size	No max.	No max.	N/A	N/A	
Maximum Lot Coverage by Structure	50% of lot	50% of lot	0%	N/A	Note 3
Maximum Building Height	50 feet	60 feet	N/A	25 feet (maximum one story)	Notes 1, 4 & 11
Maximum Building Height at Setback	25 feet	30 feet	N/A	N/A	Notes 6 Note 7 (for C) Note 8 (for MBU)
Front Yard Setback shall be as follows:					Notes 1, 5 and 6
Local/Collector Streets	5 feet	5 feet	N/A	10 feet	
Arterials	10 feet	10 feet	N/A	10 feet	
Expressway and Freeway	15 feet	15 feet	N/A	10 feet	



HARVEST LANDING SPECIFIC PLAN DEVELOPMENT STANDARDS

Development Standards	C	MBU	WQMP	HLSP	Notes
Adjoining non-residential	None	None	N/A	10 feet	
Adjoining residential	25 feet	25 feet	N/A	N/A	Note 10
Street Side Yard:	5 feet	5 feet	N/A	N/A	
Adjoining non-residential	None	None	N/A	N/A	
Adjoining residential	25 feet	25 feet	N/A	N/A	Note 10
Minimum Landscape Coverage	11%	14%	N/A	N/A	Note 2

Notes:

1. Building heights may be increased to a maximum of 100-feet above grade, provided that the front and street side yards are increased at least two (2) feet for every one (1)-foot of height increase beyond the standard set forth in Section 19.44.030 and provided that side and rear yard setbacks are increased by two (2) feet for every two (2)-foot increase beyond the standard set forth in Section 19.44.030.
2. Interior portions of a site dedicated to loading, storage, large vehicle maneuvering and parking may be permitted to forego required interior landscaping except for those properties abutting the MWD easement and the required landscaping for employee and visitor parking and outdoor employee break or amenity areas and required buffer areas.
3. FAR is the ratio of floor area divided by lot area. These development standards may be modified pursuant to the development participating in the Incentives program as described in this section.
4. The height of structure shall comply with the Federal Aviation Regulation, Part 77 restrictions for March Air Reserve Base.
5. If loading/unloading provided, setback shall not be less than 10-feet.
6. Front yards for buildings shall be increased by 5-feet for each 10 feet of building height greater than setback from property line/right-of-way to maximum structure height.
7. Setback requirements are for buildings 25-feet or less in height on the public right-of-way.
8. Setback requirements are for buildings 30-feet or less in height on the public right-of-way.
9. Small structures for maintenance purposes are permitted in the WQMP area such as maintenance rooms for EMWD pumping station etc.
10. Setback requirements are for building set back from property line of residential use to building face.
11. For the Sports Park land use: Uninhabitable architectural projections, such as towers, turrets, and chimneys may exceed the maximum building height, but shall not exceed a height of 40 feet.

Table 4.0-2, On-Site Development Standards by Density Category (Detached Residential)

Development Standards	LDR	MDR	MHDR*	Notes
Maximum Density	0.5—7.0	7.1—12.0	12.1—15.0	Dwelling Units per Acre
Minimum Lot Size per Unit	3,500 SF	2,100 SF	1,800 SF	SF = square feet
Minimum Lot Width	40 feet	35 feet	30 feet	Note 1 & 2
Minimum Lot Depth	70 feet	60 feet	50 feet	
Corner Lot Street Side Lot Width	45 feet	40 feet	35 feet	Note 14
Maximum Lot Coverage	50%	55%	75%	Note 3
Minimum Building Separation	10 feet	10 feet	10 feet	Note 10
Maximum Building Height	35 feet	35 feet	35 feet	Note 9
Front Setbacks:				
To Habitable Structure	12 feet	10 feet	8 feet	Note 4 & 12



HARVEST LANDING SPECIFIC PLAN DEVELOPMENT STANDARDS

Development Standards	LDR	MDR	MHDR*	Notes
To Garage				
1. Front Entry	20 feet	20 feet	Not Allowed	Note 5
2. Side Entry	10 feet	8 feet	Not Allowed	
To Unenclosed Porch	10 feet	6 feet	6 feet	
Side Setbacks from Street:				
To Structure	10 feet	10 feet	8 feet	Note 12
To Unenclosed Porch	5 feet	5 feet	5 feet	
Projections into Setback	3 feet	3 feet	3 feet	Note 6
Interior Side Setbacks				
To Habitable Structure	0 or 5 feet	0 or 5 feet	0 or 5 feet	Note 7
To Front-Entry Garage in Rear 1/3 of Lot	0 feet	0 feet	0 feet	
To Alley-Entry Garage	0 feet	0 feet	0 feet	
Projections into Setback	2 feet	2 feet	2 feet	Note 6
Rear Setbacks				
To Habitable Structure	12 feet	8 feet	8 feet	
To Front-Entry Garage in Rear 1/3 of Lot	0 feet	0 feet	0 feet	
To Alley-Entry Garage	<i>Not Required setback, but a 32-foot building-to-building Separation must be Maintained Along Alleys.</i>			
Projections into Setback	3 feet	3 feet	3 feet	Note 6 & 8
Minimum Outdoor Area	250 SF	250 SF	250 SF	Note 11
Landscape				Note 13

This is not a separate land use designation within the Specific Plan; however, the density is permitted pursuant to the previously approved Harvest Landing Specific Plan. The density is allowed within the Specific Plan and the density capacity is identified in Appendix A.

Notes: All setbacks shall be measured from the property line to the structure. In the case of a detached condominium development, the building separations listed on Table 4.0-2 shall apply and the setbacks and building site specifications listed in this table shall not apply.

- Any lot with a width of 35 feet shall provide garage access from an alley.
- For knuckle and cul-de-sac parcels, the minimum parcel width shall be met 20 feet from the front property line.
- The percentage of total site area covered by structures, open or enclosed, excluding the following uncovered structures: steps, courts, patios, terraces, walkways, and swimming pools.
- For variation in the front setback, 33 percent of the units may encroach up to 3 feet into the front setback.
- For front loaded garages only, a minimum of a 20-foot by 20-foot driveway apron is required.
- Projections are uninhabitable architectural features that extend beyond the building face. Projections include features such as eaves, chimneys, bay windows, stairways, and other architectural detailing.
- In the rear 1/3 of lots that employ a 0-foot setback on one side and a 10-foot setback on the other, the structure can encroach 5 feet into the interior side setback as long as a 15-foot rear building separation is maintained.
- Applies to habitable structure only. Garage projections are not permitted.
- Maximum building height is defined as the height from the top of the finished grade to the top of the roof peak.
- Zero lot line with a reciprocal easement excluded from building separation requirements.
- The outdoor area must consist of no less than 8 feet in any one direction.
- For freeway-adjacent units, a minimum of a 75 foot setback from the freeway right-of-way is required.
- Per Section 19.02.130.A, Landscaping, and Chapter 19.70, Land-scaping in the Perris Municipal Code.
- Per Section 19.02.050 Corner Clearances in the Perris Municipal Code.



HARVEST LANDING SPECIFIC PLAN DEVELOPMENT STANDARDS

Table 4.0-3, On-Site Development Standards by Density Category (Attached Residential)

Development Standards	MDR	MHDR*	HDR	Notes
Maximum Density	7.1—12.0	12.1—15.0	15.1—22.0	Dwelling Units per Acre
Minimum Parcel Size	5,750 SF	0.5 AC*	1 AC*	*Including easements
Minimum Parcel Width	50 feet	50 feet	60 feet	Note 1
Minimum Parcel Depth	80 feet	80 feet	90 feet	
Corner Parcel Street Side Lot Width	60 feet	60 feet	70 feet	
Maximum Lot Coverage	55%	55%	60%	Note 3
Minimum Building Separation				
Maximum Building Height	40 feet	40 feet	50 feet	Note 5
Front and Rear Setbacks:				
<i>To Habitable Structure</i>	<i>10 feet</i>	<i>Average of 20 feet</i>	<i>Average of 25 feet</i>	Note 3, 7 and 10
<i>To Unenclosed Porch</i>	<i>5 feet</i>	<i>5 feet</i>	<i>5 feet</i>	Note 7 & 10
<i>Projections into Setback</i>	<i>3 feet</i>	<i>3 feet</i>	<i>5 feet</i>	Note 4 & 10
Exterior Public Street Setbacks:				
<i>To Habitable Structure</i>	<i>10 feet</i>	<i>10 feet</i>	<i>10 feet</i>	Note 7, 8 & 10
<i>To Unenclosed Porch</i>	<i>5 feet</i>	<i>5 feet</i>	<i>5 feet</i>	Note 7 & 10
<i>Projections into Setback</i>	<i>3 feet</i>	<i>3 feet</i>	<i>3 feet</i>	Note 4 & 10
Interior Setbacks	N/A	N/A	N/A	
Minimum Usable Common Open Space	N/A	10% of Parcel	15% of Parcel	
Minimum Livable Area				
<i>Studio—550 SF</i>				
<i>1 Bedroom—700 SF</i>				
<i>2 Bedroom—900 SF</i>				
<i>3 Bedroom—1,100 SF</i>				
Minimum Usable Private Open Space (Note 6)				
<i>1st Floor—150 SF per Unit</i>				
<i>2nd & 3rd Floors—75 SF per Unit</i>				
<i>Minimum Dimension of 6 feet in Any Direction</i>				
Landscape				Note 9

This is not a separate land use designation within the Specific Plan; however, the density is permitted pursuant to the previously approved Harvest Landing Specific Plan. The density is allowed within the Specific Plan and the density capacity is identified in Appendix A.

Notes: All setbacks shall be measured from the greater of either (1) the property line or (2) from the back of the curb to the structure.

- For knuckle and cul-de-sac parcels, the minimum parcel width shall be met 20 feet from the front property line.
- The percentage of total site area covered by structures, open or enclosed, excluding the following uncovered structures: steps, courts, patios, terraces, walkways, and swimming pools.



HARVEST LANDING SPECIFIC PLAN DEVELOPMENT STANDARDS

3. Front entry tuck-under garages are permitted provided that they meet the setback requirement for the habitable structure and do not extend beyond the front wall of the structure.
4. Projections are uninhabitable architectural features that extend beyond the building face. Projections include features such as eaves, bay windows, stairways, porches, and other architectural detailing.
5. Maximum building height is defined as the height from grade to the top of the roof peak. An architectural projection such as a chimney or tower may exceed the maximum building height, but shall not exceed a height of 50 feet.
6. Private open space may include ground floor patios or courtyards, second- or third-floor balconies or decks, and rooftop decks. Private open space may be covered, but must be open on at least one side.
7. Setback requirements only apply to public streets that are exterior to the project.
8. For freeway-adjacent units, a minimum of a 75 foot setback from the freeway right-of-way is required.
9. Per Section 19.02.130.A, Landscaping, and Chapter 19.70, Land-scaping in the Perris Municipal Code.
10. Freeway adjacent residential units shall be set back at least 75 feet from the freeway right-of-way. These units shall utilize noise reduction design components as necessary. See the provisions for Freeway Residential Edge Conditions contained in Section 4, Design Guidelines, for more information on the treatment of freeway adjacent residential uses.

Table 4.0-4, Minimum Building Separation for Attached Residential Uses

Building Orientation	1 to 1 and 1 to 2 Stories	1 to 3 or 4 and 2 to 2 Stories	2 to 3 or 4 Stories	3 to 3 or 4 and 4 to 4 Stories	Notes
Front to Front	15 feet	20 feet	25 feet	30 feet	
Rear to Rear	10 feet	15 feet	20 feet	25 feet	
Front to Rear	15 feet	15 feet	20 feet	20 feet	
Side to Side	One Half of the Height of the Taller Building				
Sidewalls Parallel w/Front or Rear Walls	15 feet	30 feet	30 feet	30 feet	
Obliquely Aligned	Distance Specified Above May Decrease by 5 Feet at One Building Corner Provided There is an Equal or Greater Distance Increase at the Other End.				
Projections into Building Separation Area	3 feet Maximum				Note 1

Notes: Building separations also apply to detached, condominium developments.

Zero-width reciprocal easements excluded from building separation requirements.

Building separations shall be measured from habitable structure to habitable structure.

1. Projections are uninhabitable architectural features that extend beyond the building face. Projections include features such as eaves, bay windows, stairways, porches, and other architectural detailing.

4.2.1 Residential Density

The Harvest Landing Specific Plan allows for the future development of residential uses within the High, Medium and Low Residential land uses as well as the MBU and Commercial land use designations. Residential density within the Specific Plan area shall be determined exclusively by the allowable units identified in Appendix A (Prior Harvest Landing Specific Plan Land Use Map). All residential projects must comply with the applicable density provisions and the Development Standards and Guidelines as set forth in the Harvest Landings Specific Plan. This framework ensures that the Specific Plan's residential capacity is consistent with previously established entitlements and supports the City's long-term housing objectives.



HARVEST LANDING SPECIFIC PLAN DEVELOPMENT STANDARDS

The Specific Plan allows for a total residential capacity of 1,860 dwelling units allocated across four density categories as follows:

Table 4.0-5, Residential Density

Density	Units / AC	Number of Designated Units
Low	0.5—7.0	349 Units
Medium	7.1—12.0	372 Units
Medium High*	12.1—15.0	250 Units
High	15.1—22.0	889 Units
Total		1,860 Units

*This is not a separate land use designation within the Specific Plan; however, the density is permitted pursuant to the previously approved Harvest Landing Specific Plan. The density is allowed within the Specific Plan and the density capacity is identified in Appendix A.

These densities and allocations, which would remain unchanged as part of the Project, reflect the balance between providing a diverse range of housing opportunities and maintaining consistency with the circulation and infrastructure for the Project. Modifications resulting in higher densities or unit counts than those previously approved in the locations set forth in the Harvest Landing Specific Plan (refer to Appendix B) shall be subject to further review and environmental analysis.

4.2.2 Parking

Parking and loading throughout the Harvest Landing Project site shall comply with Chapter 19.69 of the City of Perris Zoning Ordinance.

Parking areas shall be designed to minimize the conflict between pedestrian and vehicular traffic. Parking areas shall provide clearly marked and well-lit pedestrian walkways to ensure pedestrian safety. Shade trees and structures shall be incorporated into parking lot design.

Parking areas for business park uses shall be designed to ensure passenger vehicles and trucks do not intermingle. Dedicated and separate access and circulation paths shall be required for each.

Table 4.0-2, *Parking Requirement*, lists the amount of parking required to adequately serve each land use type.



HARVEST LANDING SPECIFIC PLAN DEVELOPMENT STANDARDS

Table 4.0-6, Parking Requirements

Use	Standard (Parking Spaces per Square Foot [SF])
Commercial/Office/Service Uses	
Neighborhood or community shopping center	1 space for every 250 SF
General retail trades	1 space for every 250 SF
Retail furniture/appliance	1 space for every 500 SF
Restaurants, cafes, bars and cocktail lounges	1 space for every 75 SF of dining or serving area
Drive-in, fast food restaurants (1)	1 space for every 75 SF of dining or serving area
Banks, savings and loan, and financial institutions (2)	1 space for every 150 SF
Plant nurseries, lumberyard, building material yards, outdoor sales of merchandise	1 space for every 500 SF of indoor display, 1 space for every 2000 SF outdoor display
Professional Services	
Medical, dental, clinics	1 space for every 200 SF
Professional offices	1 space for every 300 SF
Veterinary service	6 spaces minimum, 1 space for every 500 SF in excess of 1,000 SF
Hospitals	1 space for every 2 beds, based on maximum capacity and 1 space for every employee on the largest shift
Urgent care centers	1 space for every 200 SF and 1 space for every employee on the largest shift
Business Park and Manufacturing Uses (2)	
Manufacturing	1 space for every 500 SF of manufacturing building area, and 1 space for every company vehicle and space required for additional uses on-site; or 1 space for every employee, whichever is largest
Parcel Hub	1 space per 1,000 SF of gross floor area for first 20,000 SF, plus 1 space per 2,000 SF for the second 20,000 SF, plus 1 space per 5,000 SF for that portion over 40,000 SF. Parking for office area comprising less than 10% of total gross square footage of the building shall be included. Office area over 10% shall be calculated at professional office rate. Truck and trailer parking shall be provided at 1 oversized space for every 5,000 SF of gross floor area. Truck doors not included in calculation.
Residential Uses (3)	
LDR	Two enclosed garage spaces are required for each detached unit
MDR	Detached: Two enclosed garage spaces per unit Attached: <ul style="list-style-type: none"> One space per unit shall be within a carport or an enclosed garage. Studio: 1 space/unit and 0.25 guest space/unit



HARVEST LANDING SPECIFIC PLAN DEVELOPMENT STANDARDS

Use	Standard (Parking Spaces per Square Foot [SF])
	<ul style="list-style-type: none"> One Bedroom: 1.5 spaces/unit and 0.5 guest space/unit. Two Bedroom: 2 spaces/unit and 0.5 guest space/unit Each additional bedroom: 0.5 space/unit up to 10 spaces, and 0.25 space/unit exceeding 10 spaces shall be distributed throughout development.
HDR	Detached: Two enclosed garage spaces per unit Attached: <ul style="list-style-type: none"> One space per unit shall be within a carport or an enclosed garage. Studio: 1 space/unit and 0.25 guest space/unit One Bedroom: 1.5 spaces/unit and 0.5 guest space/unit. Two Bedroom: 2 spaces/unit and 0.5 guest space/unit Each additional bedroom: 0.5 space/unit up to 10 spaces, and 0.25 space/unit exceeding 10 spaces shall be distributed throughout development.

- (1) In addition to the conditional use requirements set forth in Section 19.38.030, the Harvest Specific Plan requires Businesses with drive-thru service(s) to provide adequate stacking to accommodate eight (8) vehicles prior to each pick-up window to avoid conflict with on-site circulation. Unless the use can adequately show there is not a need for additional stacking.
- (2) Please refer to City of Perris Municipal Code Section 19.69.
- (3) Please refer to requirements set forth in Section 19.69 Parking and Loading Standards in the Perris Municipal Code.

Bicycle Racks

Facilities with 200 or more required parking spaces shall provide a bicycle parking area to accommodate no fewer than five locking bicycles, in conformance with the California Building Code. Facilities with 500 or more required parking spaces shall provide bicycle parking to accommodate no fewer than 15 locking bicycles. Bicycle parking shall be located near main entrances of buildings, adjacent to landscape areas.

ADA Compliant Parking

All parking lots and parking areas shall be ADA compliant, in conformance with the California Building Code.

4.2.3 General Compliance

Uses and Standards Shall Be Developed In Accordance With City of Perris Codes

All permitted uses and development standards applicable within the Specific Plan area shall be established by the Harvest Landing Specific Plan, including the Specific Plan Objectives, Chapter



HARVEST LANDING SPECIFIC PLAN DEVELOPMENT STANDARDS

2.0 (Land Use), Chapter 4 (Development Standards), and Chapter 5 (Design Guidelines). In the event of any inconsistency between the provisions of this Specific Plan and the City of Perris Municipal Code, the provisions of this Specific Plan shall prevail. Where the Specific Plan is silent on a subject, the applicable provisions of the Municipal Code shall apply.

Development Shall Be Consistent with the Harvest Landing Specific Plan

All development within the Harvest Landing Specific Plan area shall comply with the mandatory requirements of applicable City of Perris ordinances and all relevant State laws. Development shall be undertaken in substantial conformance with the Harvest Landing Specific Plan, as on file with the City of Perris Development Services Department, unless and until such time as the Specific Plan is formally amended in accordance with City procedures.

Subdivision Map Act

Lots created pursuant to the Harvest Landing Specific Plan, and subsequent tentative maps, shall be in conformance with the Development Standards of the zoning applied to the property and all other applicable City standards, as well as the Subdivision Map Act.

4.2.4 Crime Prevention Measures

Development projects shall take precautions by installing on-site security measures. Security areas shall include, but are not limited to, entry areas for automated teller machines (ATMs), display areas and bus stops. These areas shall provide 30 feet of candlepower for security lighting measures.

All facilities constructed within the Harvest Landing Specific Plan shall incorporate the following security and safety features to ensure the well-being of occupants and visitors. These measures are essential for maintaining a secure and safe environment throughout the development.

- Lights with sensors that automatically operate at night.
- Installation of fire systems, and video surveillance.
- Special lighting to improve visibility of the building address.
- Downward lighting throughout the development site to illuminate the area while reducing light pollution or light spillage.

4.2.5 Trash and Recyclable Materials

Trash and recyclable materials shall comply with the following:

- Development of all Harvest Landing Specific Plan sites shall contain enclosures (or compactors) for collection of trash and recyclable materials subject to water quality and best management practices.
- Trash receptacles shall be completely screened from public rights-of-way and parking areas, through site orientation, screening materials (walls/landscaping), or a combination of both.
- All trash enclosures shall comply with City of Perris Standards and with applicable City of Perris recycling requirements.



HARVEST LANDING SPECIFIC PLAN DEVELOPMENT STANDARDS

4.2.6 Screening

Aboveground and roof mounted utilities shall be screened from public view.

In the Multiple Business Use area, outdoor storage and/or operations shall be screened from public view. All other screening requirements will be per Section 19.02.070, *Screening*, of the Municipal Zoning Code.

Screening Methods

Acceptable screening methods include building offsets, connecting wing walls, perimeter site walls and fences, landscaping, and berming. Such screen walls shall be architecturally integrated with building by design, matching color, and material. Screen walls shall be of the same design and materials as primary buildings and a minimum of 6 feet in height for Commercial Land Use and 14 feet high for MBU Land Use designations, or as directed by the City to sufficiently screen loading docks. Screen walls exceeding 8 feet in height shall be softened with earthen berms and dense landscape as noted in Figure 4.0-1.

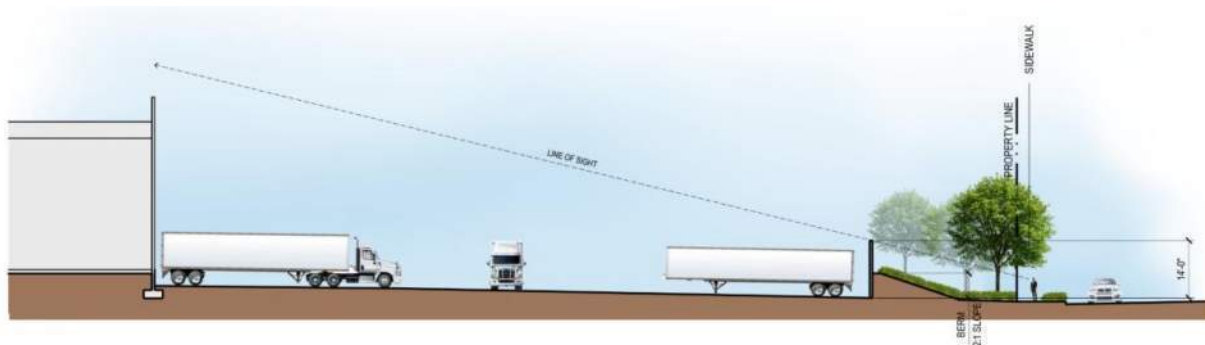
When possible, loading areas shall be located on the side or rear of a site and shall be screened from public view.

Residential screening shall follow section 19.02.070, *Screening*, of the Perris Municipal Code.

Screening of Outdoor Storage Areas, Work Areas, Etc.

The screening of outdoor storage areas, outdoor work areas, and mechanical equipment with walls that utilize the same building materials and architectural design of the buildings is required. Soften screen walls with earth berms and dense landscaping as depicted in Figure 4.0-1. The intent is to keep walls as low and unobtrusive as possible while performing their screening and security functions.

Figure 4.0-1, Screening Methods





HARVEST LANDING SPECIFIC PLAN DEVELOPMENT STANDARDS

4.2.7 Lighting

Per Section 19.02.110, Lighting, of the Perris Municipal Code. Lighting standards shall be consistent with City safety and illumination requirements.

Safety and Security

All development shall consider proper lighting for safety and security purposes.

Foot-candle Requirements Sidewalks/Building Entrances

Sidewalks shall have a minimum of 2 foot-candlepower of light across their surface. Building entrances and parking lots shall have a minimum of 1 foot-candlepower of light. Lighting standards shall be energy efficient. Based on Mt. Palomar Observatory's Dark Sky Ordinance, all projects will be conditioned to use low pressure sodium.

Up-Lighting

Up-lighting on buildings, including any building wash lighting, roof lighting, or other upward-directed architectural illumination, is prohibited within the Airport Overlay Zone due to the proximity to March Air Reserve Base/Inland Port Airport and the requirements of the Mt. Palomar Observatory Dark Sky Ordinance. Up-lighting shall be permitted only for landscaped areas, including specimen trees and accent planting, and shall be subject to review and approval by the Development Services Department.

Parking Lot Lighting

Parking area lighting shall be provided pursuant to Section 19.02.110.A of the City's Municipal Code, and in accordance with the following requirements:

- Parking areas shall have lighting which provides adequate illumination for safety and security.
- Parking lot lighting fixtures shall maintain a minimum of 1-foot candlepower across the surface of the parking area.
- Parking lot lights shall be located such that they do not conflict or displace intended tree planting locations.
- Pole footings in traffic areas shall be designed and installed such that they protect the light standard from potential vehicular damage. Above grade footing shall not exceed 24" in height and shall not obstruct walkways.
- Lighting shall be confined to Project Site illumination, and shall not spill into adjacent properties

4.2.8 Landscaping

Landscaping in Harvest Landing shall be pursuant to Section 19.02.130.A & B, *Landscaping*, and Chapter 19.70, *Landscaping*, of the City of Perris Zoning Code, except for the following:

Buildings within the MBU land use that are abutting MBU shall not be required to provide a 5-foot landscape area, except when area is visible from public right-of-way

- Harvest Landing has carefully selected plant species and locate landscaping to allow visibility of businesses and signs with freeway exposure.



HARVEST LANDING SPECIFIC PLAN DEVELOPMENT STANDARDS

- Commercial landscape requirements of section 19.70.050E with the exception of a minimum of one tree per six parking stalls. In addition, landscaping shall adhere to the landscape guidelines found in this Specific Plan.

Erosion Control

Refer to the City of Perris Standards, City of Perris Municipal Zoning Code, Chapter 19.70, Section 19.70.040, *Landscape Design Guidelines*. Prior to the installation of plant material, soil samples from representative slopes and flat areas shall be obtained by the landscape contractor and tested for agronomic suitability in order to determine proper planting and maintenance requirements for proposed plant materials with pre-planting and post-planting recommendations.

Positive Drainage to Street or Collection Device

All landscape areas shall have positive drainage to the street or collection devices.

Minimum 50% Shade Coverage

In accordance with the City of Perris Municipal Code (Section 19.71.050), shade trees shall be provided within the vehicular parking areas to attain a minimum 50% shade coverage of the parking area within five years of planting.

4.2.9 Hazardous Materials

The Hazardous Materials Management Division (HMMD) of the Riverside County Environmental Health Department is responsible for regulating hazardous materials business plans, chemical inventory, hazardous waste, underground storage tanks, and risk management plans in the County. Uses within Harvest Landing that propose to utilize chemicals or perform uses that fall under the purview of the HMMD shall be required to be reviewed and approved prior to the issuance of the certificate of occupancy.



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

5.0 DESIGN GUIDELINES

5.1 Introduction

Design Guidelines are intended to create an attractive and cohesive community identity through the provision of detailed direction for the physical design of Harvest Landing. The Design Guidelines are intended to be general and illustrative in nature. While promoting a high level of design quality, the Design Guidelines provide a wide degree of flexibility to encourage creativity on the part of property owners and designers. They are not rigid requirements; however, every development within Harvest Landing must incorporate a particular attention to detail reflected in these guidelines. These Design Guidelines establish the design framework that will be used to evaluate proposed developments. The intent of these guidelines must be met in order for a project to be approved during the development review process. Through the utilization of an established set of Guidelines, it is the City's intent to strike a balance between the creation of commercial and business park developments that are aesthetically pleasing, while respecting use and function.

5.2 Community-Wide Design Guidelines

Community-wide Design Guidelines apply to the entirety of Harvest Landing. They are intended to create a strong community identity through consistent design detail throughout the Project area.

General and Building-level Design Guidelines provide important design criteria for structures within Harvest Landing.

General

Establish a visual link in the Harvest Landing Specific Plan by using architectural and site design elements to unify the Development.

1. Quality materials should be used in the construction of buildings
2. Accessory structures, trash receptacles, mechanical equipment, parking structures, backflow preventers, security fences, and similar uses can seriously detract from the visual quality of an area. Therefore, care should be taken to minimize the visual impact of these uses through site design and visual shielding. When possible, these uses should be located away from roadways and public views, behind buildings, or in enclosed structures. Effective screening methods include landscaping, berms, walls and fences, and ornamental screening.
3. Accessory uses and utilities should be located behind the primary structure out of public view when possible. Structures that cannot be placed out of view should be shielded with berms, landscaping, attractive walls, or decorative screening.
4. Utilities should be screened and/or located underground as appropriate.
5. Accessory structures should be designed to look like a continuation or extension of the primary structure. They should have architectural detailing and landscaping similar to that of the primary structure.



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

6. Projects should present a clean and attractive professional appearance, and should utilize design features such as distinctive architectural detailing, landscaping, and lighting elements, to ensure a consistent design theme for that project.
7. Structures should be designed to relate to each other and the surrounding area.
8. A variety of paving materials should be used to clearly delineate vehicular and pedestrian areas and to create a sense of place.
9. Attractive and consistent lighting elements should be provided along roadways within the Specific Plan area. The height, brightness, and spacing of the lighting elements should be appropriate to the scale and speed of the roadway.
10. Pedestrian-scale lighting should be provided in areas with nighttime use, such walkways. The type, style, and intensity of pedestrian lighting should reflect the area's character.
11. Since Harvest Landing is in the Mount Palomar nighttime lighting sphere, lighting elements should minimize glare, spill over, and pollution.
12. Lighting fixtures should be compatible with the architectural styles of surrounding buildings and yet consistent throughout the community.
13. Iconic landscaping and buildings within the Project should be spotlighted to provide visual accent and directional reference.
14. Signage, See Section 6 of this Specific Plan for additional signage requirements.
15. Signage should be visually interesting and informative. It should be distinctive and eye-catching yet simple and tasteful. Cluttered designs and excess advertising should be avoided.
 - a. Multitenant signage is encouraged to minimize visual clutter.
 - b. Project signage should have a consistent design theme and color palette throughout the development to achieve a coordinated sense of identity.
 - c. Signage should be of a scale and style appropriate to the area and surrounding uses.
16. Business-related signage may be interior illuminated and entries should be externally illuminated.
 - a. Freestanding monument-style signage should be used. Pole signs are prohibited.
 - b. Signs should use materials and colors compatible with the surrounding areas' architecture and designs.
17. Trees, shrubs, and vines soften the visual appearance of walls and are strongly encouraged.
18. Walls should provide convenient pedestrian gates to ensure ease of pedestrian circulation.

Building-Level Design Guidelines

1. Buildings should be oriented to face onto streets and courtyards. This orientation will create more attractive, safe, and pedestrian-friendly streetscapes and public spaces.
2. Building entries should be oriented towards the street and clearly defined.
3. Avoid long, monotonous building façades and create diversity by clustering buildings around courtyards.
4. Varied massing and vertical step-back in building design prevents a monotonous streetscape and is encouraged.
5. Variations in style, architectural features, and colors create visual interest and are encouraged.



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

6. Architectural detailing and surface articulation is encouraged to be applied equally to all sides of a structure but is mandatory on all elevations that are visible to the public.
7. Establish a visual link in multi-building complexes by using architectural and site design elements to unify the Project.
8. Vary rooflines to reduce the overall mass of the building. Parapet walls and roof systems should be designed to conceal all roof-mounted equipment from adjacent properties and public rights-of-way. Flat roofs should be disguised through the use of parapet walls.
9. Avoid a single, large dominant building mass. Horizontal building masses should not exceed a height-to-width ratio of 1:3 without substantial changes in height and projecting or recessed elements.
10. Architectural elevations of principal buildings over 20 feet tall may have a clearly discernable base (approximately 3 feet), body, and cap (consisting of a cornice, parapet, awning, or eave that do not to exceed the base height). If used, the base and cap should be discernable from the body by changes in color, materials, texture, pattern, or profile.
11. Utilize high quality materials on building bases, bodies, and caps as described later in this section.
12. Utilize low reflective, subtle, neutral, or earth-tone colors on the building body. Building trim and accent areas may feature brighter colors, including primary colors. Applied paint over brick and stone is strongly discouraged.
13. Outdoor storage areas and loading docks should be screened from the street and adjacent uses.
14. Large buildings should have articulated façades, including recesses and architectural detailing, to avoid a monotonous streetscape. Openings should be recessed 2 to 4 inches to further articulate the façade.
15. Signs should be compatible with building and site design in terms of color, material, and placement.

Design Theme

The community features and facilities (roadways and entries,) of Harvest Landing should be designed with materials and styles that will strengthen the community's identity.

5.2.1 Architecture

Project Identity

Building and site development should incorporate an architectural component that provides an identity for the Project, ensuring it stands out and resonates with its intended audience. This identity can be achieved through distinctive design elements that reflect the Project's purpose, location, or cultural significance, creating a sense of place and belonging. Additionally, a well-defined architectural identity enhances the user experience, offering both functional benefits and aesthetic value, which can improve community engagement and long-term Project success. Attention to scale, massing, and building relief plays a crucial role in this identity, as these elements influence how the structure interacts with its surroundings, creates visual interest, and provides a



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

dynamic sense of depth and proportion. Thoughtfully designed scale and massing ensure that the Project is appropriately integrated into its environment, while building relief adds texture and dimension, preventing monotonous or overpowering façades.

Site Design and Site Layout

Avoid Long, Monotonous, and Unbroken Building Facades

Avoid long, monotonous and unbroken building facades that repeat the same design element along the same elevation without intermittent variations. Building design should avoid long, uninterrupted facade planes or blank walls. The exterior wall facades should be varied in depth, direction, and/or significant projections. Facades greater than one hundred (100) feet in length, should incorporate projections or recesses with a depth of one to ten feet. Such articulation should cumulatively account for at least twenty (20) percent of the length of the facade. No uninterrupted length of any facade should exceed one hundred (100) horizontal feet. Additionally, paint should be used to visually break up continuous wall surfaces and enhance facade articulation.

Fenestration

Fenestration should be used for functional and programmatic requirements and should be designed to break up the visual size of the building facade. Door and window openings should be recessed 2 to 4 inches to further articulate the facade. Buildings which include uses that do not lend themselves to fenestration (e.g., loading areas, and storage functions) should be designed so that these uses are screened from the public right-of-way.

Discernable Base, Body, and Cap

Principal buildings over 20 feet in height should strive to have a clearly discernable base, body, and cap. The cap should consist of a cornice, parapet, awning canopy or eave. The base and cap should be clearly distinguishable from the body through changes in color, material, pattern, profile or texture.

Building Relief

Building relief should be provided along all facades visible from streets and highways, areas accessible to and visible by the public.

Gates Visible From Public Areas

Gates for pedestrian and vehicular access to restricted areas that are visible from public areas (i.e., parking lots, drive aisles) should be constructed of solid durable material, wrought iron, tubular steel, or similar material when needed to serve the needs of security or screening.

Streetscapes

Streetscape elements, such as landscaping, lighting, street furniture, and signage should create an attractive, consistent, and cohesive image and complement the surrounding architectural styles. Attractively landscaped medians should be provided along major roadways to reduce



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

vehicular speed, create a pedestrian-friendly environment within commercial areas, and ensure a pleasant community image. Special patterned paving should be provided at pedestrian crossings within the Specific Plan area.

Prohibited Materials

No chain-link (with or without grapestake or vinyl inserts), barbed wire, wire, electronically charged or plain exposed plastic concrete/PCC fences are permitted.

Separate Employee Break Areas

The Harvest Landing Specific Plan will provide outdoor amenity for all employees of the site, located within the WQMP land use. In addition buildings over 100,000 square feet will provide amenity areas for employees onsite.

Scale, Massing, and Building Relief

Scaling in Relationship to Neighboring Structures

Scaling of buildings in relationship to neighboring structures and adjacent developments should be considered to promote compatible design.

Variation in Plane and Form

Variation should be provided in the plane and form of buildings and resulting adjacent spaces both inside and out with the use of recesses, varied roof lines, pop-outs, positioning and relationships of buildings in all areas visited by the general public and/or office areas.

Do Not Rely on Landscaping

Building design should not rely on landscaping to soften, buffer or otherwise provide relief for massive building form, but rather it should be used to accent superior architectural designs.

Distinct Visual Link

A distinct visual link should be established in multi-building complexes by using architectural and site design elements to unify the development as Shown below in Figures 5.0-1.

Break Up Tall Structures

Tall structures (20 feet or more) should be broken up by providing different treatments to the lower, middle, and top stories that define these three parts.

Avoid Monotony

Monotony and repetition in building elevations and the street scene should be avoided by incorporating varying building heights, massing, roof lines, fluted wall treatments, design elements, color variation, reveal lines, window treatments, texture and materials, building placement, and landscaping.

See Figure 5.0-1, *Example Building Designs*.

Figure 5.0-1, Example Building Designs



Provide Vertical or Horizontal Offsets

Vertical or horizontal offsets should be provided in the wall surfaces including columns, projections, and recesses.

Downspouts

Downspouts should be internalized to avoid external damage and should drain under walkways to landscape areas, underground storm drain and loading docks to avoid slip hazards.

Conceal Roof Mounted Equipment

Parapet walls and roof systems should be designed to conceal all roof-mounted mechanical equipment from view to adjacent properties and public rights-of-way.



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

High Quality Natural Materials

The use of high-quality natural building materials such as brick, stone, tinted/textured concrete (tilt-up) should be incorporated. The following is a list of permitted materials for the building base, body, and cap. Other materials not specifically mentioned may be permitted on a case-by-case basis.

- Building Base: Brick, native stone, manufactured stone or decorative concrete masonry units.
- Building Body: Wood, brick, native stone, manufactured stone, concrete, glass, or stucco. Imitation wood siding and metal siding are permitted so long as they are complimentary to the surrounding design. Mirrored or highly reflective glass is prohibited. Spandrel glass may be used to conceal floor systems.
- Building Cap: The building cap should consist of materials introduced on the base and/or body of the building. Cornices and parapets should be distinguishable from the building body by design and profile. Awnings, canopies, and eaves should generally incorporate alternate color and materials.

Architectural Elevations and Details

Primary Building Entries

Recognizable building entrances should be provided and highlighted through the massing of the building, as well as special architectural materials and/or design features.

Elements of a Building

Elements should relate logically to each other, as well as to surrounding buildings in order to enhance the given or potential characteristics of a particular building and area.

Large Sites with Multiple Buildings

A consistent design character and style should be developed and adhered to that provides complementary buildings, ancillary structures, and landscape elements in conjunction with these standards.

Visual Relief

Articulating details should include doorway or entry surrounds, windows, balconies, details such as horizontal bands, recessed or textured design elements, accent windows, awnings, accenting cornice treatments, exposed expansion joints, reveals, change in texture, or other methods of visual relief.

See Figure 5.0-2, *Example Building Designs*.

HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

Figure 5.0-2, Example Building Designs



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

Roofs and Parapets

Integral Part of the Building Design

Roofs should be an integral part of the building design and overall form of the structure and should relate to the general design and nature of other roofs along the street, as well as harmonize with the surrounding development.

Overall Mass

Building roofs should be designed to reduce the overall mass of a structure.

Varied Roof Lines

The use of varied roof lines, as shown in Figure 5.0-3, *Varied Roof Lines*, is encouraged. Permitted roof styles include gable, vaulted, and hip roofs. Flat roofs are permitted if sufficiently disguised through the use of parapet walls. Superficial application of artificial roof elements, such as a mansard, to disguise a flat roof, should not be used. This does not preclude roof top equipment wells when set behind conventional roof forms.

Figure 5.0-3, Varied Roof Lines



Form and Materials

Roof forms and materials should be stylistically consistent with the overall design theme of the building.

Avoid Monotony

The monotony of long and large unbroken roofs should be avoided through the use of gables, dormers, height offsets, or other architectural variations.



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

Variation in Parapet Height

Variation in parapet height should be used in conjunction with wall relief or as any distinctive feature to break a long horizontal parapet line.

Flat Roof and Parapets

Special attention should be given to the finish of parapets when buildings have flat roofs. Parapets should be finished with cornices, other horizontal decoration and/or clean edges with no visible flashing, depending on the architectural style of the buildings. Distinction must be achieved with enhancements more substantial than a paint band.

Community Entries/Special Roadways

Specific gateways and intersections within Harvest Landing have been identified to reinforce its boundaries and provide a sense of arrival. These features will reinforce the design theme for the community through a consistent or complimentary blend of hardscape, plant materials, and entry monumentation. Please refer to Figure 3.0-4 for specific locations of gateways and key intersections. The gateways are strategically located at key intersections around the boundary of the Specific Plan area. The design of these gateways should include a consistent application of elements, all within the street rights-of-way, such as landscaping, signage on one or both sides of the street, fencing/walls, and lighting at these key entrances into the community.

Lighting Posts

Lighting for the public right-of-way will be consistent throughout the Harvest Landing Specific Plan area. The design of the light posts and fixtures should be architecturally compatible with the theme of the community. The intent is to provide continuity throughout the Specific Plan area and create visual interest in the landscape. Light posts should be constructed of metal and include the Harvest Landing logo, which should be constructed from flat cutout painted aluminum and be attached to the light standard with stainless steel straps.

5.2.2 Landscaping

General Guidelines

Unspecified Uses

All areas not devoted to parking, drive isles, buildings, basins or operational areas should be landscaped and permanently maintained.

Perimeter Landscape

Buildings should have perimeter landscape, except where loading docks, plazas and entries would interrupt planting. Landscape areas should be provided on all sides of buildings visible to the public.



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

Planters and Pots

Planters and pots should be used in building recesses and adjacent to exterior walls. Pot and planter materials should complement the architectural style, texture, and color of the building and should be properly irrigated and drained. Planted pots are encouraged in the Commercial land use.

Slopes

Cut slopes are level areas in the landscape formed by cutting into a slope and adding a retaining wall to create stability while fill slopes are the surface formed from earth deposited to build a road or trail. Cut slopes that are equal to or greater than five (5) feet in vertical height and fill slopes equal to or greater than five (5) feet in vertical height should be planted with a ground cover to protect the slope from erosion and instability. Slopes exceeding three (3) feet in vertical height should all be planted with shrubs spaced not more than ten (10) feet on center or with trees spaced not to exceed 30 feet on center, or with a combination of shrubs and trees at equivalent spacing, in addition to the groundcover.

Avoid Interference with Project Lighting/Utilities/Emergency Apparatus

Landscaping should not interfere with the lighting of the Project area or restrict access to utilities (i.e. electrical boxes, meters, etc.) or emergency apparatus (i.e. fire hydrants or fire department connections).

Plant Maintenance

All specimen trees should be fine pruned after planting to allow for both vehicular and pedestrian safety.

Maintenance Intensive/Litter Producing Trees Discouraged

Trees that produce litter, are shallow rooted or have other maintenance intensive characteristics should not be used in parking areas, pedestrian plazas, or courtyards.

Plant Material Requirements and Purpose

All planting areas should be designed to be consistent with plant material horticultural requirements and work with the purpose of the planting (i.e. aesthetics, screening, wind, etc.).

Structures Wrapped by Landscaping

Exterior building sides (excluding screen loading type areas) should be grounded by landscaping. A minimum landscape strip of 5 feet should be provided between parking, sidewalks, and other paved areas adjacent to the structure.

Turf and Ground Cover Areas to be Cross Ripped

All future turf and ground cover areas should be cross ripped to a depth of 6 inches both ways through the use of a rototiller or equivalent machine. All soil amendments should be blended in and rototilled to a depth of 6 inches.



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

Landscape in Parking Lots

Minimum 50% Shade Coverage

In accordance with the City of Perris Municipal Code (Section 19.71.050), shade trees shall be provided within the vehicular parking areas to attain a minimum 50% shade coverage of the parking area within five years of planting. Parking lot shade trees should be of an evergreen variety capable of producing a large canopy to achieve this shade requirement.

Planter Islands

Planter islands should have a minimum width of eight (8) feet curb to curb, bounded on the outside by a 6-inch-high concrete curb (or its equivalent). Curb break and wheel stops may be substituted where landscaped swales adjacent to the paving are intended for water quality management purposes.

Parking Lot Screening

Parking lots should be screened from the public rights-of-way to a height of 36 inches by use of primary structures or combination of earthen berms, shrubs.. If walls are incorporated into the design, they must be aesthetically compatible with the Project design and no taller than 36 inches within the setback area, as measured from ground surface to top of wall.

One Tree per Eight Parking Spaces

A minimum of one tree per eight parking spaces should be provided within passenger vehicle parking lots and its immediate perimeter.

Concrete Curbs, Mow Strips, or Combination

Landscaping in parking lots or along drive aisles should be protected or delineated with six-inch concrete curbs, concrete mow strips, or the combination of both, as approved by the City of Perris. This requirement may be waived or modified as necessary, to mitigate water quality management requirements.

Planter Rows Between Opposing Parking Stalls or Diamond Planters

Planter rows between opposing parking stalls or diamond planters with a minimum inside width of 5-feet should be allowed for tree plantings capable of providing 50% shade coverage of the parking area, as required.

Screening

Plant Screening Maturity

Plant materials specified to be used for screening purposes such as trash enclosure, transformers or loading areas, should include plants that reach maturity within three years of installation.



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

Screenwall Planting

Screenwalls should be made aesthetically pleasing with the incorporation of plant material and vines, where applicable and feasible. Projects within the commercial land use will adhere where screen walls face public right of way.

Trash Enclosures

Trash enclosures should be visually enhanced by screening and softening with landscaping and overhead trellis treatment in the commercial land use and when in direct line of public view.

Streetscape Landscaping

Streetscapes in Harvest Landing are vital in creating a community identity and a visual hierarchy in the street classifications, theme, unification, and quality. These public areas will be the only community spaces threading through the community and will serve as unifying elements that enhance the vehicular and pedestrian experiences.

The design concept for the streetscapes is to provide regimented, identifiable, and generously landscaped areas that soften views of the buildings and parking facilities while providing an enjoyable experience. To ensure the visual and spatial continuity within Harvest Landing and aid in the identification of street classifications, the landscape design and plant material for the streetscapes has been set forth in this Specific Plan. The plant material specified includes native and appropriate non-native drought tolerant species. Trees of varying textures and heights, shrubs, decorative grasses, and groundcover should be used to buffer and separate adjacent land uses, reduce maintenance requirements, and conserve resources. The parkway area beneath trees should be planted with drought-tolerant ground covers from the above plant palettes.

Main Entries, Major Roadways, and Plazas

Landscaping along major roadways and at Project entries should create an attractive and cohesive community identity. Formal plantings of non-native species should be permitted at key entries and intersections to highlight these areas. Median plantings should reflect the use of the roadway of which they are a part. Median plantings along Perris Boulevard and Placentia Avenue should be more formal than those along Project arterials, to reflect the hierarchy and character of the roadways.

Large specimen trees should be used in main entries and plazas. Landscaped parkways should border both sides of all entry drives to create a sense of arrival, as shown in Figure 5.0-4, *Streetscape Landscaping*.



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

Figure 5.0-4, Streetscape Landscaping



Approved Streetscape Plants

Table 5.0-1, *Approved Streetscape Planting*, includes is a list of approved trees for streetscapes, organized by street type. This is a recommended palette and additional plants should be used that complement the listed plants.

Table 5.0-1, Approved Streetscape Planting

<i>Botanical Name</i>	<i>Common Name</i>	<i>Size/Spacing</i>
Primary Arterial Streets		
Trees		
Koelrueteria bipinnata	Chinese Flame Tree	24" box - 40' oc
Shrubs/Groundcover		
Dietes bicolor	Fortnight Lily	5 gal - 36" oc
Rhaphiolepis Clara	Indian Hawthorn	1 gal - 36" oc
Arctotis a. 'Big Magenta'	African Daisy	1 gal - 24" oc
Secondary Arterial Streets		
Trees		
Chitalpa tashkentensis	Chitalpa	24" box - 30' oc
Quercus agrifolia	Coast Live Oak	24" box - 40' oc
Cercidium 'Desert Museum'	Blue Palo Verde	24" box - 30' oc
Shrubs/Groundcover		
Dietes bicolor	Fortnight Lily	5 gal - 36" oc
Trachelosperum jasminoides	Star Jasmine	1 gal - 24" oc



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

<i>Botanical Name</i>	<i>Common Name</i>	<i>Size/Spacing</i>
Tulbaghia violaceae	Society Garlic	1 gal - 24" oc
Arctotis a. 'Big Magenta'	African Daisy	1 gal - 24" oc
Hemercallis hybrid Orange	Day Lily	1 gal - 24" oc
Collector Road		
Trees		
Platanus acerifolia	London Plane Tree	24" box @ - 35' oc
Shrubs/Groundcover		
Dietes bicolor	Fortnight Lily	5 gal - 36" oc
Carex Pansa	California Meadow Sedge	1 gal - 18" oc
Private Road		
Chitalpa Tashkentensis	Chitalpa	24" box - 40' oc
Quercus Agrifolia	Coastal Live oak	24" box - 40' oc
Planted Medians		
Trees		
Pinus eldarica	Afghan Pine	24" box - 40' oc
Cercidium 'Desert Museum'	Blue Palo Verde	24" box - 30' oc
Olea wilsonii	Wilson Olive	24" box - 30' oc
Shrubs/Groundcover		
Tulbaghia violaceae	Society Garlic	1 gal - 24" oc
Carex Pans	California Meadow Sedge	1 gal - 18" oc
Hemercallis hybrid Orange	Day Lily	1 gal - 24" oc
Rhaphiolepis Clara	Indian Hawthorn	1 gal - 36" oc

Scale and Size of Landscaping

Landscaping should be kept in scale with adjacent buildings and should be maintained at an appropriate size at maturity.

All areas required to be landscaped should be planted with groundcovers, shrubs, or trees selected from the plant palette contained in this section. The material should be planted in the following sizes and in accordance with all City of Perris standards and minimum requirements:

- **Trees:** Twenty-five percent (25%) of the site trees (excluding all street and screen trees) provided should be a minimum 24-inch box size. The balance of the trees should have a minimum size of 15 gallons.
 - All **15-gallon trees** should be staked with two pressure-treated lodge pole tree stakes that are eight-feet in length and two-inches in width. An equivalent staking material may be used in the same dimensions if approved by the Planning Department.
 - All **24-inch box trees** should be staked with two pressure-treated lodge pole tree stakes that are eight-feet in length and two-inches in width. An equivalent staking material may



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

be used in the same dimensions if approved by the Planning Department. Larger trees should be guy-wired per City of Perris standards.

- **Street Trees:** Street trees must be a minimum **36-inch box** size.
- **Shrubs:** The majority of all shrubs used should have a minimum size of 1 gallon. Smaller shrubs may be used where rapid growth characteristics warrant.

5.2.3 Outdoor Furnishings

Employee Break Areas and Amenities

The Harvest Landing Specific Plan will provide an outdoor amenity area for all MBU and Commercial Employees. The area designated as Water Quality Maintenance land use will be designated for employees to use as a break area. Employees are encouraged to take their breaks walking the WQMP along the installed permitted walkways and enjoying their lunch outdoors on the provided outdoor seating areas.

On-Site-Outdoor Break Areas

Employee break areas should include an eating area (tables and seating) covered by overhangs, patio covers, pergolas, etc. This area should be designed to create a sense of privacy and separation using enhanced landscaping and paving, as well as landscape screening/low garden walls or combination thereof.

Additional Amenities for Buildings Exceeding 100,000 S.F.

Buildings exceeding 100,000 square feet should require employee amenities such as cafeterias, exercise rooms, locker rooms and shower, walking trails and recreational facilities.

Connection to Adjacent Amenities

Site design should consider pedestrian access when adjacent to, trails, or other community amenities.

Site Furnishings

Site furnishings such as benches, tables, trash receptacles, planters, tree grates, kiosks, drinking fountains, and other pedestrian amenities should be integral elements of the building and landscape design, and placed in plazas, at building entrances, open spaces and other pedestrian areas to create a more pedestrian friendly environment. Site furnishings exceeding three feet in height should not block pedestrian access or visibility to plazas, open space areas and/or building entrances and should be made of durable, weather-resistant and vandal-resistant materials. Site furnishings should be depicted on all site plans and landscape plans.

Figure 5.0-5, *Bench Examples*, and 5.0-6, *Planter Examples*, show examples of site furnishings.

HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

Figure 5.0-5, Bench Examples



Figure 5.0-6, Planter Examples





HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

ATM and Vending Machines

ATM machines, and reverse vending machines should be incorporated into the site design and, to the extent possible, compatible with the design, colors, or style of the structure. Exterior placement of vending machines is discouraged.

5.2.4 Walls and Fencing

Specific Purpose

Walls and fences are generally used for security purposes and to screen areas from public view. Although walls may be necessary, their design should provide variety and visual interest. If there is not a specific purpose for their use, they should not be utilized.

Walls and fences should comply with the following guidelines in addition to the requirements outlined in section 19.02.040.B, Restriction for Walls, Fences, and Hedges, of the Municipal Zoning Code:

- Solid walls and fences should not dominate the street scene in the residential and commercial zones. They should only be used when necessary for noise attenuation, privacy, and shielding of incompatible adjacent uses. The MBU zoning allows for screen walls to shield operations from public right-of-way.
- View fences (which are partially solid and see-through material such as wrought iron) provide a visually attractive alternative to solid walls and fences. They allow for safety and privacy while preserving views and creating more visually appealing neighborhoods within the Residential, MBU and Commercial zones. View fences should be used instead of solid walls when feasible.
- Wall faces that are visible to the public should be constructed of attractive materials and finished with architectural detailing or articulation. The incorporation of high-quality materials and surface articulation are strongly encouraged.
- Walls and/or wall surfaces not visible to the public do not need the same high level of design detail.
- Pilasters should be incorporated into wall design dictated by the planning department.
- Trees, vines, and landscaping should be used to soften the visual appearance of the walls. Intermittent undulation of walls will help to create a varied street scene and is strongly encouraged.
- Walls and fences should be made of durable and yet attractive materials that complement the adjacent architecture and are resistant to graffiti. When solid walls are necessary, split-face block, slumpstone brick, stone, concrete tilt, or materials with similar visual qualities should be used. Likewise, view fences should incorporate visually attractive materials such as tubular steel, wrought iron, and stone (or faux-stone).
- Long, monotonous walls are to be avoided. Walls should be undulated with breaks, recesses, and offsets, and columns, especially at entries and important intersections. Long walls should be made more attractive and visually interesting through the incorporation of surface articulation and pilasters.



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

Materials

Walls and fences should be designed and constructed of materials similar to and compatible with the overall design character and style of the development. Permitted materials include split-face masonry, stone veneer, brick, slump, block, concrete-tilt, wrought iron or tubular steel, as well as a combination of wrought iron and tubular steel with masonry columns.

Avoid Long Expanses of Monotone Fence/Wall Surfaces

Long expanses of fence or wall surfaces should be architecturally designed to prevent monotony. Design features should include:

- Varied heights, wall plain offsets, and angles.
- Pilasters or distinctive elements.
- Accent capping, trim, reveals.
- Changes of material and finishes where appropriate.
- Trellis/vine panels, landscape pockets.
- Decorative painting designed to complement the building's architectural elements.

Most Walls Not Permitted within Street Side Landscaping Setback

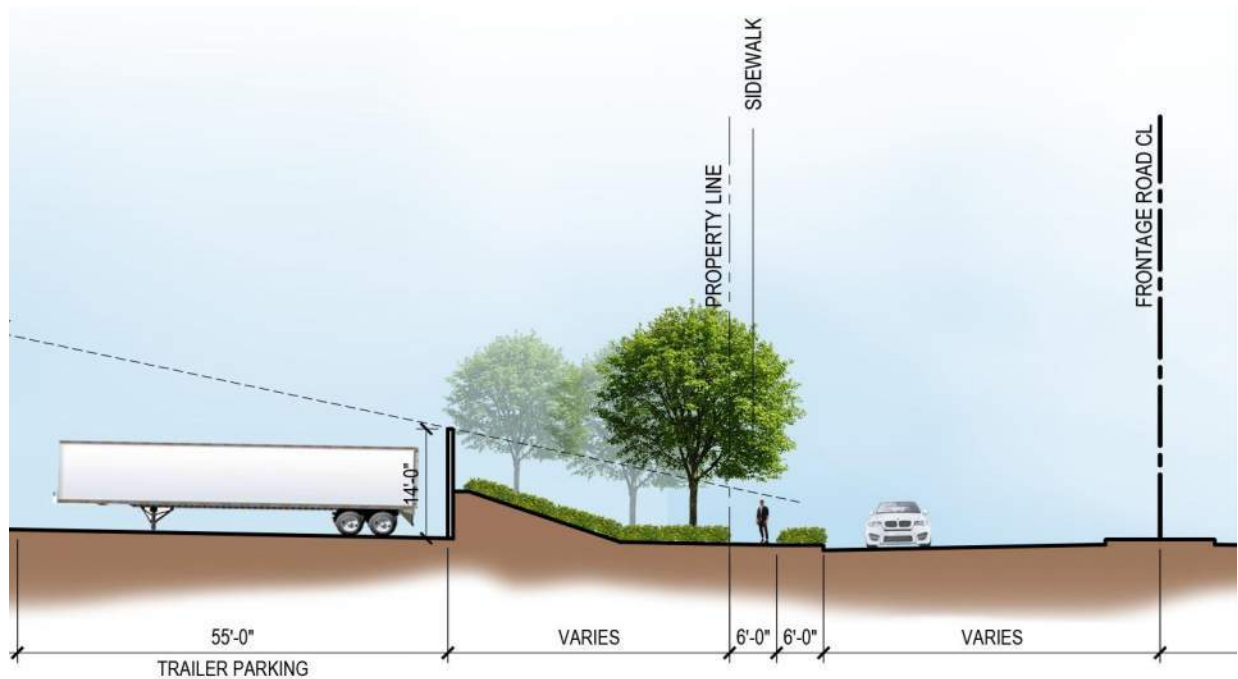
Low-profile parking lot screen walls or garden walls within street-side landscaping setbacks will be limited to a street side visible height of 36 inches. When security fencing is required along the street side landscape setback area, it should be constructed of wrought iron, tubular steel or similar material supported by masonry columns.

Height

Effectively soften screen wall height and mass with earthen berms and dense landscaping as shown in Figure 5.0-7, *Screen Wall with Berming*. The intent is to give walls the appearance of being as low and unobtrusive as possible while performing their screening and security functions. The height of screen walls along street frontages should not exceed the maximum height necessary to effectively serve their purpose and should not appear to exceed a height of 8 feet when viewed from the public right-of-way unless otherwise approved by the City Planning Manager, and in no case should the wall/structure itself exceed 14 feet.



Figure 5.0-7, Screen Wall with Berming



Walls and fences should comply with the following guidelines in addition to the requirements outlined in section 19.02.040.B, *Restriction for Walls, Fences, and Hedges*, of the Municipal Zoning Code.

5.2.5 Lighting

Low wattage down-lighting should be used on commercial buildings, provided all exterior lighting complies with Riverside County Ordinance No. 655 regulating light pollution and its detrimental impact on astronomical observation and research.

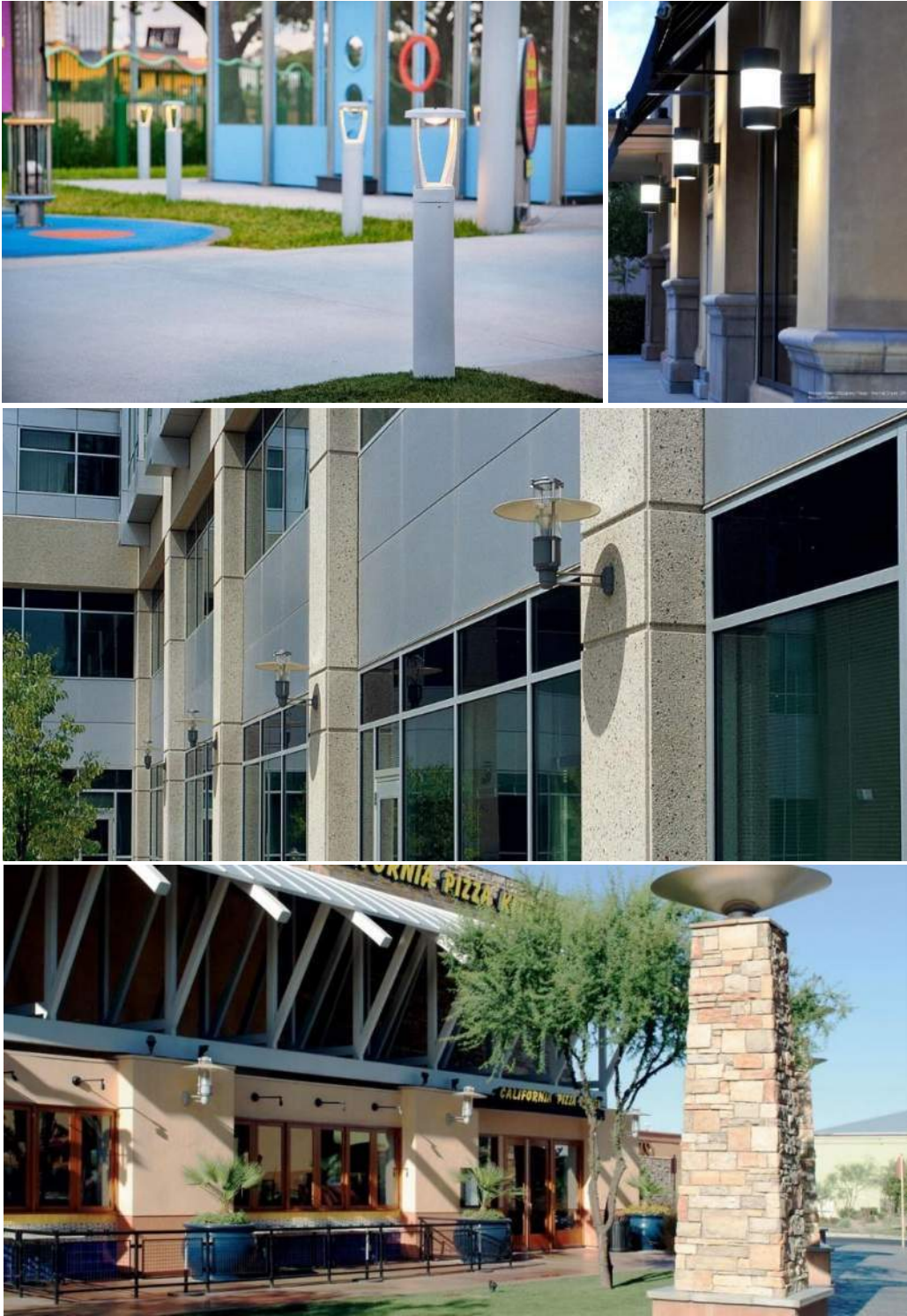
Complimentary Lighting Fixtures

Lighting should contribute to the overall character of the surrounding community, site architecture, or other site features. The fixtures should complement the furnishings, as well as other lighting elements used throughout and surrounding the site, such as pedestrian pathway lighting, and lighting used in adjacent site amenities and the public right-of-way. See Figure 5.0-8, *Complimentary Lighting Fixture Examples*.

Any illumination, including free standing or wall-mounted lighting, for security, loading docks, parking areas, or internal roads should utilize full-cut-off fixtures, and be directed downward and away from adjoining properties and public right-of-way (i.e., bulb/source is not visible above the horizontal plane).

HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

Figure 5.0-8, Complimentary Lighting Fixture Examples



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

Monumentation Lighting

Lighting for entry monumentation should illuminate the sign graphics and gently wash the components of the signage with light.

Compatible with Architecture

Lighting should be architecturally compatible with the building and site design, as shown in Figure 5.0-9, *Lighting Examples*. These lights should be low profile and in scale with the setting and can include post lights and light bollards.

Figure 5.0-9, *Lighting Examples*



Down-Lighting

Where appropriate, down-lighting should be used on exterior elevations and landscaping as part of the overall architectural style of the building, accenting and highlighting interesting architectural and landscape architectural features. See Figure 5.0-10, *Down-Lighting Examples*.

HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

Figure 5.0-10, Down-Lighting Examples



Accent Lighting

The use of accent lighting is encouraged but should be combined with functional lighting to highlight special focal points, building/site entrances, public art and special landscape features.

High Intensity Lighting

Sites requiring high intensity lighting where high visibility and color retention are important are required to switch to an alternative low-level lighting of these areas from 11 p.m. until daylight.

Lighting Fixtures Shield

All lighting fixtures should be fully shielded with cut-off fixtures so that there is no glare emitted onto adjacent properties or above the lowest part of the fixture. Figure 5.0-11 shows more examples of shielded lighting fixtures.

Figure 5.0-11, Lighting Fixture Examples



Foot-candle Requirements Sidewalks/Building Entrances

Sidewalks should have a minimum of 2 foot-candlepower of light across their surface. Building entrances and parking lots should have a minimum of 1 foot-candlepower of light. Lighting



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

standards should be energy efficient. Based on Mt. Palomar Observatory's Dark Sky Ordinance, all projects will be conditioned to use low pressure sodium.

Outdoor Lighting

All outdoor lighting and utilities, including spotlights, floodlights, electrical reflectors and other means of illumination for signs, structures, landscaping, and similar areas, should be made of metal, unbreakable plastic, recessed, or otherwise designed to reduce the problems associated with damage and replacement of fixtures. Fixtures should be vandal proof. Fixtures should be anchored with concrete footing if low voltage lighting is used.

Decorative Lighting Guidelines

Lighting should primarily serve the purpose of nighttime safety and security. However, when used creatively, it can enhance the visual appeal of a structure, highlight points of interest, and define open spaces and pathways. Careful consideration should be given to ensure that lighting achieves these objectives without negatively impacting adjacent developments, roadways, or residences. Figure 5.0-12 shows examples of post lighting used to illuminate walkways or parking areas.

Figure 5.0-12, Post Lighting Examples



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

5.2.6 Color Palette

Facades

Low reflectance, subtle, neutral, or earth tone colors as the predominant colors should be used on building facades, as shown in Figures 5.0-13 and 5.0-14.

Building Trim and Accent Areas

Building trim and accent areas may feature brighter colors within the color palette of the building. Applied paint over brick or stone on any part of a building facade or other site elements is discouraged.

Figure 5.0-13, Façade Examples



Figure 5.0-14, Façade Example





HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

5.2.7 Access/Parking

Internal Pedestrian Walkways

Internal walkways should provide connection between building entries, plazas, and courtyards within the Project and be covered when possible. Enhanced paving should also be provided in plazas, and courtyards within the Multiple Business Use and Commercial land uses.

Paving For Walkways Visible from Public Rights-of-Way/Public Access

Enhanced paving should be used in areas visible from public rights-of-way or utilized for public access to define business entries, pedestrian walkways, and within plazas and patios.

Pedestrian Access Between Buildings/Parking Areas/Amenities On/Off-Site

Parking areas should be designed with pedestrian walkways which link the buildings to the street sidewalk system, creating an extension of the pedestrian environment. This can be accomplished by using design features such as walkways with crosswalks noted, enhanced paving, trellis structures, and/or landscape treatment.

Access Points Easily Identifiable

Entry drives should be easily identifiable through the use of enhanced landscaping and special pavements (accent colors, textures, and patterns). Signage should also be used to identify customer and service entrances. Driveways used exclusively for deliveries or loading activities are excluded. See Figure 5.0-15, *Enhanced Driveway Features*.

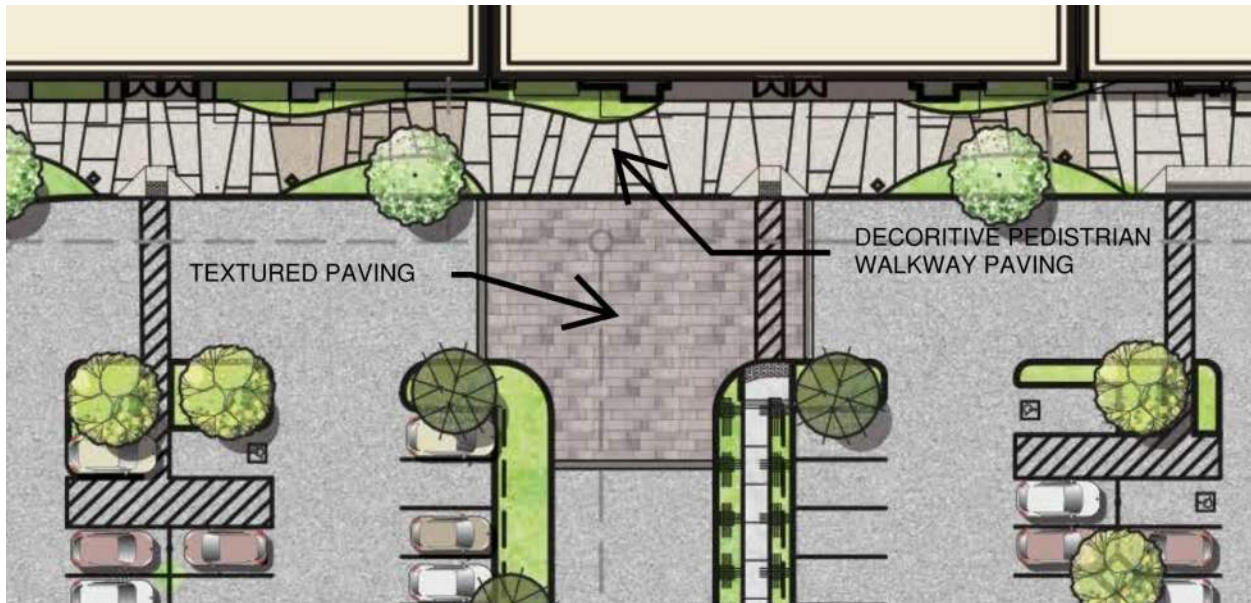
Figure 5.0-15, Enhanced Driveway Features



Limited Store Front Parking

To promote the visibility of the business, store parking should be limited as shown in Figure 5.0-16, *Pedestrian Access and Drop-Off*. Should store front parking be provided, landscaping treatments should be required to provide a more visually appealing store front and parking should be limited to the greatest extent possible. Site design should address the intended functions of the facility beginning with safe, definable site access that creates a sense of arrival.

Figure 5.0-16, Pedestrian Access and Drop-Off



Shared Access

Shared driveway access for passenger vehicles should be utilized wherever feasible. Reciprocal ingress/egress access easements should be provided to enhance circulation and parking, facilitating smooth vehicular movement between properties and minimizing the number of access points to adjoining streets. In areas where shared access could lead to confusion or congestion for trucks and passenger vehicles, separate driveway access will be implemented.

Avoid Conflicts

To enhance safety and ensure efficient site circulation, it's important to minimize conflicts between trucks, cars, and pedestrians through careful planning and design, such as the following measures that should be incorporated into Project design:

- Design pedestrian walkways to avoid conflicts with vehicle circulation by utilizing separated pathways for direct access from public rights-of-way and parking areas to building entrances.
- Incorporate internal pedestrian linkages throughout the site to ensure safe and convenient movement between different areas.
- Provide separate circulation routes for trucks, cars, and pedestrians to reduce the likelihood of conflicts and enhance safety.
- Place loading areas and dock facilities in locations that minimize interaction between trucks and visitor/customer vehicles.
- To the greatest extent feasible, separate access to loading and delivery areas from parking areas designated for visitors and customers.



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

5.2.8 Public Art

Public art is often used as a means of creating civic pride. It is a means in which to engage a broad and diverse spectrum of people. Typically, public art is used to recognize the city and/or its components by use of historic monuments or displays. Public art adds value to both public and private development as well as infrastructure by creating a sense of community, and can be educational and promote social gathering and interaction. It also becomes an identifiable point amongst the urban environment.

Professional Artist/Location

Public art should be created by a recognized, professional artist and shall be subject to approval by the Development Services Director. Selection criteria should include artistic merit, broad experience as a professional artist, references, experience applicable to the type of project and interest in and understanding of the City and surrounding area. Public art should be properly located so as to receive proper recognition by the viewing public.

5.3 Multiple Business Use (MBU)

5.3.1 Architecture

Developments within the MBU land use should prioritize thoughtful architectural design that enhances both functionality and visual appeal. Buildings should incorporate design elements that break up large, monotonous facades and contribute to a cohesive yet dynamic built environment. A combination of massing, material variation, and architectural detailing should be used to create visually engaging structures that integrate well within the surrounding business park context. The following guidelines outline key architectural principles to ensure high-quality design within the MBU area. In addition, developments should adhere to the guidelines set forth in section 5.2.1 Architecture.

Scale, Massing, and Building Relief

Building and site development should incorporate an architectural component that enhances the Project's identity. Primary building entries should be highlighted through the massing of the building, as well as special architectural materials and/or design features.

Buildings within the MBU land use will follow Section 5.2 Community Wide Design Guidelines. The MBU area allows for business park and manufacturing uses, buildings of these uses should:

Avoid Long, Monotonous, and Unbroken Building Facades

MBU buildings should avoid long, monotonous facades by incorporating varied depths, projections, or material changes. For facades over 100 feet, recesses or projections ranging from one to ten feet in depth are encouraged. Paint and texture variations should be used to break up long stretches and add visual depth.



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

Discernable Body, and Cap

MBU buildings over 20 feet in height should strive to have a clearly discernable base, body, and cap. The cap should consist of a cornice, parapet, awning canopy or eave. The base and cap should be clearly distinguishable from the body through changes in color, material, pattern, profile or texture. Figures 5.0-17 and 5.0-18 provide examples for reference.

Varied Roof Lines

The use of varied roof lines is encouraged. Flat roofs are permitted if sufficiently disguised through the use of parapet walls. Superficial application of artificial roof elements, such as a mansard, to disguise a flat roof, should not be used. This does not preclude roof top equipment wells when set behind conventional roof forms.

Figure 5.0-17, MBU Architecture Illustration



Figure 5.0-18, MBU Architecture Illustration



Orientation/Placement of Business Park and Manufacturing Operations

Business park, manufacturing operations and truck courts should be screened from the public view and oriented away from residential uses, according to required setbacks.

5.3.2 Landscaping

The landscape plant palette presents permitted species and minimum plant or box sizes for landscaping in different locations within the Specific Plan area. The plant palette was selected to complement and enhance the thematic setting for the community, appropriateness to climatic and soil conditions, ease of maintenance, and water conservation. All landscape plans and installations shall adhere to City design guidelines, codes, and regulations. Plant material should include those listed in the plant palette below, or shall otherwise be approved by the City.

If approved by the City of Perris, plants should be consistent with California Friendly Landscape and meet all minimum City of Perris Water Conservation Requirements as defined in Development Code Chapter 19.70, *Landscaping*, including but not limited to:

- Use of drought-tolerant plants.
- Use of landscaped areas designed to retain irrigation water.
- Use of satellite-based irrigation timers.
- Use of automatic irrigation systems.
- Use of plant groupings with similar irrigation requirements to reduce over-irrigation.
- Extensive use of mulch in landscaped areas.
- Installation of drip irrigation systems, where appropriate.
- Limited use of turf for active purposes only.
- Limited use of impervious surfaces.

Tables 5.0-2 below lists the plant palette for Multiple Business Use land use within Harvest Landing. This is a recommended palette and additional plants should be used that complement the listed plants.



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

Table 5.0-2, On-Site Plant Palette – MBU Land Use

<i>Botanical Name</i>	<i>Common Name</i>
Trees	
Cercidium 'Desert Museum'	Blue Palo Verde
Chilopsis linearis	Desert Willow
Chitalpa tashkentensis	Chitalpa
Olea europaea	Olive
Pinus canariensis	Canary Island Pine
Pinus eldarica	Afghan Pine
Platanus acerifolia	London Plane
Prosopis chilensis	Chilean Mesquite
Quercus agrifolia	Coast Live Oak
Schinus molle	California Pepper
Ulmus parviflora	Chinese Elm
Tristania conferta	Brisbane Box
Shrubs	
Baccharis p. 'Centenial'	Coyote Bush
Callistemon 'Little John'	Dwarf Bottle Brush
Cassia phyllodenia	Silverleaf Cassia
Dietes bicolor	Fortnight Lily
Ligustrum j. Texanum	Texas Privet
Muhlenbergia capillaris	Pink Muhly
Muhlenbergia rigens	Deer Grass
Rhamnus californica	Coffeeberry
Rosmarinus o. 'Tuscan Blue'	Rosemary
Salvia c. 'Allen Chickering'	Allen Chickering Sage
Salvia leucantha	Mexican Sage
Westringia fruticosa	Coast Rosemary
Westringia f. 'Grey Box'	Dwarf Coast Rosemary
Groundcover	
Acacia redolens 'Low Boy'	Dwarf Acacia
Baccharis p. 'Pigeon Point'	Dwarf Coyote Bush
Carissa m. 'Green Carpet'	Prostrate Natal Plum
Hemerocallis hybridus-Yellow	Yellow Day Lily
Lantana 'Gold Mound'	Yellow Lantana
Lonicera j. 'Halliana	Hall's Honeysuckle
Myoporum parvifolium	Myoporum
Rosmarinus o. 'Huntington Carpet'	Prostrate Rosemary
Trachelospermum jasminoides	Star Jasmine

Planter Rows Between Opposing Parking Stalls or Diamond Planters

Diamond planters should use rock or mulch coverings. Planter rows between opposing parking stalls or along perimeter landscape buffers may be designed as vegetated swales for utilization



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

as infiltration trenches for run-off, as a method of pollutant mitigation to manage water quality. These areas should be designed without curbs where wheel stops are provided.

5.3.3 Outdoor Furnishings

Employee Break Areas

The Harvest Landing Specific Plan will provide an outdoor amenity area for all MBU and Commercial Employees. The area designated as WQMP will be designated for employees to use as a break area. Site and landscape plans for the WQMP area should reserve open space for outdoor recreation and dining. Employees should be encouraged to take their breaks walking the WQMP along the installed permitted walkways and enjoying their lunch outdoors on the provided outdoor seating areas.

Business Parks and manufacturing uses should provide a shared outdoor break area. It should include tables and seating covered by overhangs and patio covers. This area should be defined to create a sense of privacy from public and separation

Employee Break Area Amenities and Elements

Enhanced employee break areas should be scaled accordingly to the size and demands of the particular user or facility and that demonstrate a higher level of design treatments by incorporating seating, sculpture, trash receptacles, ash urns, pedestrian scaled lighting enhancements and other furnishings as appropriate for the specific user. Plazas and open space areas should provide both a friendly and inviting vision and environment by incorporating some of the following elements:

- Architectural features and furnishings adhering to a consistent theme.
- Seating, such as benches, tables, and chairs, and/or low seating walls.
- Enhanced paving using a combination of textures and patterns, site walls including tree grates.
- Decorative light fixtures. Pedestrian scale, bollard, or other accent lighting. Note enhanced walkway lighting does not act as sole lighting.
- Landscaping of special interest, landscape buffering, screen walls, trellises, pergola structures and large-scale canopy trees.
- Public art or other focal point amenity. Public art is highly encouraged and incentivized by the City.

Amenities for Buildings Exceeding 100,000 Square Feet

Business/Professional Offices exceeding 100,000 square feet shall require employee amenities such as, but not limited to, cafeterias, exercise rooms, locker rooms and shower, walking trails, recreational facilities, per Section 19.44.080(b)(4) of the Municipal Code.

Connection to Adjacent Amenities

Site design should include provisions for pedestrian access when adjacent to area wide open space, trails, or other community amenities.



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

5.3.4 Walls and Fencing

Developments within the MUB land use should utilize the guidelines in Section 5.2.4 Walls and Fencing.

Specific Purpose

Walls and fences are generally used for security purposes and to screen areas from public view. Within the MBU there are many allowed uses that will be required to screen operations from public right-of-way. It is important that these guidelines are followed to ensure operations are screened and the consistency within Harvest Landing.

Walls and fences should comply with the following guidelines in addition to the requirements outlined in section 19.02.040.B, Restriction for Walls, Fences, and Hedges, of the Municipal Zoning Code.

Screen Walls

Materials

Screening walls within the MBU should be designed and constructed of materials similar to and compatible with the overall design character and style of the development. Permitted materials include split-face masonry, stone veneer, brick, slump, block and concrete-tilt.

Avoid Long Expanses of Monotone Wall Surfaces

Long expanses wall surfaces for screening should be architecturally designed to prevent monotony. Design features should include:

- Varied heights, wall plain offsets, and angles.
- Pilasters or distinctive elements.
- Accent capping, trim, reveals.
- Changes of material and finishes where appropriate.
- Trellis/vine panels, landscape pockets.
- Decorative painting designed to complement the building's architectural elements.

Height

Wall height and mass should be screened by using earthen berms and dense landscaping as shown previously in Figure 5.0-7. The intent is to give walls the appearance of being as low and unobtrusive as possible while performing their screening and security functions. The height of screen walls along street frontages should not exceed the maximum height necessary to effectively serve their purpose and should not appear to exceed a height of 8 feet when viewed from the public right-of-way unless otherwise approved by the City Planning Manager, and in no case should the wall/structure itself exceed 14 feet.



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

5.3.5 Lighting

Developments within the MBU land use should utilize the guidelines in Section 5.2.5 Lighting when designing projects.

Accent lighting, including uplighting and downlighting should be incorporated into MBU developments to enhance and complement the overall architectural style of the building, and highlight interesting architectural and landscape architectural features. Figure 5.0-19, *MBU Lighting Example*, provides an example of anticipated lighting within developments.

Figure 5.0-19, MBU Lighting Example



5.3.6 Color Palette

MBU building colors should be consistent within the land use and focus on low reflectance, subtle, neutral, or earth tone colors as the predominant colors should be used on building facades, as shown in Figure 5.0-20, *MBU Façade Examples*. Developments within the MBU land use should utilize the guidelines in Section 5.2.6 Color Palette when designing projects.

Figure 5.0-20, MBU Façade Examples



5.3.7 Access/Parking

The circulation for the MBU land use within the Harvest Landing Specific Plan is designed to ensure safe, efficient, and orderly movement of both commercial and passenger vehicles.

Truck Access

To maintain clear separation between different types of traffic, truck entry and circulation will be entirely segregated from passenger vehicle parking areas. Dedicated truck entrances will be provided, ensuring that all commercial vehicles access the site through separate driveways. There will be no comingling of truck and passenger vehicle driveways and parking areas. This approach reduces the risk of conflicts between heavy vehicles and passenger cars, thereby enhancing safety for both drivers and pedestrians.

Parking

Passenger vehicle parking will be located in designated areas with independent access points. Signage, striping, and barriers will further reinforce this separation throughout the Project site. The circulation design aligns with regional transportation objectives while supporting the Project's



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

operational needs, ensuring smooth traffic flow and compliance with all applicable local and state regulations.

Pedestrian Access and On-Site Circulation

Internal Pedestrian Walkways

Enhanced paving should be used on internal walkways to provide connections between building entries and to the public right-of-way. Enhanced paving should also be provided in plazas, and courtyards within the MBU land use area.

Pedestrian Access Between Buildings/Parking Areas/Amenities On/Off-Site

Pedestrian walkways should be embellished and defined by landscaping, trees, lighting, textured paving, and/or trellises.

Walkways through Parking Lots

Pedestrian walkways through parking lots of MBU land use developments should be accented with distinctive design features such as raised, colored and/or textured pavement, a widened roadway, or a combination of the former.

5.3.8 Outdoor Storage

In the Multiple Business Use area outdoor storage and/or operations are permitted with the requirement that all outdoor storage should be screened from public view. All other screening requirements will be per Section 19.02.070, Screening, of the Municipal Zoning Code.

Outdoor Display

In the Multiple Business Use area outdoor display and/or operations should be screened from public view. All other screening requirements will be per Section 19.02.070, Screening, of the Municipal Zoning Code.

Outdoor display area of products covering less than 5% of the lot area is allowed upon approval of a Minor Development Plan Review by the Planning Department pursuant to Chapter 19.54. Outdoor display area of products covering more than 5% of the lot area is allowed upon approval of a Conditional Use Permit.

Outdoor Storage of Trucks Permitted

The Harvest Landing Specific Plan envisions a successful mix big box commercial, business park research and development, storage, restaurants, and service commercial. As such, the underlying economic driver is the movement of goods and services with truck transportation storage as the integral use of many businesses. To this extent, outdoor storage of trucks is permitted by right but should be regulated during the site review process with all proposed outside truck storage shown on the site plan and properly screened through the use of building placement and orientation,



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

building offsets, connecting wing walls, perimeter site walls and fences, landscaping, and berming.

5.4 Commercial/Retail

The following guidelines aim to ensure high-quality design, encouraging diversity in scale, materials, and architectural details to create vibrant and pedestrian-friendly commercial spaces.

5.4.1 Architecture

Commercial developments should incorporate architectural elements that enhance visual appeal, promote walkability, and create a sense of place. Thoughtful site planning, building orientation, and facade articulation contribute to a cohesive and inviting streetscape. Entrances should be well-defined and easily identifiable, while massing and geometric variation should be used to break up large building forms.

Developments should adhere to the guidelines set forth in Section 5.2.1 Architecture.

Site Design and Site Layout

Building Entrances

Defined and recognizable building entrances should be provided to ensure they can be differentiated from other facade enhancements, as shown in Figure 5.0-21, *Commercial/Retail Building Entrance Example*. Items such as roof lines and building materials should be varied to discern between a window and an entry.

Primary building entries should be highlighted through the massing of the building, as well as special architectural materials and/or design features. Greater height can be used to highlight and accentuate entries in the form of tower elements, tall voids, or entry meeting plazas.

Figure 5.0-21, Commercial/Retail Building Entrance Example





HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

The following guidelines are intended to enhance the relationship between buildings and public streets, creating a more inviting and functional streetscape:

- Encourage building frontages and entrances to be oriented toward the public right-of-way to enhance the streetscape and pedestrian experience.
- Position buildings so that entrances and access points are easily visible from a distance, benefiting both pedestrians and vehicular traffic.
- Reinforce building entries with architectural materials and landscape features to ensure they are clearly identifiable and welcoming.
- Where possible, locate loading areas and employee parking lots to the side or rear of buildings to minimize their visibility from public spaces.

Primary Entry Drive

The primary entry drive should be oriented toward the main entrance of the building.

Promote Walkability

Promoting walkability and circulation is encouraged through placement of buildings and pedestrian circulation facilities.

Distinct Visual Link

A well-designed entry should offer a visual link to the building and entry through the use of business signs, paving, and landscaping. Establish a distinct visual link in multi-building complexes by using architecture, landscape, site design elements and pedestrian connections to unify the Project.

Create Diversity and Sense of Community

Long, monotonous building facades should be avoided. Site design should create diversity and a sense of community by clustering buildings around courtyards, plazas, and landscaped open spaces. Figure 5.0-22, *Commercial Façade Example*, below shows how monotony can be avoided.

Figure 5.0-22, Commercial Façade Example



Utilize Building for Screening

Utilize building placement, accented walls, or unique design to effectively screen views of loading docks, storage areas, and/or outdoor work areas that would otherwise be visible to public view.



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

Plazas Required for Over 200,000 S.F. Commercial Centers

Commercial centers over 200,000 square feet require a plaza of at least one (1) square foot per 500 square feet of building area.

Outdoor Seating Area (Over 10,000 S.F. Building Area)

Covered outdoor seating areas accessible to patrons should be provided for retail and food service areas over 10,000 square feet of building area.

Scale, Massing, and Building Relief

Geometric Variation

This element should be used to break the monotony of the common rectangular box form by incorporating a variation of elements that include but are not limited to: rounded and clipped corners; trapezoidal and cylindrical entry towers; concave/convex wall projections; freeform or multi-faceted building footprints.

Windows and Storefronts

Windows and storefronts should be designed as defined, offset, openings within a solid wall rather than large unbroken expanses of a flush wall and windowpane. Large-scale openings in walls with inserted glass walls may be appropriate for entry conditions from plazas. Highlighting windows is encouraged through projections, trim or lentil elements.

Attractive Facades

Attractive facades should be provided through careful detailing, especially at the base of buildings, along eaves, and parapets and around entries and windows.

See Figures 5.0-23 and 5.0-24, *Example Building Designs*.

Figure 5.0-23, Example Building Designs



Figure 5.0-24, Example Building Designs



Avoid Single, Large Dominant Building Mass

A single, large, dominant building mass should be avoided to the extent feasible. Specifically, horizontal masses should not exceed a height to width ratio of 1:3 without substantial variation in massing that includes a change in height and projecting or recessed offsets.

Recess Second or Subsequent Floors

Second or subsequent floors should be recessed and include balconies or outdoor space.

Windows Glazing

Window glazing used in commercial development should permit views into the establishment. Use of highly reflective and spandrel glass is strongly discouraged.

Commercial Plaza Design Guidelines

Enhanced plazas areas should exhibit a higher level of design treatments that incorporate seating, sculptures and other appropriate forms of public art, trash receptacles, ash urns, pedestrian scaled lighting enhancements, and other furnishings as appropriate for the specific user. Plazas should provide a friendly and inviting vision and environment by incorporating some of the following elements as depicted in Figure 5.0-25, *Commercial Plaza Examples*:

- Enhanced visitor area(s) (i.e., a plaza, patio, courtyard, linear promenade, terrace, or usable landscaped area) should be scaled accordingly to the size and demands of the particular user or facility.
- Architectural features and site furniture, adhering to a consistent theme.
- Seating, such as benches, tables and chairs, and/or low seating walls.
- Enhanced paving using a combination of textures and patterns, site walls including tree grates.
- Decorative light fixtures and pedestrian scale, bollards and other accent lighting. Enhanced walkway lighting should not act as sole lighting.
- Landscaping of special interest, landscape buffering, screen walls, trellises, pergola structures and large-scale canopy trees.

HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

- Public art or other focal point amenity. Public art is highly encouraged and incentivized by the City.
- Plazas should be sheltered and buffered as much as possible from the sun, noise and traffic of adjacent streets, trash receptacles, parking, loading areas, or other incompatible land uses.

Figure 5.0-25, Commercial Plaza Examples





HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

5.4.2 Landscaping

The landscape plant palette presents permitted species and minimum plant or box sizes for landscaping in different locations within the Specific Plan area. The plant palette was selected to complement and enhance the thematic setting for the community, appropriateness to climatic and soil conditions, ease of maintenance, and water conservation. All landscape plans and installations shall adhere to City design guidelines, codes, and regulations. Plant material should include those listed in the plant palette below, or shall otherwise be approved by the City.

If approved by the City of Perris, plants should be consistent with California Friendly Landscape and meet all minimum City of Perris Water Conservation Requirements as defined in Development Code Chapter 19.70, *Landscaping*, including but not limited to:

- Use of drought-tolerant plants.
- Use of landscaped areas designed to retain irrigation water.
- Use of satellite-based irrigation timers.
- Use of automatic irrigation systems.
- Use of plant groupings with similar irrigation requirements to reduce over-irrigation.
- Extensive use of mulch in landscaped areas.
- Installation of drip irrigation systems, where appropriate.
- Limit use of turf for active purposes only.
- Limit use of impervious surfaces.

Tables 5.0-3 below lists the plant palette for Commercial land use within Harvest Landing. This is a recommended palette and additional plants should be used that complement the listed plants.

Table 5.0-3, On-Site Plant Palette – Commercial Land Use

<i>Botanical Name</i>	<i>Common Name</i>
Trees	
Chilopsis linearis 'Bubba'	Desert Willow
Cercis c. 'Forest Pansy'	Forest Pansy Redbud
Geijera parviflora	Australian Willow
Lagerstroemia h. 'Tuscarora'	Hybrid Crape Myrtle
Lagerstroemia i. 'Natchez'	White Crape Myrtle
Olea eruopaea 'Wilsonii'	Fruitless Olive
Pinus eldarica	Mondell Pine
Platanus acerifolia	London Plane Tree
Platanus racemose	California Sycamore
Pyrus c. 'Chanticleer'	Callery Pear
Quercus r. 'Fastigiata'	Columnar English Oak
Tristania conferta	Brisbane Box
Ulmus p. 'Drake'	Drake Evergreen Chinese Elm
Zelkova s. 'City Sprite'	City Sprite Zelkova



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

<i>Botanical Name</i>	<i>Common Name</i>
Shrubs/Groundcovers	
Agave 'Blue Glow'	Blue Glow Agave
Agave d. 'Variegata'	Variegated Smooth Agave
Aloe 'Blue Elf'	Blue Elf Aloe
Anigozanthos flavidus	Kangaroo Paw
Baccharis p. 'Twin Peaks'	Twin Peaks Coyote Bush
Bougainvillea 'Raspberry Ice'	Raspberry Ice Bougainvillea
Bulbine f. 'Hallmark'	Stalked Bulbine
Caesalpinia gilliesii	Mexican Bird of Paradise
Callistemon v. 'Little John'	Dwarf Bottlebrush
Cistus x purpureus	Orchid Rockrose
Dasyliiron wheeleri	Desert Spoon
Eremophila g. 'Mingenew Gold'	Gold Emu Bush
Grevillea l. 'Coastal Gem'	Coastal Gem Grevillea
Lantana 'New Gold'	New Gold Lantana
Lavandula 'Meerlo'	Meerlo Lavender
Muhlenbergia capillaris	Pink Muhly Grass
Lomandra 'Breeze'	Dwarf Mat Rush
Lomandra 'Platinum Beauty'	Platinum Beauty Lomandra
Penstemon h. 'Margarita BOP'	Margarita BOP Blue Bedder
Rhamnus californica 'Eve Case'	Dwarf Coffeeberry
Rosa 'Flower Carpet'	Flower Carpet Rose
Rosmarinus o. 'Huntington Carpet'	Creeping Rosemary
Russelia equisetiformis	Red Firecracker Plant
Salvia greggii 'Flame'	Furman's Red Sage
Salvia leucantha	Mexican Bush Sage
Westringia 'Mundi'	Mundi Coast Rosemary
Westringia 'Blue Gem'	Blue Gem Coast Rosemary
Zauschneria californica	California Fuchsia
Furcraea f. 'Mediopicta'	Mauritius Hemp
Leucophyllum f. 'Compacta'	Texas Ranger

5.4.3 Outdoor Furnishings

Extension of Indoor Display Areas

Outdoor display areas should be designed as an extension of typical indoor display areas using such space defining elements as perimeter landscaping, distinctive placement areas, enhanced surface treatment, or decorative security fencing.

Approval with Site Plan

Outdoor display areas should be included and approved by the City with the site plan approval.



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

No Outdoor Storage Permitted Other Than as Specified

Outdoor storage within the Commercial land use is prohibited. However, exceptions may be made for accessory uses directly associated with and incidental to the primary use of the site, provided that the storage area occupies less than 10% of the total site or floor area. All outdoor storage should adhere to these limitations to maintain the aesthetic quality and functional integrity of the development.

Shopping Cart Storage Material

Businesses that provide shopping carts for customer use are required to include designated storage areas for carts within the majority of their parking aisles. These designated areas should be strategically placed for customer convenience and safety, ensuring easy access from multiple locations across the parking lot.

To maintain an orderly and aesthetically pleasing parking environment, the use of tubular or flimsy metal holding structures for cart storage is strictly prohibited. Instead, businesses must implement durable, well-constructed enclosures made from high-quality materials such as concrete, stone, or heavy-duty weather-resistant composites. These storage areas should be designed to seamlessly integrate with the overall design and architecture of the parking lot, promoting a cohesive and professional appearance.

Shopping Cart Storage Screening

Outside shopping cart storage areas should be screened through the use of walls and/or raised planters constructed as an element of the building.

Outdoor Storage Restrictions

Other than noted above, no other outdoor storage is permitted in the Commercial Zone.

5.4.4 Walls and Fencing

Walls and fences are generally used for security purposes and to screen areas from public view. Although walls may be necessary, their design should provide variety and visual interest. If there is not a specific purpose for their use, they should not be utilized within the Commercial land use.

Developments within the Commercial land use should utilize the guidelines in Section 5.2.4, *Walls and Fencing*.

5.4.5 Lighting

Accent lighting, including uplighting and downlighting should be incorporated into MBU development to enhance and complement the overall architectural style of the building, and highlight interesting architectural and landscape architectural features. Figure 5.0-26, *Commercial Lighting Example*, provides an example of anticipated lighting within developments. Low wattage down-lighting shall be used on commercial buildings, provided all exterior lighting

HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

complies with Riverside County Ordinance No. 655 regulating light pollution and its detrimental impact on astronomical observation and research. Developments within the Commercial land use should utilize the guidelines in Section 5.2.5 Lighting when designing.

Figure 5.0-26, Commercial Lighting Example



5.4.6 Color Palette

Commercial developments should focus on low-reflectance, subtle, neutral, or earth-tone colors as the predominant facade tones. To ensure a cohesive visual identity within Harvest Landing, projects should use colors that complement one another and create a unified streetscape. Developments within the Commercial land use should utilize the guidelines in Section 5.2.6, *Color Palette*, when designing projects.

5.4.7 Access/Parking

Shared Parking

Shared parking with adjacent neighboring uses is encouraged, provided minimum parking requirements are met and uses have alternating peak hour parking demands. Refer to Section 4.3 Parking in the Harvest Landing Specific Plan as well as Chapter 19.69 of the City of Perris Zoning Ordinance for shared parking standards.

Avoid Long Continuous Drive Aisles

Large parking lots should avoid long, continuous drive aisles to limit the opportunity for high-speed vehicular travel. Where long drive aisles best serve a site, they should utilize curves and stop signs or textured pavement at strategic locations in place of speed bumps.



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

Screening Parking Lot

Parking lots should be screened from public view through the use of berms, low walls and/or plant materials.

Loading Area Placement

Consideration should be given to the placement of loading areas away from sensitive receptors (schools, residences, hospitals, etc.), public gathering areas or other uses that might be impacted by noise and associated loading activities, as well as locating away from public view. In other cases where placement of loading facilities cannot be accommodated away from these areas, additional setbacks, sound walls, screening or combination thereof may be required.

Walkways through Parking Lots

Pedestrian walkways through commercial development parking lots should be accented with distinctive design features such as, painted crosswalks, raised, colored and/or textured pavement, a widened roadway, or a combination of the preceding.

5.5 Residential

The following guidelines aim to ensure high-quality design, encouraging diversity in scale, materials, and architectural details to create vibrant and pedestrian-friendly commercial spaces.

General Residential Guidelines

1. Avoid monotony through the utilization of varied setbacks, garage orientation and location, and building massing.
2. Designs with the garage as a dominant feature along the street are strongly discouraged. The placement, orientation, and size of the garage should be varied and include a mixture of front, side, rear, and alley-loaded garages as feasible.
3. Alley-loaded products help to create a pedestrian-friendly streetscape and are encouraged.
4. Varied front and side setbacks create a more interesting street scene and are strongly encouraged.
5. Roadways should be oriented to create views of neighborhood parks and other amenities.
6. The creation of pocket parks and pedestrian trails and paths within neighborhoods is encouraged.
7. Alleys should be landscaped to improve their aesthetic quality.
8. In multifamily developments, adjacent buildings should have varied setbacks, floor plans, color and material palettes, and architectural detailing.
9. In multifamily developments, the architectural building edge should be used in place of walls whenever feasible.
10. Architectural styles and detailing are extremely important to the identity and appeal of residential neighborhoods as well as individual homes. A mix of well-defined architectural styles in each neighborhood will add to the character of the project and is encouraged.



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

11. View fencing and decorative walls should be utilized where residential uses are adjacent to open space areas, such as parks and paseos. They allow for privacy without completely blocking views.
12. On-street parking should be provided on local and private residential streets when possible. On-street parking tends to slow traffic and serves as a barrier between vehicular traffic and pedestrians, creating a safer pedestrian environment.

Building-Level Design Guidelines

1. Box-like designs are prohibited. Varied architectural detailing and projections should be used to accentuate specific features and ensure a visually pleasing and varied experience within a neighborhood.
2. Porches, detailed entries, and stoops add to the character of a neighborhood and are encouraged. The majority of units should incorporate a front entry feature into their design. These features should be varied along the street to create visual interest. If possible, these features should project forward of a front entry garage door.
3. Variation in floor plans, unit types, roof form, colors, and materials adds character and visual interest to a neighborhood and should be incorporated into a project's design.
4. Entry features, such as gates, trellises, arches, and arbors should be employed to add visual interest and variety within the neighborhood.
5. Buildings on corner lots set the tone for the neighborhood. Therefore, these buildings should be subject to special design attention and detailing on both street-facing sides.
6. Utilize a variety of architectural features and detailing methods to differentiate adjacent structures. Features such as articulated entry features (porches and stoops), bay windows, and color and material variations are encouraged.
7. Front-entry garage doors should not be the dominant feature of a façade. They should be recessed into the garage wall and should include patterns, modulation, or indentations to create visual interest. Decorative panels, windows, arched doorways, and ornamental trim are strongly encouraged.
8. Large multiple-family dwellings should have articulated façades, including recesses and architectural detailing, to avoid a monotonous streetscape.

Second Dwelling Units

Per Chapter 19.81, *Accessory Dwelling Units and Junior Accessory Dwelling Units*, of the Municipal Zoning

5.5.1 Architecture

The General Development Guidelines outlined below apply to all residential uses within the Harvest Landing Specific Plan area.

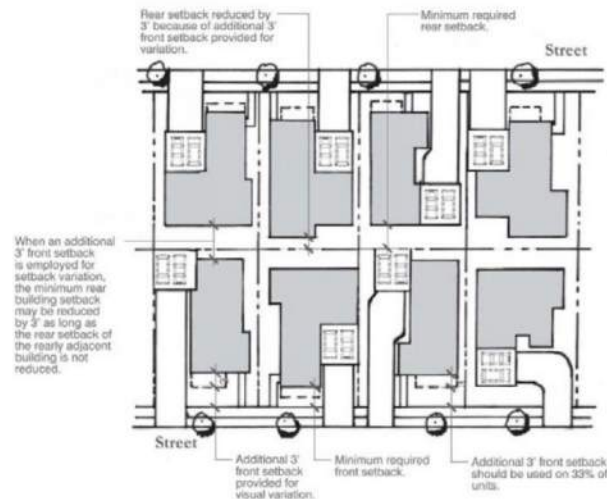
Product Variation

Harvest Landing will be an attractive and visually interesting development. Accordingly, residential neighborhoods will include a variety of product types and design styles as follows:

HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

- All single-family detached projects shall include a variety of front yard building setbacks. One-third of the units should have different front yard setbacks by three feet. In these cases, the rear setbacks may be reduced by three feet.

Figure 5.0-27, Product Variation Example



- All single-family detached projects over five units should have a minimum of three different material and color palettes. No two homes with identical color or materials palettes shall be located adjacent to one another.
- All single-family detached projects over five units shall include a minimum of three unit designs. No two homes with identical designs shall be located adjacent to one another.
- All single-family detached projects over five units shall have a minimum of three primary roof materials and roof designs. No two homes with identical roof designs and materials shall be located adjacent to one another.
- All attached projects shall employ techniques listed in the Building-Level Design Guidelines section of this Specific Plan that address variation of massing, setbacks, rooflines, materials, colors, and styles, and address corner treatments, building/entry orientation, and building/façade articulation.

Garage Variation

To avoid the monotony of single-family neighborhoods that employ the same garage placement, projects in Harvest Landing are required to include a variety of garage placements and orientations. Standard garage placement in detached projects is a front-loaded garage set back 20 feet from the front property line. Single-family detached projects with more than five units shall utilize alternative garage orientation and placement on 50 percent of the units. The following are potential alternative garage placements:

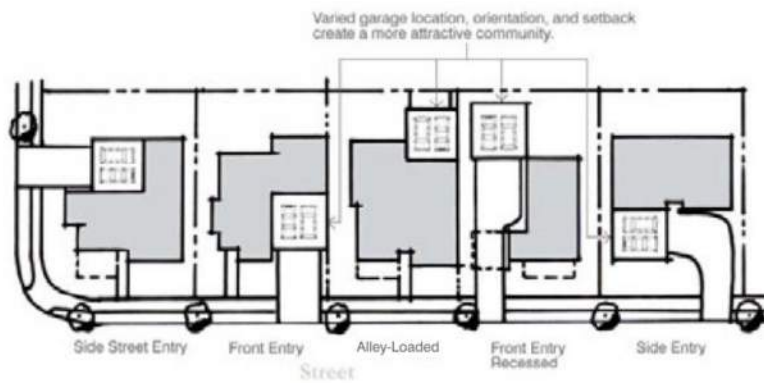
- Alley-loaded garages

HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

- Side-entry garages
- Split garages
- Courtyards or driveways providing multiple unit access
- Straight-in garages in rear two-thirds of the lot

For all garage orientations, roll-up garage doors with automatic openers are required.

Figure 5.0-28, Garage Variation Example






HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

5.5.2 Architectural Styles

The architectural styles identified below reflect those anticipated within the Harvest Landing Specific Plan area. The list is not exhaustive, and alternative architectural styles may be permitted subject to review and approval through the applicable design review process, accompanied by documentation describing key architectural characteristics.


Table 5.0-4, Residential Architectural Style – Tuscan

<i>Tuscan</i>	
Style Elements	
Form	<ul style="list-style-type: none"> • Informal floor plans and building massing • Simple box-like forms with various plate heights • Occasional use of arcades and colonnades
Roof	<ul style="list-style-type: none"> • Shallow pitched gabled or hip roofs 3½:12 to 5:12 • Large overhangs of up to 24" with exposed beam ends and soffits • Variegated clay or concrete tile in terra cotta and earth tones
Walls	<ul style="list-style-type: none"> • Smooth stucco finish • Occasional use of brick • Randomly laid stone with horizontal bonding courses
Windows	<ul style="list-style-type: none"> • Small single-casement windows • Special groupings of casements for prominent accent windows • Arched openings for large expanses of glass • Occasional use of horizontal stone lintels
Colors	<ul style="list-style-type: none"> • Subdued earth tones • Occasional use of brighter earth tones; terra cotta, ochre, and peach • Accented window surrounds with subtle color contrast
Details	<ul style="list-style-type: none"> • Accent shutters • Awnings • Trellis and pergolas • Wrought iron balconettes • Window flower boxes
Typical Elevation	



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

Table 5.0-5, Residential Architectural Style – Cottage

<i>Cottage</i>	
Style Elements	
Form	<ul style="list-style-type: none"> • Gentle to steep pitched roofs with projecting gable ends • Typically steep second-story roofs breaking over first-story elements • Gable, hip, and/or shed roof dormers
Roof	<ul style="list-style-type: none"> • 6:12 to 12:12 high pitch roof • Flat concrete tile • Tight to 12" overhangs • Multiple gable element roofs with broken pitches
Walls	<ul style="list-style-type: none"> • Stone or brick veneer base • 1½- to 2-story stone or brick accent wall planes at front elevation • Sand stucco or smooth finish • Siding accents in gable ends
Windows	<ul style="list-style-type: none"> • Bay windows • Rectangular multipaned windows • Round and curved top windows • Simplified windows on side and rear elevations
Colors	<ul style="list-style-type: none"> • Light earth-tones • Contrasting cool or warm trims • Whites or dark stucco body colors
Details	<ul style="list-style-type: none"> • Shutters and recessed windows • Decorative pot shelves • Battered columns • Entry door porches • Wood columns with braces
Typical Elevation	



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES


Table 5.0-6, Residential Architectural Style – Monterey

<i>Monterey</i>	
Style Elements	
Form	<ul style="list-style-type: none"> • Single-gable roof side-to-side or back-to-back • 2-story horizontal massing, simple form • Second-story balcony at front elevations • Vertical massing held to 25% to 35% of front elevation • Horizontal and vertical massing breaks
Roof	<ul style="list-style-type: none"> • 5:12 to 8:12 normal main roof pitch • Balcony shed roof break at 3½ to 4½ slope • Shingle texture flat tile or 's' concrete roof tile with variegated or blended colors • Overhangs with continuous fascia board and exposed rafters
Walls	<ul style="list-style-type: none"> • Smooth stucco or sand finish • Horizontal or vertical siding at 2nd floor balcony wall • Horizontal or vertical siding accents at gable ends
Windows	<ul style="list-style-type: none"> • Vertical window shape with multiple panes often used in groupings • Gothic shape or curved top feature window at first floor • Colonial window and door trim • Simplified windows on side and rear elevations
Colors	<ul style="list-style-type: none"> • White-tones or medium-range earth tones and white-toned trims
Details	<ul style="list-style-type: none"> • Wood-framed balcony with knee-brace posts and simple wood column detail • 4" to 6" diameter round gable tile vents • Arched or sloping fin walls • Recessed or shuttered windows • Brick veneer wainscot at base of building
Typical Elevation	



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES


Table 5.0-7, Residential Architectural Style – Farmhouse

<i>Farmhouse</i>	
Style Elements	
Form	<ul style="list-style-type: none"> • Variable pitched roofs with moderate overhangs • Expressive structural elements such as rafters, brackets and columns • Variable window types • A mixture of materials such as stone and wood siding • Porches incorporating heavy timber columns with knee braces.
Roof	<ul style="list-style-type: none"> • Roofs gabled with a pitch range from 3:12 to 9:12 for the main roof • Roofs with simple gable, hipped or gambrel forms, often with dormers • Roof materials to be 40-year composition asphalt shingles, concrete tile or clay tile
Walls	<ul style="list-style-type: none"> • Walls clad with stucco or wood siding
Windows	<ul style="list-style-type: none"> • Windows broad in proportion • Windows have trim details over the window head • Box and angled bay windows used as accents • Picture windows • Windows double hung with multiple pane over one division
Colors	<ul style="list-style-type: none"> • Colors reflective of and complementary to materials used for walls, such as wood siding • Stone and brick used as accents
Details	<ul style="list-style-type: none"> • Simple eave projections with trimmed rafter tails and fascia • Broad porches, encompassing the full width of the house • Piers and balustrades of the same material as the main mass of the house • Porch balustrades are connected to column supports
Typical Elevation	



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

Table 5.0-8, Residential Architectural Style – Craftsman

<i>Craftsman</i>	
Style Elements	
Form	<ul style="list-style-type: none"> • Cross-gabled or gable-fronted one or two-story box-like massing under main roof • Entry porch
Roof	<ul style="list-style-type: none"> • 4:12 to 8:12 normal roof pitch • Shingle texture flat concrete roof tile • Plain cut rafter tails • Overhangs 6" to 18" • Hip and gable roofs
Walls	<ul style="list-style-type: none"> • Horizontal siding • Plain wood shingle siding • Sand or smooth stucco finish • Blended stucco and siding • Stucco with stone base, battered wall forms
Windows	<ul style="list-style-type: none"> • Ribbon windows – 3 or more ganged • Single-hung windows • Mullion upper half of window • Simplified windows on side and rear elevations
Colors	<ul style="list-style-type: none"> • Light or deep earth-tone colors
Details	<ul style="list-style-type: none"> • Stone and brick blended accent chimney • Full length porch • Layered wood entry door and window details • Classically styled columns (i.e., Battered or Heavy Square) • Decorative ridge beams and purlins • Stone or brick base • Triangular knee braces
Typical Elevation	



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

5.5.3 Residential Home Types

A range of residential home types is anticipated to create a diverse and cohesive community that accommodates varying housing needs and lifestyles. The residential home types identified below represent examples of housing products that may be developed within Harvest Landing. This list is not intended to be exclusive, and alternative residential products may be proposed and considered through the applicable review and approval process. The examples provided are intended to illustrate representative housing types within each density range.




HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

Table 5.0-9, Residential Home Types – Single Family Detached

<i>Single Family Detached</i>	
Style Elements	
Characteristics	<ul style="list-style-type: none"> • Homes are oriented to face the street. • Alternating architectural styles, colors, and garage orientations create a more interesting streetscape.
Garage Orientation	<ul style="list-style-type: none"> • Front loaded or alley loaded
Alleys	<ul style="list-style-type: none"> • Sometimes
Front Door Orientation	<ul style="list-style-type: none"> • Street facing
Density Type	<ul style="list-style-type: none"> • Low Density Residential and Medium Density Residential
Typical Photo	
Typical Site Plan	<p>Typical Site Plan</p>

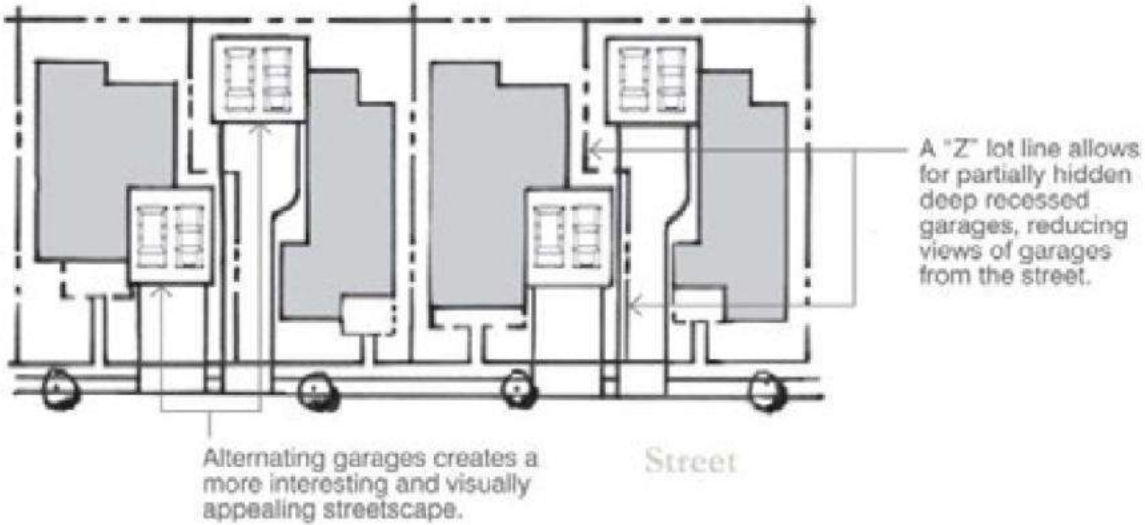
HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

Table 5.0-10, Residential Home Types – 2-Pack Single Family Detached

<i>2-Pack Single Family Detached</i>	
Style Elements	
Characteristics	<ul style="list-style-type: none"> • A "Z" lot line allows for alternating garages to be deeply recessed, decreasing the visual effect of garages on the streetscape. • Garages are alternatively located in the front and rear portions of the lot, creating variation along the street front.
Garage Orientation	<ul style="list-style-type: none"> • Front loaded
Alleys	<ul style="list-style-type: none"> • No
Front Door Orientation	<ul style="list-style-type: none"> • Street facing
Density Type	<ul style="list-style-type: none"> • Low Density Residential and Medium Density Residential
Typical Photo	

Typical Site Plan

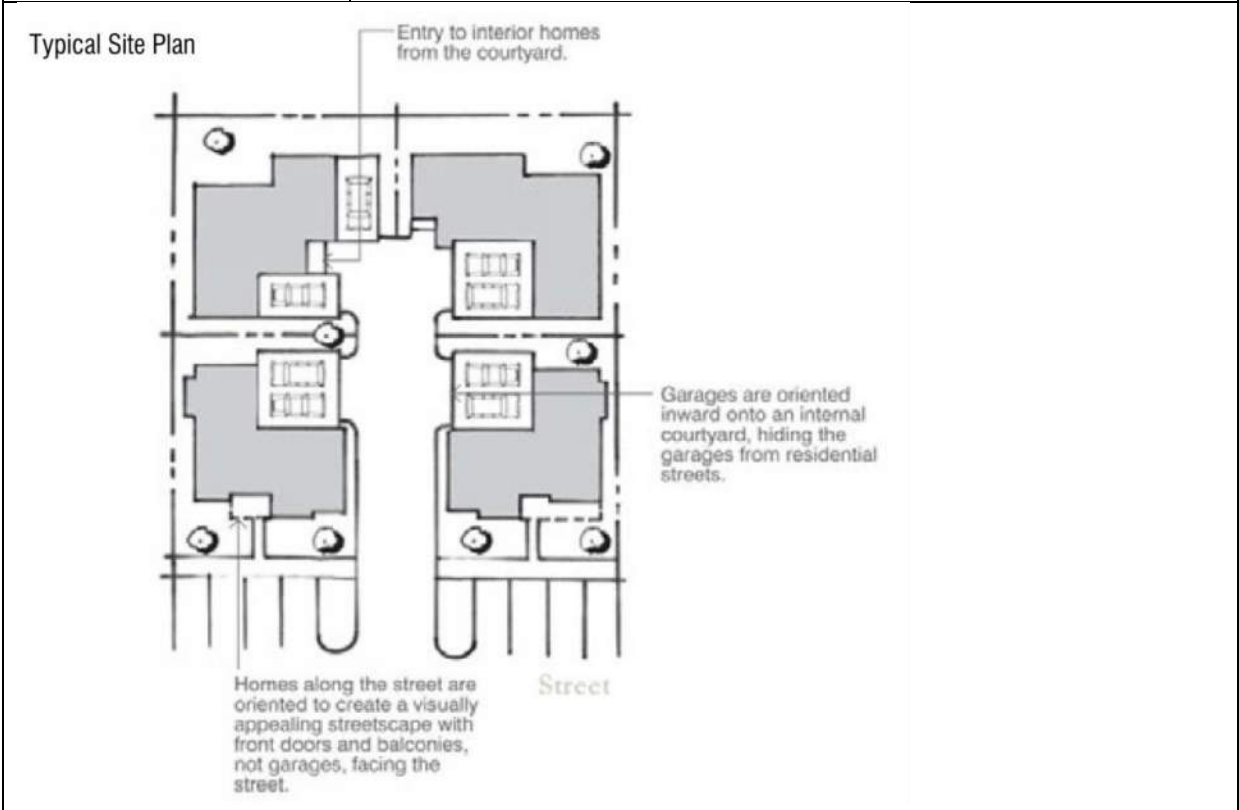
Typical Site Plan



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES


Table 5.0-11, Residential Home Types – 4-Pack Courtyard Single Family Detached

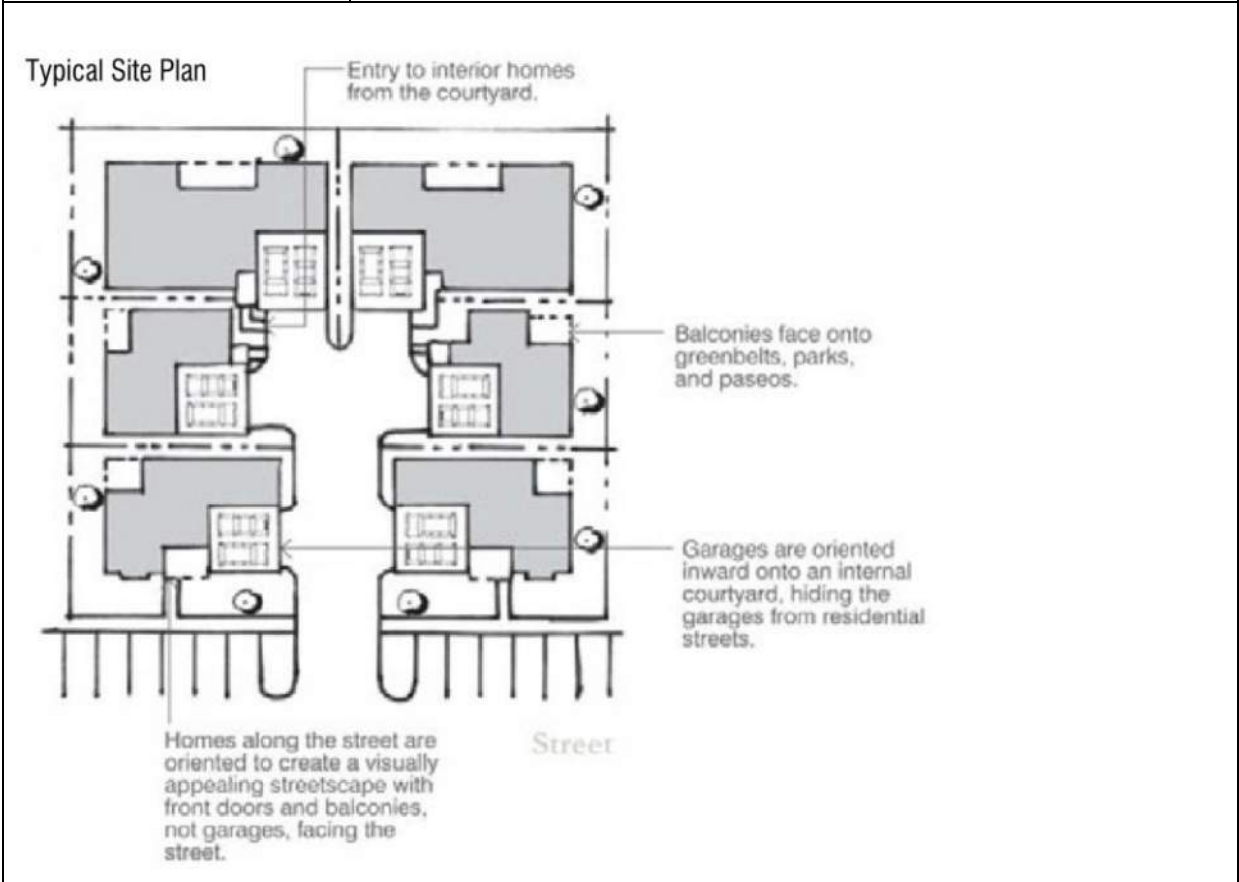
<i>4-Pack Courtyard Single Family Detached</i>	
Style Elements	
Characteristics	<ul style="list-style-type: none"> • Four single-family detached homes clustered around an interior courtyard
Garage Orientation	<ul style="list-style-type: none"> • Onto interior courtyard
Alleys	<ul style="list-style-type: none"> • No
Front Door Orientation	<ul style="list-style-type: none"> • Street facing for homes along street • Into interior courtyard or onto greenbelts/parks for interior homes
Density Type	<ul style="list-style-type: none"> • Medium Density Residential
Typical Photo	



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

Table 5.0-12, Residential Home Types – 6-Pack Courtyard Single Family Detached

<i>6-Pack Courtyard Single Family Detached</i>	
Style Elements	
Characteristics	<ul style="list-style-type: none"> • Six single-family detached homes clustered around an interior courtyard
Garage Orientation	<ul style="list-style-type: none"> • Onto interior courtyard
Alleys	<ul style="list-style-type: none"> • No
Front Door Orientation	<ul style="list-style-type: none"> • Street facing for homes along street • Into interior courtyard or onto greenbelts/parks for interior homes
Density Type	<ul style="list-style-type: none"> • Medium Density Residential
Typical Photo	

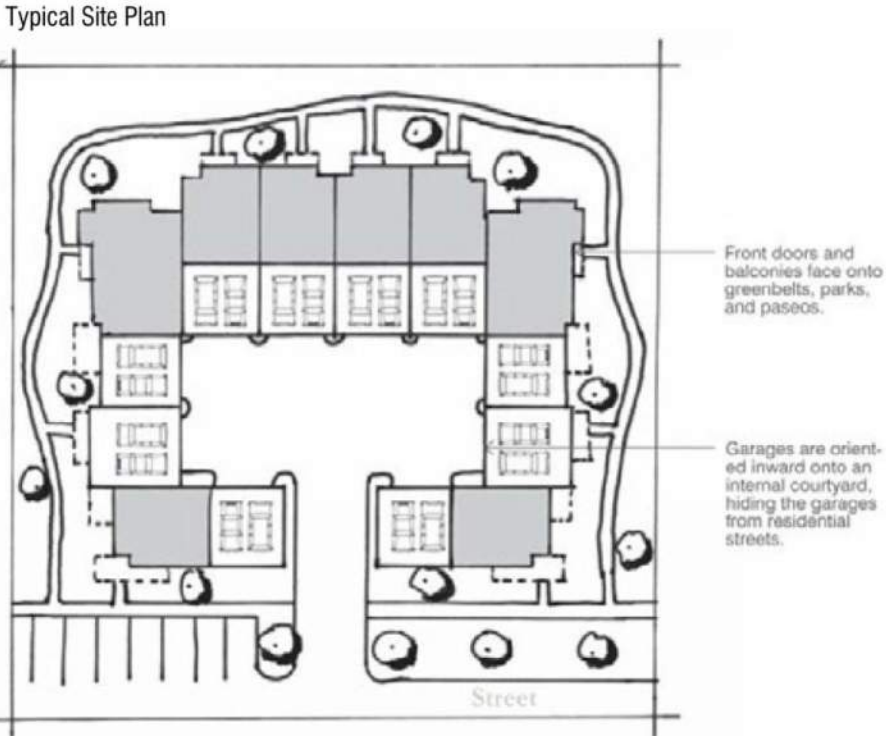


HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

Table 5.0-13, Residential Home Types – Motorcourt Townhome

<i>Motorcourt Townhome</i>	
Style Elements	
Characteristics	<ul style="list-style-type: none"> • Townhomes clustered around a central courtyard • Homes face onto greenbelts, paseos, or parks
Garage Orientation	<ul style="list-style-type: none"> • Onto interior courtyard
Alleys	<ul style="list-style-type: none"> • No
Front Door Orientation	<ul style="list-style-type: none"> • Street facing for homes along street • Onto greenbelts/parks for interior homes
Density Type	<ul style="list-style-type: none"> • Medium Density Residential, Medium High Density Residential, and High Density Residential
Typical Photo	

Typical Site Plan




HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

Table 5.0-14, Residential Home Types – Row Townhome

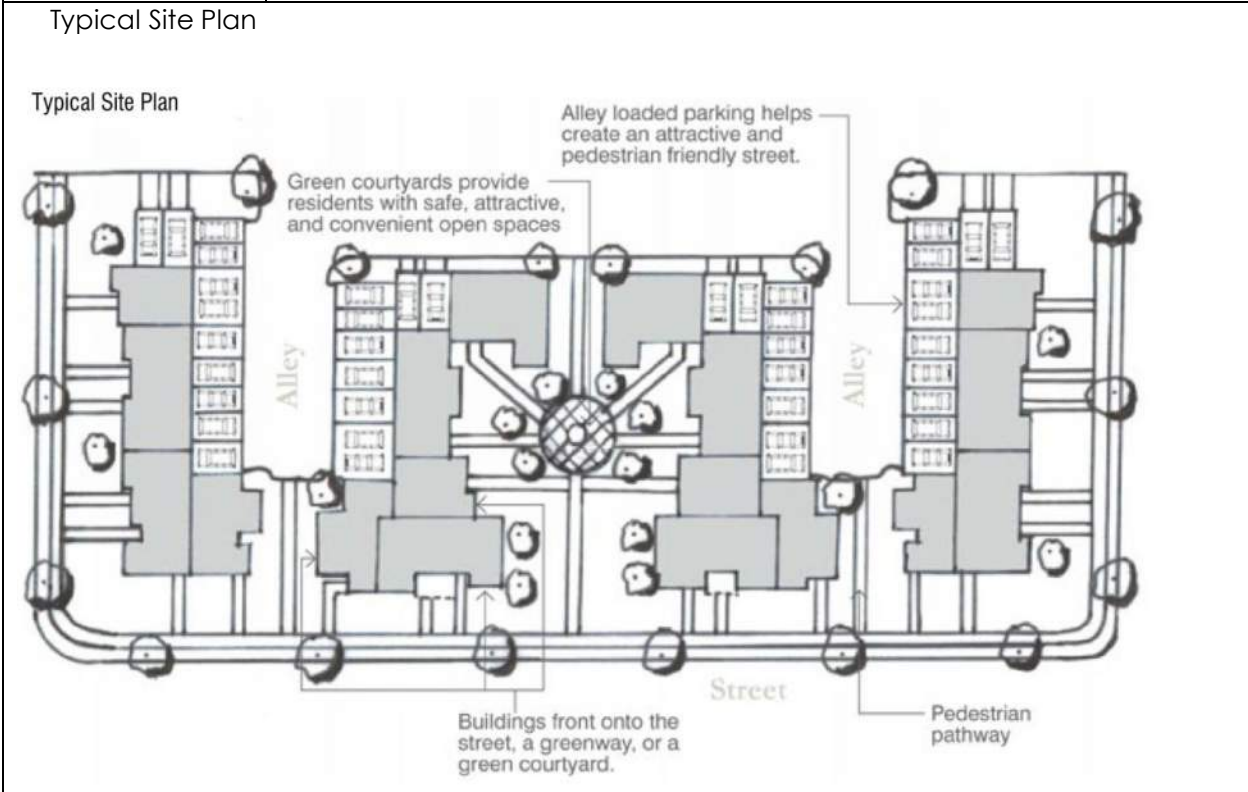
<i>Row Townhome</i>	
Style Elements	
Characteristics	<ul style="list-style-type: none"> • Townhomes lined along alleys fronting onto a greenbelt or paseo
Garage Orientation	<ul style="list-style-type: none"> • Onto alley
Alleys	<ul style="list-style-type: none"> • Yes
Front Door Orientation	<ul style="list-style-type: none"> • Street facing for homes along street • Onto greenbelts/parks for interior homes
Density Type	<ul style="list-style-type: none"> • Medium Density Residential, Medium High Density Residential, and High Density Residential
Typical Photo	
Typical Site Plan	<p style="text-align: center;">Typical Site Plan</p>

HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

Table 5.0-15, Residential Home Types – Greencourt Townhome

<i>Greencourt Townhome</i>	
Style Elements	
Characteristics	<ul style="list-style-type: none"> • Townhomes clustered around a central courtyard • Homes face onto greenbelts, paseos, or parks Often separated by green pedestrian pathways
Garage Orientation	<ul style="list-style-type: none"> • Onto interior alley
Alleys	<ul style="list-style-type: none"> • Yes
Front Door Orientation	<ul style="list-style-type: none"> • Street facing for homes along street • Onto greenbelts/parks for interior homes
Density Type	<ul style="list-style-type: none"> • Medium Density Residential, Medium High Density Residential, and High Density Residential
Typical Photo	

Typical Site Plan





HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

5.5.4 Landscaping

Landscaping within the residential areas of the Specific Plan shall be designed to be cohesive with, and complementary to, the MBU and Commercial land uses. Plant materials shall be selected for compatibility with the overall thematic setting of the Specific Plan, appropriateness to local climatic and soil conditions, ease of maintenance, and water conservation. All landscape plans and installations shall comply with applicable City design guidelines, codes, and regulations. Plant materials shall consist of species identified in the approved MBU and Commercial plant palettes, or other species as approved by the City.

If approved by the City of Perris, plants should be consistent with California Friendly Landscape and meet all minimum City of Perris Water Conservation Requirements as defined in Development Code Chapter 19.70, *Landscaping*, including but not limited to:

- Use of drought-tolerant plants.
- Use of landscaped areas designed to retain irrigation water.
- Use of satellite-based irrigation timers.
- Use of automatic irrigation systems.
- Use of plant groupings with similar irrigation requirements to reduce over-irrigation.
- Extensive use of mulch in landscaped areas.
- Installation of drip irrigation systems, where appropriate.
- Limit use of turf for active purposes only.
- Limit use of impervious surfaces.

As practical, ample street and shade trees should be provided to create an attractive and pedestrian friendly environment.

5.5.5 Walls and Fencing

Walls and fences are generally used for security purposes and to screen areas from public view. Although walls may be necessary, their design should provide variety and visual interest. Developments within the Residential land uses should utilize the guidelines in Section 5.2.4, *Walls and Fencing*.

5.5.6 Lighting

Developments within the Residential land uses should utilize the guidelines in Section 5.2.5 Lighting when designing projects.

Pedestrian-scale lighting should be provided in areas with nighttime use. The type, style, and intensity of pedestrian lighting should reflect the use of the character of the area.

5.5.7 Low Income and Senior Housing Bonuses

Per Chapter 19.57, Density Bonuses and Other Incentives for Low Income and Senior Housing.



HARVEST LANDING SPECIFIC PLAN DESIGN GUIDELINES

5.5.8 Access/Parking

On-Street Parking

On-street parking should be provided on local and private residential streets when possible. On-street parking tends to slow traffic and serves as a barrier between vehicular traffic and pedestrians, creating a safer pedestrian environment.



HARVEST LANDING SPECIFIC PLAN SIGNAGE

6.0 SIGNAGE

6.1 Introduction

The purposes of a sign program are to establish uniform sign design guidelines and sign area allocations for all uses and/or buildings on a site. In addition to the following specifications, signage within the Harvest Landing Specific Plan area shall adhere to Chapter 19.75, *Sign Regulations*, of the City's Zoning Code. An application for a sign program shall be approved by the Planning Manager. The signage package for the Harvest Landing Specific Plan can be found in Appendix B for references.

6.2 Location

Location of signs shall be no closer than 5 feet from the property line and shall be located in a landscaped planter to aid in visual appearance.

On-site traffic circulation signage shall be organized to effectively direct vehicles to appropriate parking areas.

6.3 Design

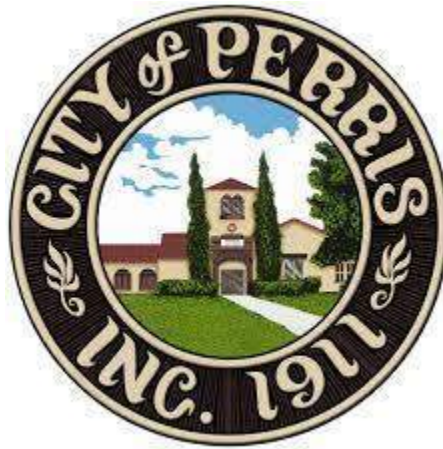
Harvest Landing Logo

Any sign program along a major roadway shall include signage at main and secondary entrances, and at major intersections, including the Harvest Landing logo and the City logo.

Figure 6.0-1 Harvest Landing Logo



Figure 6.0-2 City of Perris Logo



Color Schemes

Color schemes for signage shall be related to other signs and graphics in the vicinity to achieve a coordinated sense of identity. The exposed backs and sides of signs shall be architecturally treated to blend with the exterior character of the buildings and site.

Address Identification Signage

Address identification signs shall be 12 inches high and located on the upper corner of the building wall facing each applicable public right-of-way, pursuant to Perris Municipal Code Section 19.75.

Lighting and Maintenance of Signage

"Canned" signs shall be prohibited. Interior illuminated channel letters should be used instead. All signs shall be maintained in good repair.

Neon Signage

Neon signage shall comply with March Air Reserve Base/Inland Port Airport restrictions for lighting.

Multiple Buildings and/or Tenants

Commercial, offices, business park and manufacturing complexes, and similar facilities with multiple buildings and/or tenants shall submit a 'sign program' for the placement of on-site signs according to a compatible design that is common to all structures and uses.

Major Roadway Zones/Freeway Corridor

Commercial, offices, business park and manufacturing complexes, and similar facilities with multiple buildings and/or tenants will be required to include the Harvest Landing Logo in their main signage, as well as projects located along major roadways or freeway corridors. Projects along the Freeway Corridor will be required to include the City of Perris Logo or name in the main signage.

HARVEST LANDING SPECIFIC PLAN SIGNAGE

Monument Signs

Monument signs shall be located such that visibility is not impaired by mature landscaping. They should also incorporate colors, materials and design of primary buildings.

Gateway Monuments

Site identity signs located at intersections around the border of the Harvest Landing Specific Plan area shall include freeway monument/pylons where adjacent to the freeway/frontage road, and freestanding site monument/pylons and multi-tenant monuments at the other boundary intersections. Monuments at key intersections will help to identify entrance into Harvest Landing as shown in Figure 6.0-1, *Example Monuments*. Additionally, Figure 6.0-2, *Pylon and Monumentation Signage*, shows the locations for pylon and monument signage around the perimeter of Harvest Landing.

The Specific Plan will incorporate two pilot monument signs located as depicted in Figure 3.0-4. Locations of the monument signs were chosen to provide key visual aids to the patrons of Perris and provide visibility of the commercial tenants to the 215 Freeway.

Figure 6.0-3, Pylon Monument Example



HARVEST LANDING SPECIFIC PLAN SIGNAGE

Figure 6.0-4, Pylon Monument Examples



Figure 6.0-5, Tenant Monument Sign Examples



HARVEST LANDING SPECIFIC PLAN SIGNAGE

Figure 6.0-6, Pylon and Monumentation Signage Locations





HARVEST LANDING SPECIFIC PLAN SIGNAGE

6.4 Prohibited Signs

Other than Grand Opening Signs as permitted in Municipal Code Section 19.75.090.D, no banners, flags, pennants, balloons, tethered inflatable, signs within public right-of-way, projecting signs or off-site directional signs shall be permitted.



HARVEST LANDING SPECIFIC PLAN IMPLEMENTATION AND ADMINISTRATION

7.0 IMPLEMENTATION AND ADMINISTRATIVE

7.1 Specific Plan Implementation

Whether regulatory or policy oriented, all Specific Plans must contain a “program of implementation measures, including regulations, programs, public works projects, and financing measures” pursuant to California Government Code, Section 65451 (a)(4).

Other documents have been prepared and processed concurrently with the adoption of the Harvest Landing Specific Plan, including a General Plan amendment and a certified Environmental Impact Report (EIR). These documents will form the basic framework to guide future development within the Specific Plan Project site.

All development within the Specific Plan Project site shall comply with the requirements and standards set forth in this document. Where conflicts exist between the following standards and those found in the City of Perris Municipal Zoning Code, the standards contained in this document shall apply and prevail. The provisions of the City of Perris Municipal Zoning Code shall apply to any area of site development, administration, review procedures, environmental review, landscaping requirements, and parking regulations not expressly addressed by this Specific Plan document.

When there is a question or ambiguity regarding the interpretation of any provision of this Specific Plan, the Director of Development Services has the authority to interpret the intent of such provision. The Director of Development Services may, at his/her discretion, refer interpretations to the Planning Commission for its consideration and action. Such a referral shall be accompanied by a written analysis of issues related to the interpretation. The Planning Commission action may be appealed to the City Council. All interpretations made by the Director of Development Services may be appealed to the Planning Commission in accordance with the appeal procedures set forth in the City's Municipal Code.

7.1.1 Development Phasing

The proposed phasing of parcel development is described on Table 7.0-1, *Development Phasing*. Overall, the Specific Plan is expected to be developed starting in 2025 and built out in 2030. The Specific Plan Phasing is discussed in detail in the Harvest Landing Development Agreement.

The boundaries of the land use designations are approximate and generally follow streets and property lines. Minor changes in boundary alignment and location are permissible with approval by the Director of Development Services. However, the intended character and overall location of the land use categories must be maintained.

The roadways and infrastructure necessary to serve and provide access will be developed in conjunction with the development in each of the two phases; however, grading may not conform



HARVEST LANDING SPECIFIC PLAN IMPLEMENTATION AND ADMINISTRATION

to the phasing plan, subject to the Engineering and Planning Department's approval. Grading for the entire site may occur at one time.

It should be emphasized that the phasing program described in this section is a projection based on a judgment of future planning and market factors over the next several years. Therefore, this phasing program is not to be taken as a mandatory development sequence. Development area sequencing may change as the result of future conditions that neither the City nor the developer has knowledge of as of the date of this submittal.

However, the basic standards will not change and are required to be complied with regardless of shifts in the composition of each development phase. The developers of property within Harvest Landing will be required to comply with all grading, drainage, and road improvements as specified in the Specific Plan.

Table 7.0-1, Development Phasing

Land Use	Acres	Anticipated Square Footage Build Out
Phase 1		
Multiple Business Use	138.13	1,727,579
Commercial	46.24	423,007
Office	N/A	5,500
WQMP	12.91	0
Roadways	25.73	0
Total Phase 1	223.01	2,156,086
Phase 2		
Multiple Business Use	51.08	1,668,784
Roadways	17.89	0
Total Phase 2	68.97	1,668,784
Phase 1 + Phase 2		
Overall Total	291.98	3,824,870

*Some areas outside of the Harvest Landing Specific Plan are included within this roadway calculation, including roadway improvements to Frontage Avenue from Nuevo Road to Placentia Avenue.

With respect to the backbone circulation network, the Specific Plan will be phased consistent with the Development Agreement. The phasing and limits of required improvements shall be as governed by the Development Agreement and Conditions of Approval. ¹

¹ Traffic-related requirements identified in the Conditions of Approval or the overall Harvest Landing TIA shall be applied to future development where applicable. Projects that generate trips below the analyzed thresholds and do not result in impacts beyond those evaluated in the Environmental Impact Report will not be subject to additional traffic conditions, and may be subject to a reduced improvements. Applicants may also provide updated traffic documentation to support proposed revisions to existing Conditions of Approval.



HARVEST LANDING SPECIFIC PLAN IMPLEMENTATION AND ADMINISTRATION

7.1.2 Specific Plan Financing

The financing of construction, operation, and maintenance of public improvements and facilities will include funding through a combination of financing mechanisms. Financing may involve a combination of impact fees and exactions, special assessment districts, landscaping and lighting districts, and community facilities districts, and other mechanisms as agreed to by the Master Developer/Landowner and City.

The City and developer will cooperate to ensure that the public facilities are built in accordance with all requirements of the Specific Plan. A Development Agreement and conditions of approval will be used to facilitate this process.

Financing and Maintenance Mechanisms

Financing District

Construction of required infrastructure (such as sewer and water lines, storm drains, and roads) may be financed through the establishment of a financing district (e.g., Assessment District, Community Facilities District, or Road and Bridge Benefit District).

Landscape Maintenance Responsibility

Maintenance for the landscape within the street rights-of-way shall be provided by a landscape maintenance district (LMD). The responsibility for all on-site landscape maintenance shall be that of the property owner residing in the facility and shall be completely independent of the LMD. When water quality BMPs are adjacent to the street rights-of-way, an easement will be provided for this facility. The maintenance of the landscape and the BMPs will become the responsibility of the LMD.

Any landscaping within public rights-of-way will require approval by the City and assurance of continuing maintenance through the establishment of a landscape maintenance district, similar mechanism, or Conditions, Covenants and Restrictions (CC&Rs), as approved by the City Engineer with input from the Director of Development Services.

7.1.3 Project Maintenance

Infrastructure Improvements

Within Harvest Landing, the Master Developer/Landowner will be responsible for constructing the required infrastructure improvements, such as streets, water lines, sewers, storm drains, and recycled water lines.

All necessary infrastructure improvements will be developed in conjunction with the roadway improvements.



HARVEST LANDING SPECIFIC PLAN IMPLEMENTATION AND ADMINISTRATION

Maintenance

Per Section 19.02.080, *Property Maintenance*, of the Municipal Zoning Code.

The creation and operation of a Maintenance Assessment District is required for maintaining the aesthetic quality of Harvest Landing. It is anticipated that maintenance responsibilities may be the responsibility of businesses, Community Facilities Districts (CFD), Assessment District, and/or other maintenance mechanisms. The public and private entities are described below and in Table 7.0-2, *Maintenance Plan*.

Project Roadways

All public roadways shall be incorporated into the City's system of roads for operation and maintenance approved by the City. All private roads shall be owned and maintained by the developer(s) and agreements for maintenance discussed in CC&Rs.

Table 7.0-2, Maintenance Plan

Type	Developed by	Owned by	Maintained by
Streetscape			
Community Roadways (curb-to-curb: arterials, secondary arterials, and collectors)	Developer	City of Perris	City of Perris
Neighborhood Roadways (curb-to-curb: local streets)	Developer	City of Perris	City of Perris
Private Streets	Guest Builder	CC&R	CC&R
Community Roadway Medians (primary arterials, secondary arterials, and collectors)	Developer	City of Perris	LLD/ City of Perris
Neighborhood Roadway Medians (local streets)	Guest Builder	City of Perris	LLD/ City of Perris
Community and Secondary Entries	Developer	CC&R	CC&R
Street Lighting (community and neighborhood roadways)	Developer	City of Perris	LLD/ City of Perris
Community Walls and Fences	Developer	CC&R	CC&R
Infrastructure			
Water systems (off-site)	Developer	EMWD	EMWD
Water systems (on-site)	Developer	Owner	Owner
Non-potable water systems	Developer	EMWD	EMWD
Sewer systems (on-site)	Developer	Owner	Owner
Sewer systems (off-site)	Developer	EMWD	EMWD



HARVEST LANDING SPECIFIC PLAN IMPLEMENTATION AND ADMINISTRATION

Type	Developed by	Owned by	Maintained by
Drainage systems (on-site)	Developer	Owner	Owner
Drainage systems (off-site)	Developer	EMWD	EMWD

Notes:

Certain facilities and improvements may be subject to reimbursement agreements.

LLD = Landscape and Lighting District or special maintenance district

RCFC = Riverside County Flood Control

EMWD= Eastern Municipal Water District

7.2 Specific Plan Entitlement Process

7.2.1 Agency Responsibilities

The City's Director of Development Services shall be responsible for administering the provisions of the Harvest Landing Specific Plan in accordance with the provisions of this Specific Plan, the State of California Government Code, Subdivision Map Act, and the City of Perris General Plan and Municipal Code.

Decision Making Bodies and Responsibilities

City Council

The City Council of the City of Perris is the final decision-making authority on all major amendments to the Specific Plan, permit revocations, and the referral of all permit types requiring council approval as listed below in Table 7.0-3, *Permit Types and Processes*. The City Council also serves as the hearing body for appeals of Planning Commission decisions.

Planning Commission

The Planning Commission is the advisory body to the City Council in land use decisions for the City of Perris and the approval authority of land development requests such as Major Modifications, and some Development Plan Reviews as determined by the Director of Development Services and as shown in Table 7.0-3, *Permit Types and Processes*. The Planning Commission may also refer any application to the City Council.

Development Services Director

The Director of Development Services or designee shall be the approval authority on all permitted uses, Administrative Development Plan Reviews, substantial conformance reviews and Minor Modifications. In addition, the Director of Development Services, or designee, may refer any application to the next higher authority due to special issues, impacts related to the project, or controversy. Refer to Table 7.0-3, *Permit Types and Processes*.



7.2.2 Permit Types and Processes

Table 7.0-3, Permit Types and Processes

Application Type	Approval Authority	Public Hearing
Accessory Uses	Director of Development Services	No
Conditional Use Permit	Planning Commission	Yes
Determination of Public Convenience or Necessity	Planning Commission	Yes
Administrative Development Plan Review	Director of Development Services	No
Development Plan Review	Planning Commission	Yes
Specific Plan Amendment	City Council	Yes
Temporary Outdoor Uses	Director of Development Services	No
Variances	Planning Commission	Yes
Major Modification	Same authority as original project	Yes
Minor Modification	Director of Development Services	No
Substantial Conformance	Director of Development Services	No
Other Actions	Approval Authority	Public Hearing
Administrative Determination	Director of Development Services	No
Permitted Uses*	Director of Development Services	No
Tentative Track and Parcel Maps	Planning Commission	Yes

* Pursuant to the prior Harvest Landing Specific Plan (refer to Appendix B), any modification that would result in increased residential densities or unit counts beyond those previously approved in the locations set forth in Appendix B shall require submittal to the Airport Land Use Commission (ALUC) for a consistency determination and may be subject to additional discretionary review and supplemental environmental analysis at the discretion of the Director of Development Services in accordance with applicable State and local regulations.

Furthermore, if a proposed residential project is located adjacent to an existing or approved commercial or business park development, the City shall retain the discretion to require additional review, coordination, and/or compatibility analyses to ensure appropriate land use interface, buffering, and mitigation of potential conflicts.

Accessory Uses (A)

These types of uses are only allowed subject to compatibility with the primary use of the property. Accordingly, Accessory Uses are clearly subordinate to, and supportive of, the primary use of the property. Accessory Uses are not allowed to be processed prior to the primary use, but may be processed concurrently with or after the primary use has been entitled. An Accessory Use may be approved after the primary use has been entitled only if no modifications to the entitled Development Plan are required as a result of the Accessory Use. The Director of Development Services or designee is authorized to approve or deny requests for accessory uses.



HARVEST LANDING SPECIFIC PLAN IMPLEMENTATION AND ADMINISTRATION

Administrative Determination (AD)

When a land use is proposed, but not specifically listed within this Specific Plan as an allowable use, the Director of Development Services or designee shall have the authority to determine if the proposed use is a Permitted Use and appropriate application for land use approval. In doing so, the proposed project will follow the approval procedure for that permit type.

Conditional Use Permits (CUP)

A Conditional Use is one which is not permitted by right but may be conditionally allowed in order to properly regulate the operational characteristics of the proposed use in order to account for any potential impacts. Certain types of land uses within the Specific Plan are to be processed as a Conditional Use Permit. The Planning Commission is authorized to approve or deny such requests, upon a recommendation from the Development Services Department.

Determination of Public Convenience or Necessity (PCN)

As required by the California Department of Alcohol Beverage Control, the City must review and make finding of "Public Convenience and Necessity" for any business that wishes to sell alcohol beverages, where there already may be an "undue concentration" of such businesses within the same census tract. The Planning Commission is authorized to approve or deny any requests for determination of public convenience or necessity, upon a recommendation from the Development Services Department.

Development Plan Review (DPR)

All proposed structures or exterior modifications in commercial, business park and manufacturing, or residential land use areas (landscape, parking, and lighting), must be designed and reviewed through a Development Plan Review. The purpose of this application is to provide the City with certain site design information, such as floor plans, elevations, required parking, etc. Any such application can be processed concurrently with any other discretionary planning applicable Permit. As such, the Planning Commission, and/or the City Council is authorized to approve, conditionally approve, or deny any requests as per City of Perris Municipal Code 19.50.040.

Administrative Development Plan Review (ADPR)

An Administrative Development Plan Review applies to development proposals that are permitted uses within the Specific Plan and do not require a Conditional Use Permit or other higher-level entitlement. The purpose of the ADPR is to ensure that the proposed development complies with all applicable development standards, design guidelines, performance criteria, and development regulations established by this Specific Plan.

Projects eligible for ADPR include any development that is permitted by right within the applicable zone, regardless of size, intensity, or land use category, provided the use itself is permitted and no discretionary permit beyond the ADPR is required.

The ADPR is a discretionary review process, intended to evaluate site layout, building design, circulation, access, parking, landscaping, screening, and conformance with all applicable



HARVEST LANDING SPECIFIC PLAN IMPLEMENTATION AND ADMINISTRATION

standards. The Director of Development Services or designee is authorized to approve, approve with conditions, or deny an Administrative Development Plan Review.

Major/Minor Modifications (MM)

The Director of Development Services or designee shall review any requests for revisions or modifications to approved projects and determine whether the proposed changes are “Major” or “Minor.” Major Modifications are modifications to an approved permit that do not change the basic concept or use allowed by the original approval but may include but are not limited to, a significant increase in intensity of approved use, changes resulting in significant adverse effects, expansion within the approved permit area or changes to the original conditions or approval including extensions to the overall life of the permitted use. Major Modifications to approved projects shall be reviewed and processed in the same manner as the originating project.

Minor Modifications are changes to an approved permit that do not change the basic concept or use allowed by the original approval or the effect of an approval to surrounding property that may include but are not limited to: (1) modifications for upgrading facilities, up to 2,500 square feet, including increasing facilities size*; (2) modifications for compliance with requirements of other public agencies; (3) modifications necessary to comply with the final conditions of approval; (4) minor improvements to site and architectural plans that do not increase the overall Specific Plan build out square footage but are necessary to meet particular design intent and/or suit the needs of a new tenant; (5) modifications to on-site circulation and parking, lighting, fencing or walls (placement and/or height), landscaping and/or signage requirements, provided those modifications will have no adverse effect upon public health, safety, welfare or the environment and; (6) proposed modification is exempt from provisions of California Environmental Quality Act. The Director of Development Services or designee shall review all requests for Minor Modifications to approved projects and make a determination to approve or deny such requests. The approval of such modifications shall not extend the expiration date of the original approval, unless specifically requested by the revision.

* Facility sizes may be increased, up to 2,500 square feet, provided that the overall trip generation does not exceed the established thresholds. As such, if a project proposes to increase square footage for one building while reducing intensity elsewhere within the Specific Plan area, it will not require additional review or approval beyond the Director of Development Services.

Permitted Uses (P)

Permitted uses are those which shall be allowed provided they comply with existing Specific Plan, City Ordinances and policies. See Table 2.0-2, *Allowed Land Uses*, for types of land uses within the Specific Plan that are to be processed as a permitted use. Permitted uses are subject to review, final determination by the Director of Development Services or designee.

Temporary Outdoor Events

Events that are considered to be occurring on a recurring and/or a temporary basis are required to be approved through a Temporary Use Permit (TUP). All such events shall comply with Section



HARVEST LANDING SPECIFIC PLAN IMPLEMENTATION AND ADMINISTRATION

19.60 of the City of Perris Zoning Code. The Director of Development Services or designee is authorized to approve or deny such requests.

Tentative Tract / Parcel Maps (TTM/TPM)

Any application for the division of land with the Specific Plan is to be processed as a Subdivision application. The Planning Commission is authorized to review and approve proposed applications for Parcel Maps (4 lots or fewer). The Planning Commission shall review and recommend either approval or denial of all tentative maps (five lots or more) to the City Council. The City Council is authorized to approve or disapprove any proposed requests.

Other Applications

Notwithstanding any indication to the contrary, nothing in this Specific Plan shall be construed to imply that the entitlement process for any other application not listed in this section is in any way modified from the normal procedures as set forth in Section 19.54.30, *Review authority and processing procedures*, of the City of Perris Zoning Ordinance.

Procedures

Internal Review

The Development Services Department shall be the lead Department for any Planning application. In doing so, the Department must consult with any other City department, County department, State department, or any other reviewing agency that has jurisdiction or authority over the application as deemed appropriate. During the review of the applications, it is the responsibility of the Development Services Department to review such applications against any and all City regulations.

Public Hearing Process

All decisions by the Planning Commission and the City Council shall be heard at a public hearing that is publicly advertised in accordance with Section 19.56, *Public Hearing Procedures*, of the City of Perris Zoning Ordinance. No building permits, grading permits, sign permits, or any other permits may be issued until the Approval Authority has approved the project.

Appeals

Any decision by the Director of Development Services or designee may be appealed to the Planning Commission, who shall then set the matter for a public hearing. The Planning Commission shall then approve or deny the appeal, and confirm or overturn the decision of the Director of Development Services or designee.

Any decision by the Planning Commission may be appealed to the City Council, who shall then set the matter for a public hearing. The City Council shall then approve or deny the appeal, and confirm or overturn the decision of the Planning Commission. Decisions of the City Council are final.



HARVEST LANDING SPECIFIC PLAN IMPLEMENTATION AND ADMINISTRATION

7.3 Specific Plan Amendment Process

7.3.1 Specific Plan Amendments

Any change to the Specific Plan boundaries, land use designations, land use allowances, development criteria, circulation plan, public facility plan, or other major component will require a Specific Plan Amendment. The Planning Commission is authorized to review and recommend either approval or denial to the City Council. The City Council is authorized to approve or disapprove any proposed requests.

7.3.2 Substantial Conformance Determinations

The City recognizes that existing site conditions may constrain the extent to which the development standards and guidelines set forth in this Specific Plan can be met. The Substantial Conformance Determination process is established to provide a means for the Director or his/her designee to approve certain development projects, as defined in this Section, that will not cause or create any of the following conditions:

1. Alter or contravene the permitted uses of the Property;
2. Increase the square footage of any Project building or the total Project building square footage in a manner that substantially alters the intent of the original concept as approved in the Existing Development Approvals;
3. Modify the site layout of any portion of the Project in a manner that substantially alters the intent of the original concept as approved in the Existing Development Approvals;
4. Increase the height of any Project building, if such increase in height would exceed the requirements specified in the Existing Land Use Regulations, except as otherwise provided by this Agreement, including the Reservations of Authority;
5. Delete any requirement for the reservation or dedication of land for public purposes within the Property, unless such deletion is warranted due to a decrease in the Project's square footage;
6. The creation of new nonconformities, such as, but not limited to, a decrease in the number of on-site parking spaces below the required minimum, reduction of the water retention areas to less than the minimum required to achieve NPDES compliance, constriction in the required vehicular access or fire lanes, or reduction of handicapped accessibility; or
7. Require a subsequent or supplemental Environmental Impact Report pursuant to Section 21166 of the Public Resources Code

The Substantial Conformance Determination process is established to provide a means for the Director or his/her designee to approve certain development projects that involve minor modifications, including but not limited to:

1. Modification of any site plan or design element in this Specific Plan that improves circulation, reduces grading, improves drainage, or enhances infrastructure.
2. Minor changes to the architectural or landscape guidelines or sign standards for interior signs.



HARVEST LANDING SPECIFIC PLAN IMPLEMENTATION AND ADMINISTRATION

3. Changes to the internal circulation plan—including alignments, width, or improvements—that do not adversely affect external circulation patterns.
4. Changes in utility and/or public service providers or the location or size of facilities needed to support the approved land use plan.
5. Changes to site plan, architectural building or entry design that are consistent with the development standards and design criteria of this Specific Plan.
6. Refinements to Specific Plan language which increase clarity and do not change policy intent.

Any request for a Substantial Conformance Determination shall be in the form of a letter to the Director describing the proposed deviation from the standards or guidelines of this Specific Plan. Minor modifications to the Specific Plan, as defined above, shall not require a Specific Plan Amendment and will be subject to the City established Minor Modification requirements, application, and fee.

Within 30 days of receiving the letter and any applicable fees, the Director or his/her designee shall review the proposal for Substantial Conformance with the standards and guidelines of this Specific Plan and shall issue a written determination and findings as to how the proposal complies or does not comply with the provisions of this Specific Plan.

A written record of such Substantial Conformance determinations shall be maintained on file with the official copy of the adopted Specific Plan, or the Specific Plan document may be modified. No public notice and no public hearing shall be required. If the proposal is found to be in substantial conformance, no further action on the part of the Director or the applicant is required.



HARVEST LANDING SPECIFIC PLAN LAND USE MAP WITH RESIDENTIAL DENSITY BOUNDARIES

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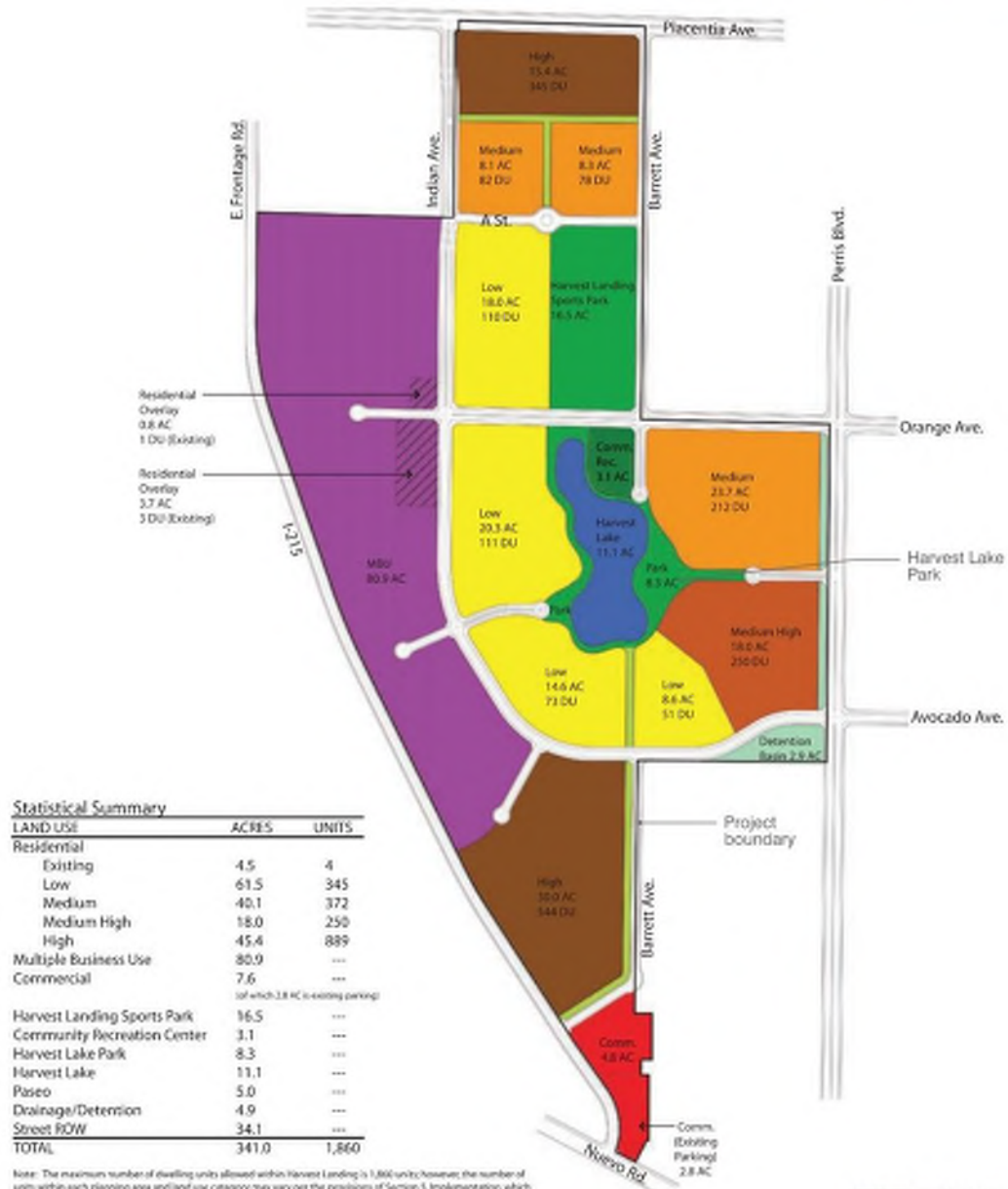
HARVEST LANDING SPECIFIC PLAN - APPENDIX A



HARVEST LANDING SPECIFIC PLAN - APPENDIX A



LAND USE PLAN



Statistical Summary

LAND USE	ACRES	UNITS
Residential		
Existing	4.5	4
Low	61.5	345
Medium	40.1	372
Medium High	18.0	250
High	45.4	889
Multiple Business Use	80.9	---
Commercial	7.6	---
Harvest Landing Sports Park	16.5	---
Community Recreation Center	3.1	---
Harvest Lake Park	8.3	---
Harvest Lake	11.1	---
Paseo	5.0	---
Drainage/Detention	4.9	---
Street ROW	34.1	---
TOTAL	341.0	1,860

Note: The maximum number of dwelling units allowed within Harvest Landing is 1,860 units; however, the number of units within each planning area and land use category may vary per the provisions of Section 5. Implementation, which allows for the transfer of residential density. Therefore, this land plan depicts the intended development pattern and subsequent transfer of units may result in the minor shifting of the residential land use category and/or residential units shown in each planning area. The units depicted on this plan are used to determine total units not the maximum density of each land use category.





SIGNAGE PROGRAM

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Harvest Landing Retail Center Entitlement Signage Criteria

November 17, 2025



432 Culver Boulevard,
Playa Del Rey, CA 90293
310-822-5223
SelbertPerkins.com

AZURE
DEVELOPMENT

944 S Greenwood Avenue
Montebello, CA 90640
323-477-1160

Contents

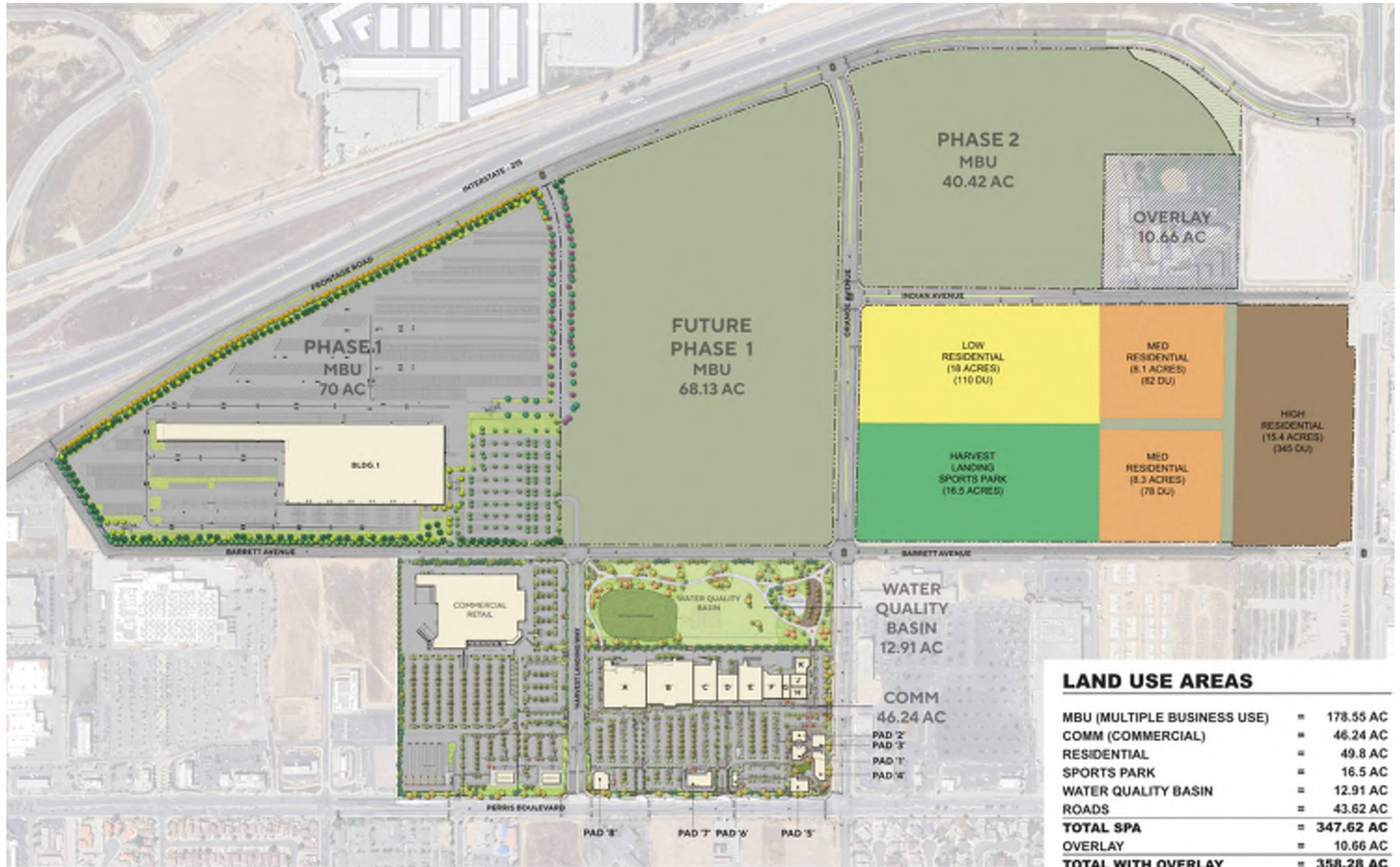
- 3 Site
- 4 Sign Types Summary Diagram
- 5 Sign Regulations Summary

Proposed Sign Locations

- 7 Sign Type S1 – Locations
- 8 Freeway Identity - Design & Placement
- 9 Sign Locations: Public
- 10 Tenant Sign Locations: Building A East Elevation
- 11 Tenant Sign Locations: Building A North & South Elevations
- 12 Tenant Sign Locations: Pad 1 Elevations
- 13 Tenant Sign Locations: Pad 2 Elevations
- 14 Tenant Sign Locations: Pad 3 Elevations
- 15 Tenant Sign Locations: Pad 4 Elevations
- 16 Tenant Sign Locations: Pad 5 Elevations
- 17 Tenant Sign Locations: Pad 6 Elevations
- 18 Tenant Sign Locations: Pad 7 Elevations
- 19 Tenant Sign Locations: Pad 8 Elevations

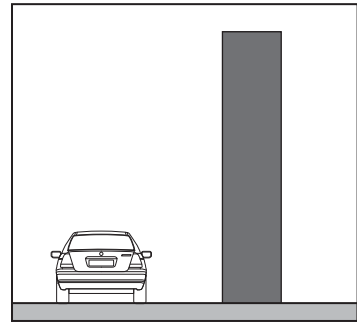
Signage Criteria & Reference

- 21 Site Signage: S1 Freeway Monument/Pylon Identity
- 22 Site Signage: S2 Site Monument/Pylon Identity
- 23 Tenant Signage: T1 Awning/Marquee
- 24 Tenant Signage: T2 Freestanding Monument/Pylon
- 25 Tenant Signage: TB Blade
- 26 Tenant Signage: T4 & T5 Wall
- 27 Tenant Signage: T6 Window

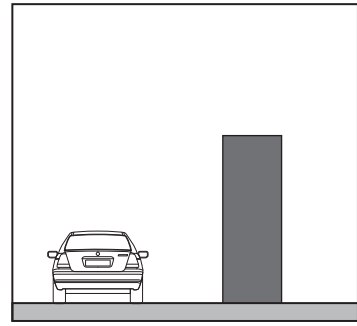


LAND USE AREAS	
MBU (MULTIPLE BUSINESS USE)	= 178.55 AC
COMM (COMMERCIAL)	= 46.24 AC
RESIDENTIAL	= 49.8 AC
SPORTS PARK	= 16.5 AC
WATER QUALITY BASIN	= 12.91 AC
ROADS	= 43.62 AC
TOTAL SPA	= 347.62 AC
OVERLAY	= 10.66 AC
TOTAL WITH OVERLAY	= 358.28 AC

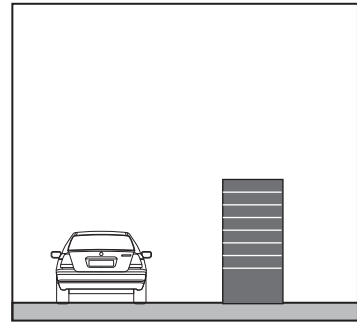
SITE IDENTITY SIGNS [S]



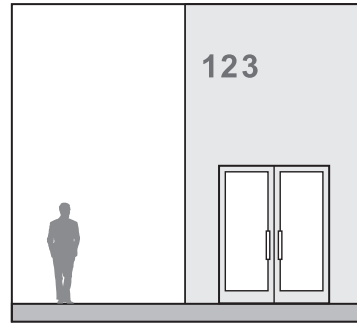
S1 FREEWAY MONUMENT/PYLON IDENTITY



S2 FREESTANDING SITE MONUMENT/PYLON IDENTITY

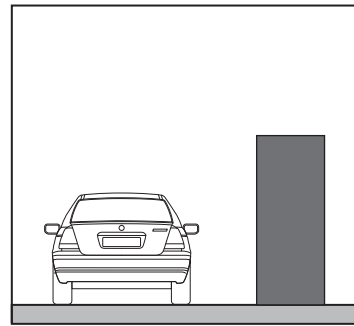


S3 MULTI-TENANT MONUMENT IDENTITY

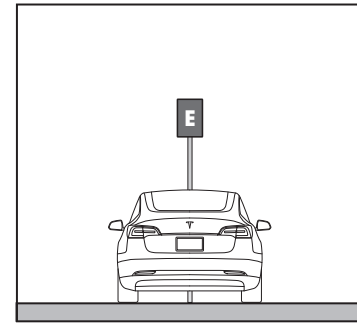


S4 BUILDING ADDRESS

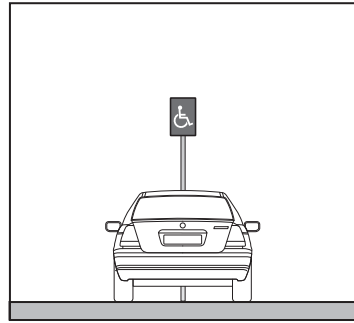
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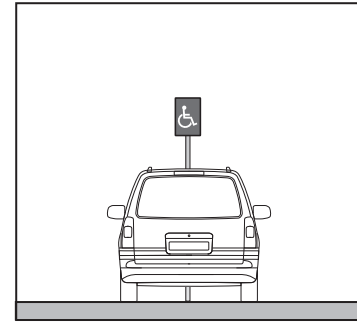
R1 VEHICLE PARKING REGULATIONS



R2 EV PARKING

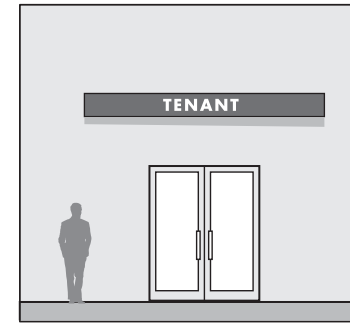


R3 ACCESSIBLE PARKING

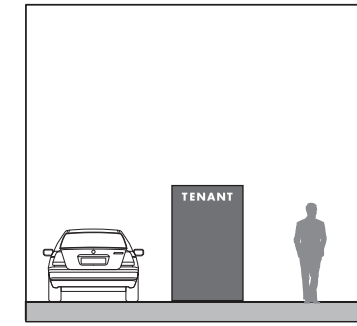


R4 ACCESSIBLE VAN PARKING

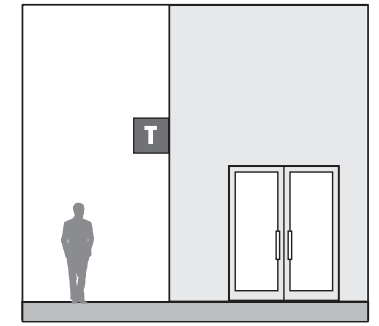
TENANT IDENTITY SIGNS [T]



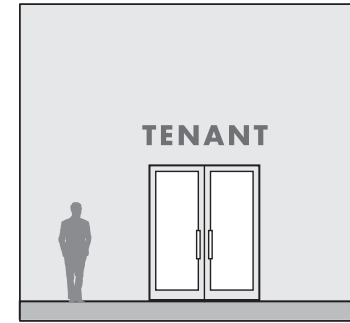
T1 TENANT AWNING/MARQUEE IDENTITY



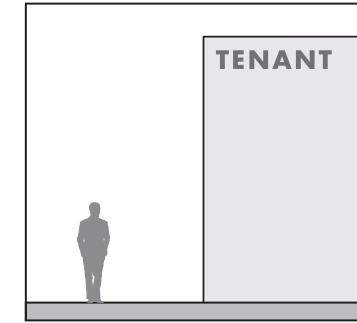
T2 TENANT FREESTANDING MONUMENT/PYLON IDENTITY



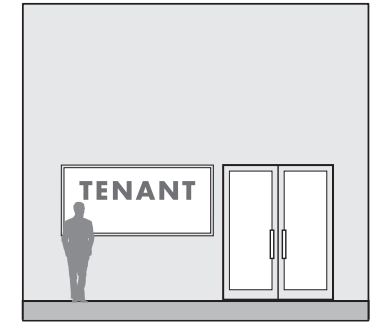
T3 TENANT PROJECTING IDENTITY



T4 TENANT WALL IDENTITY

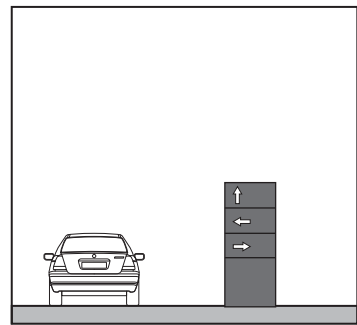


T5 TENANT WALL IDENTITY - HIGH

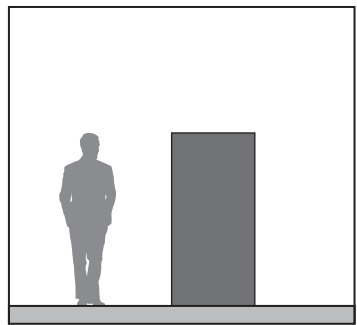


T6 TENANT WINDOW IDENTITY

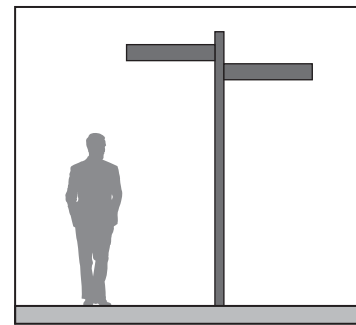
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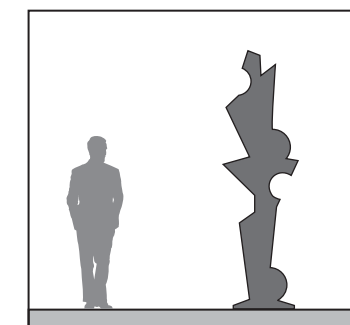


D2 PEDESTRIAN DIRECTORY/MAP



D3 PEDESTRIAN DIRECTIONAL

AMENITY SIGNS [E]



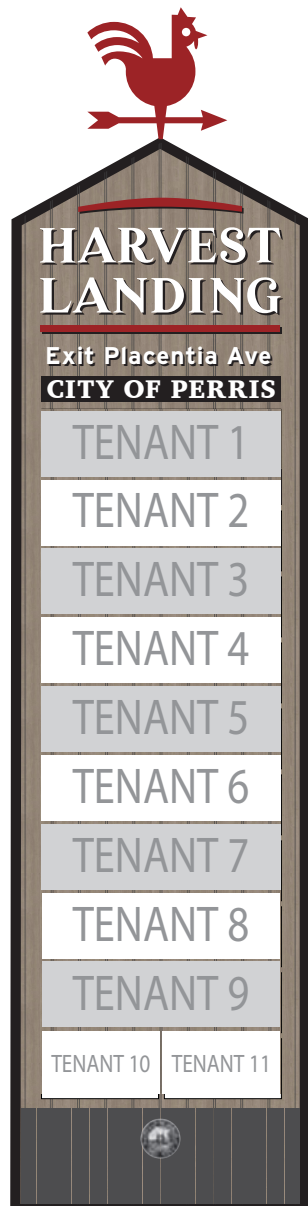
E1 ART ELEMENT

Signage defined by City of Perris Code of Ordinances

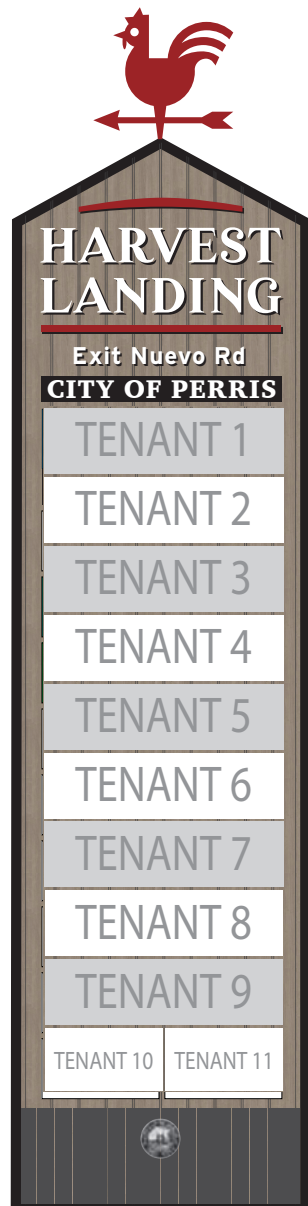
PERRIS CODE OF ORDINANCES: CHAPTER 19.75 SIGN REGULATIONS SUMMARY

SIGN ID	SIGN TYPE	DESCRIPTION	MAX. SIZE ALLOWANCE	MAX. QUANTITY	ILLUMINATION	NOTES	CODE REFERENCE
SITE IDENTITY SIGNS							
S1	FREEWAY	site identity / electronic display / tenant directory	<ul style="list-style-type: none"> 100 sf / 25' height 100 sf / 50' height within 300 ft of freeway 	1 per parcel/site	yes	<ul style="list-style-type: none"> must be located within 660 ft of freeway edge line support structure must be min. 30% of sign width electronic sign max size = 50% of sign area 	19.75.100 (b4)
S2	FREESTANDING MONUMENT / PYLON	site identity / tenant directory	<ul style="list-style-type: none"> 1–5 acres: max 32 sf / 5' ht 5–15 acres: max 50 sf / 6' ht or max 100 sf / 20' ht 15–25 acres: max 75 sf / 8' ht max 150 sf / 20' ht 	2 per site	yes	<ul style="list-style-type: none"> 1 sign per street frontage of site must be located 5' or more from property line 	19.75.100 (b1.c2)
TENANT IDENTITY SIGNS							
T1	TENANT AWNING / MARQUEE	tenant identity	<ul style="list-style-type: none"> max. letter height 8" max. symbol/logo height 12" 	unspecified	external	<ul style="list-style-type: none"> max size not to exceed 80% of awning/marquee width permitted on building facades adjacent to walkway only allowed on front of awning only, all side of marquee signage area included in total allowable for building frontage 	19.75.100 (a3)
T2	TENANT FREESTANDING MONUMENT / PYLON	tenant identity	<ul style="list-style-type: none"> 1–10,000 sf: max 20 sf / 4' ht 10,000–30,000 sf: max 32 sf / 5' ht 30,000+ sf: max 50 sf / 5' ht 	2	unspecified	<ul style="list-style-type: none"> 1 sign per street frontage of building allowed in addition to wall signs 	19.75.100 (b1.c1)
T3	TENANT UNDER-CANOPY	tenant identity / projecting blade	9 sf	1	unspecified	<ul style="list-style-type: none"> propose blade identity 	19.75.100 (a4)
T4	TENANT WALL	tenant identity	max. sq. ft. = tenant frontage lf x 1 sf (not to exceed 200 sf)	2	unspecified	<ul style="list-style-type: none"> size not to exceed 80% of linear building width * sum total allowance not to exceed 200 sf for all signs 	19.75.100 (a1.a-c)
T5	TENANT WALL: HIGH	tenant identity	<ul style="list-style-type: none"> 2–4 stories high: max 100 sf 5–6 stories high: max 150 sf 7–10 stories high: max 250 sf 	1 per frontage	unspecified	<ul style="list-style-type: none"> no signs on stories 1–2 allowed with this sign type size not to exceed 80% of linear building width must be placed near top of building 	19.75.100 (a1.d)
T6	TENANT WINDOW	tenant identity	9 sf	1 per public entrance	unspecified	no surface painted signs	19.75.100 (a2)

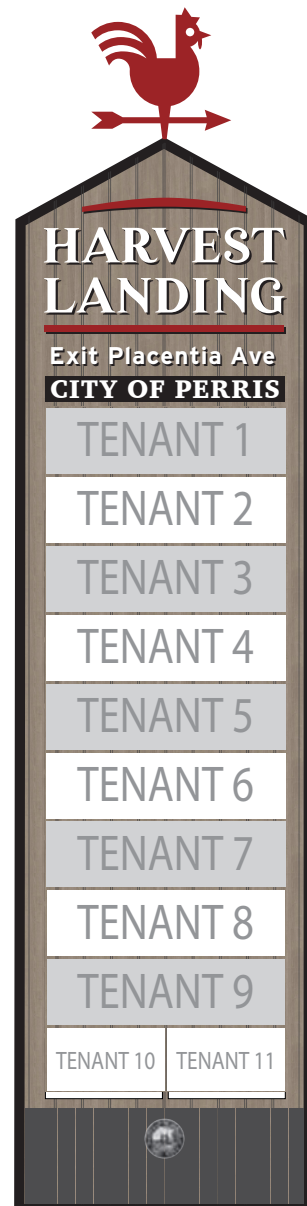
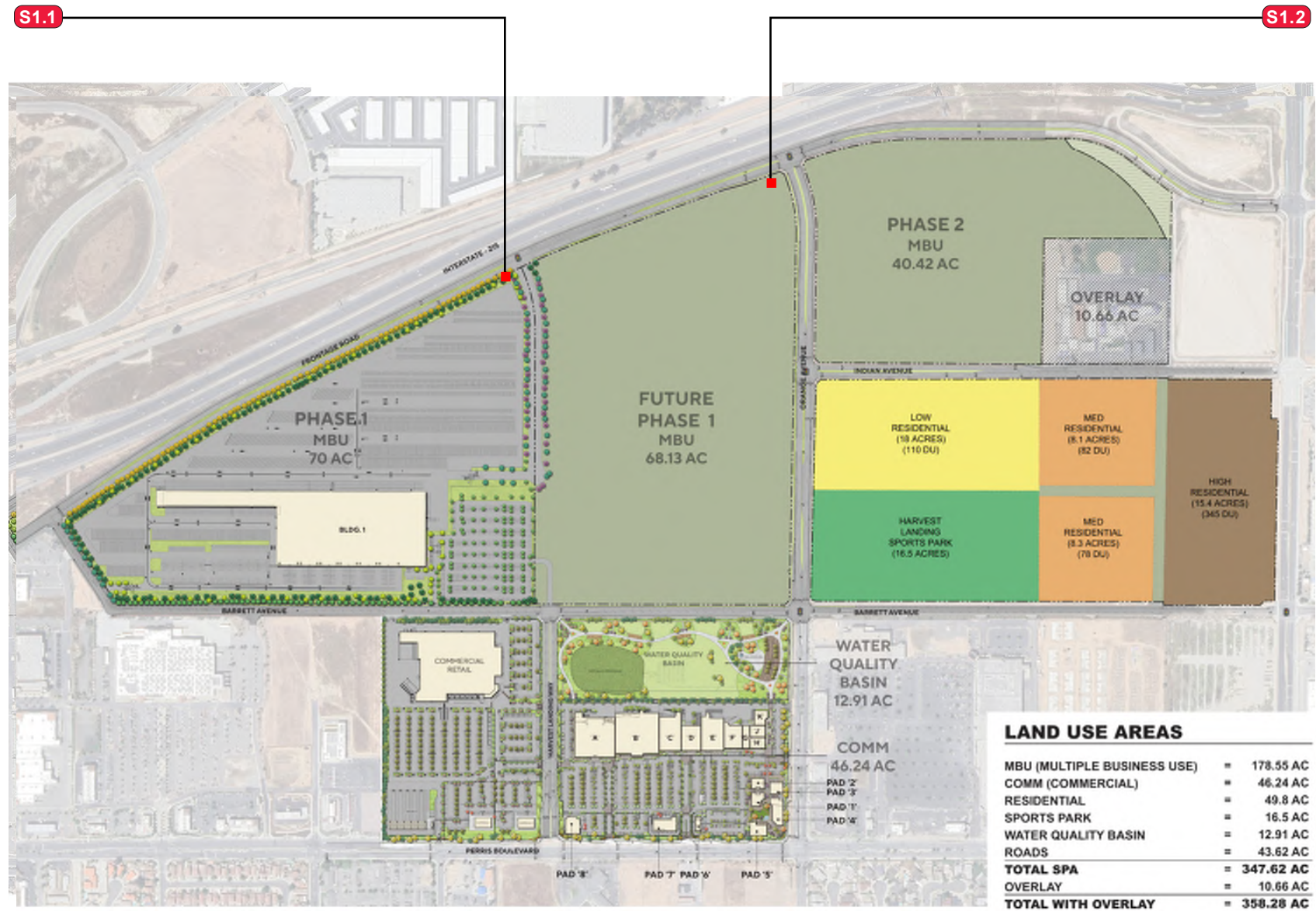
Proposed Sign Locations



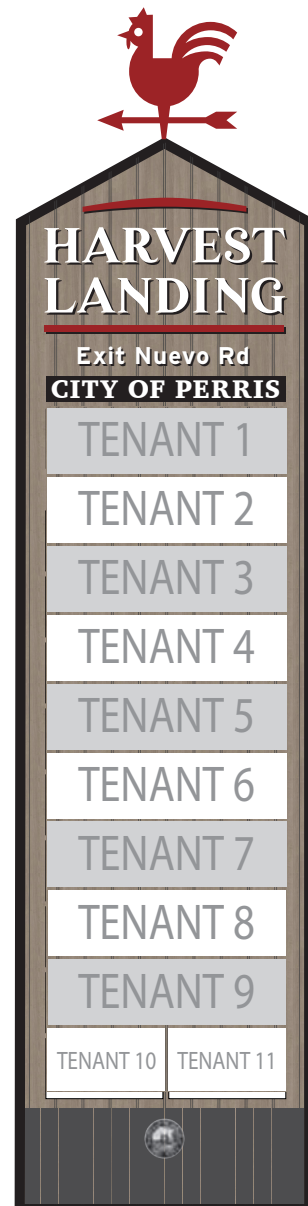
SOUTH SIDE (A)



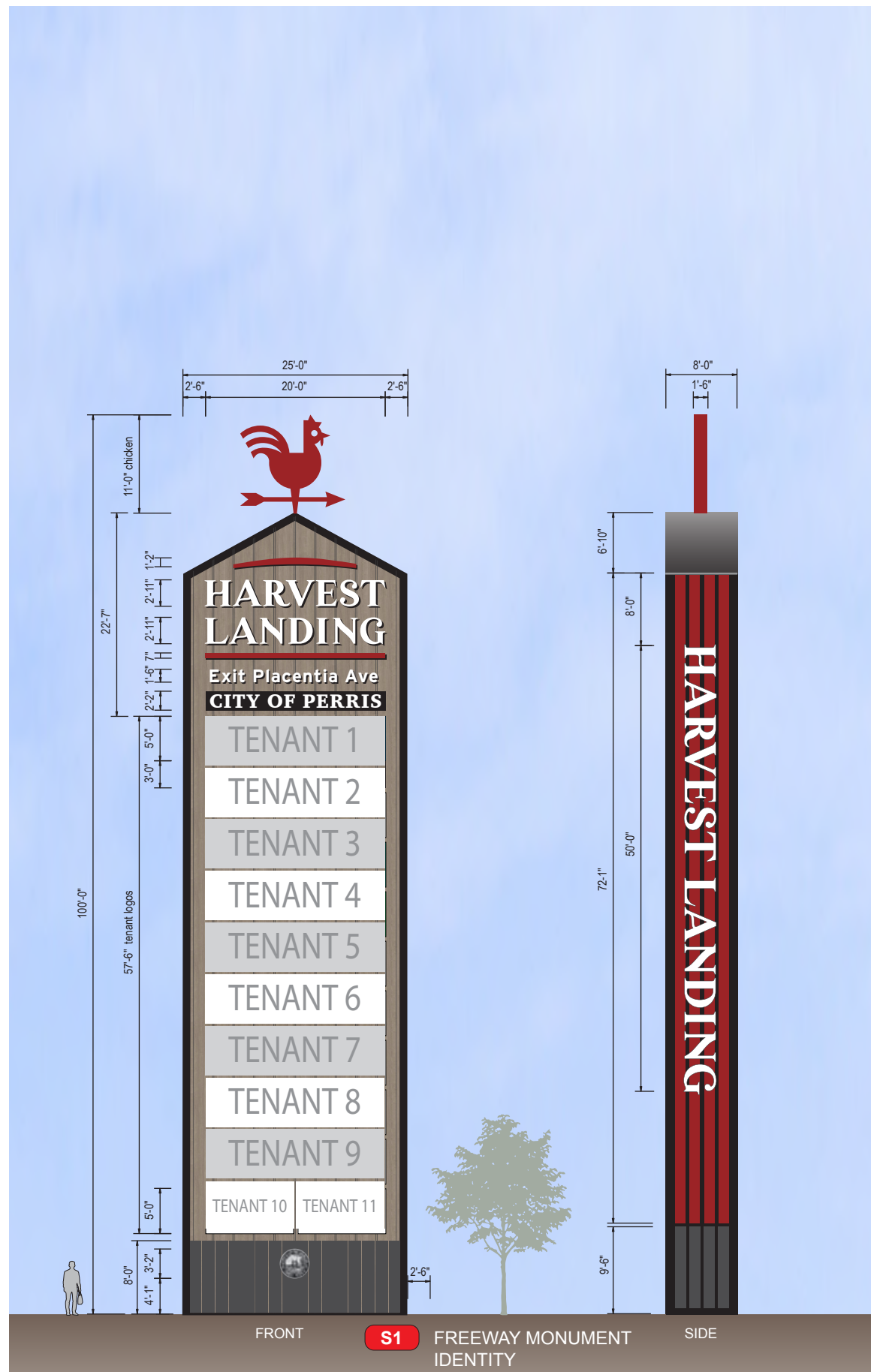
NORTH SIDE (B)

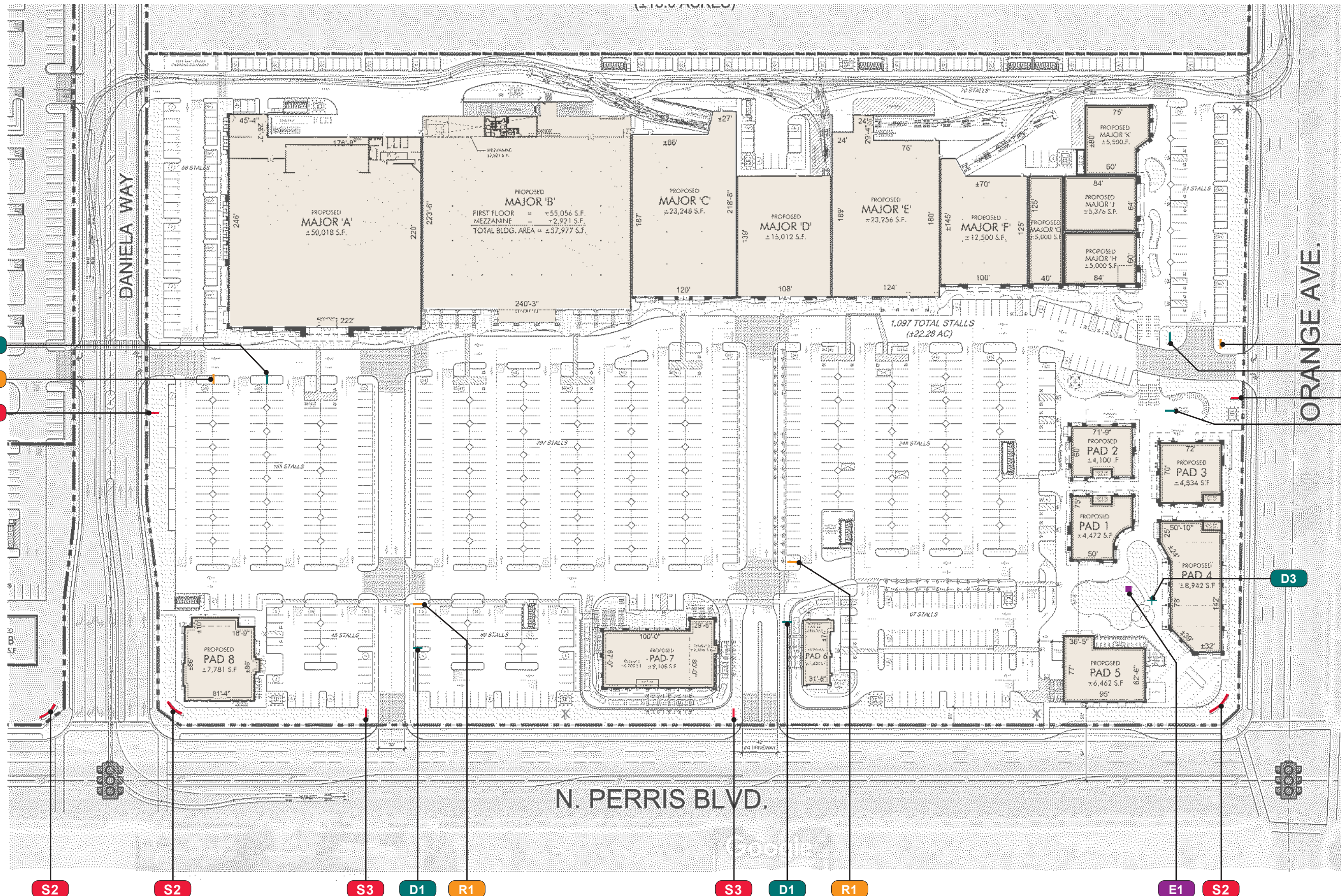


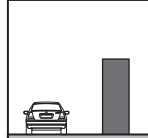
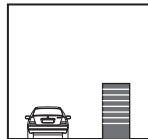
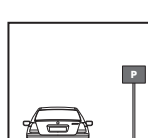


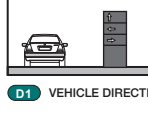
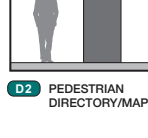
SOUTH SIDE (A)



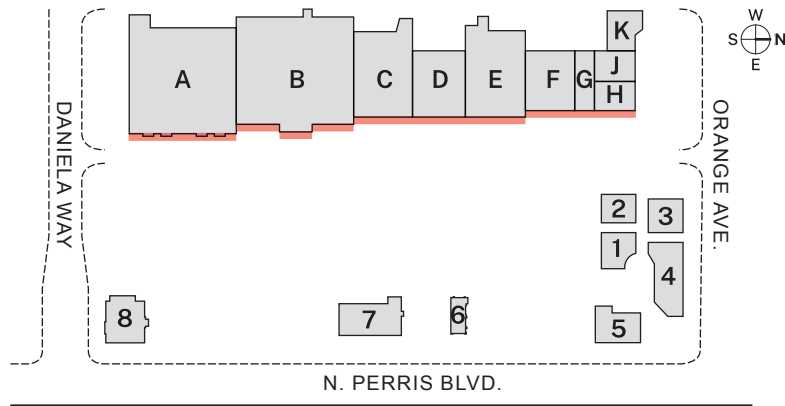
NORTH SIDE (B)





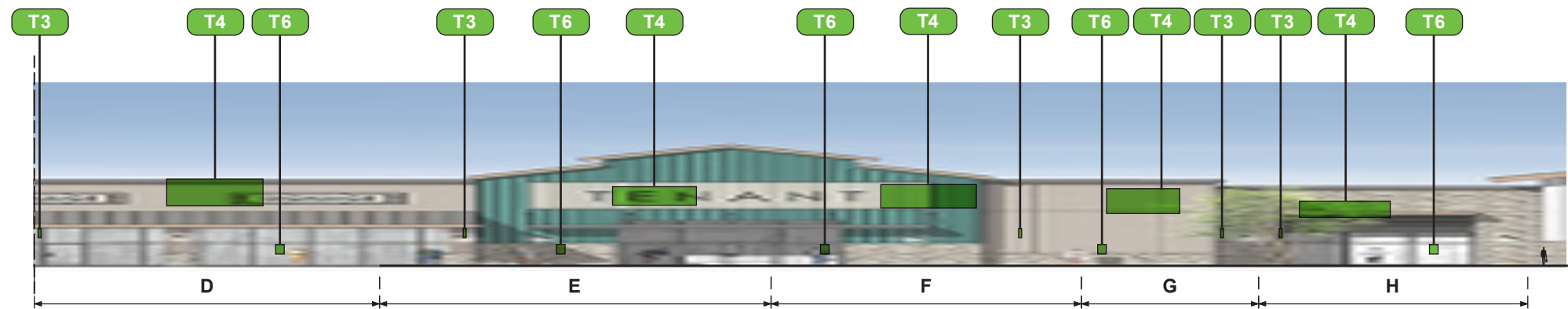
-  **S2** FREESTANDING SITE MONUMENT/PYLON IDENTITY
-  **S3** MULTI-TENANT MONUMENT IDENTITY
-  **R1** VEHICLE PARKING REGULATIONS
-  **D1** VEHICLE DIRECTION
-  **D2** PEDESTRIAN DIRECTORY/MAP
-  **D3** PEDESTRIAN DIRECTIONAL
-  **E1** ART ELEMENT

KEY PLAN



PROPOSED SIGNAGE

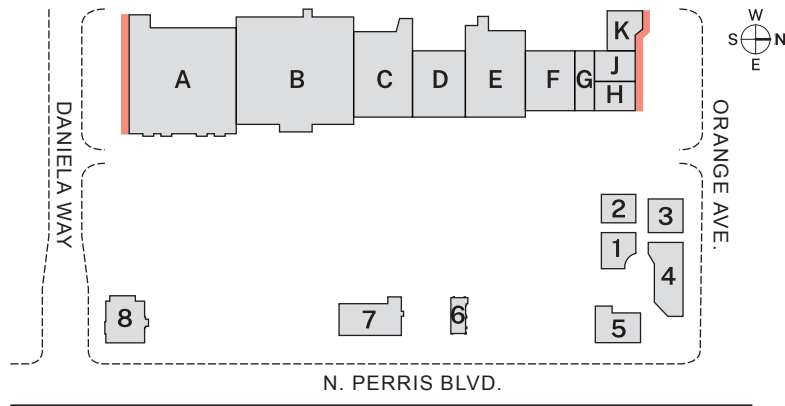
TENANT	FRONTAGE LINEAR FT	T3 BLADE (4 SF MAX.)	T4 WALL (SF FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
A	222	4	222	9
B	200	4	200	9
C	120	4	120	9
D	108	4	108	9
E	124	4	124	9
F	100	4	100	9
G	40	4	40	9
H	84	4	84	9



BUILDING A / EAST ELEVATION

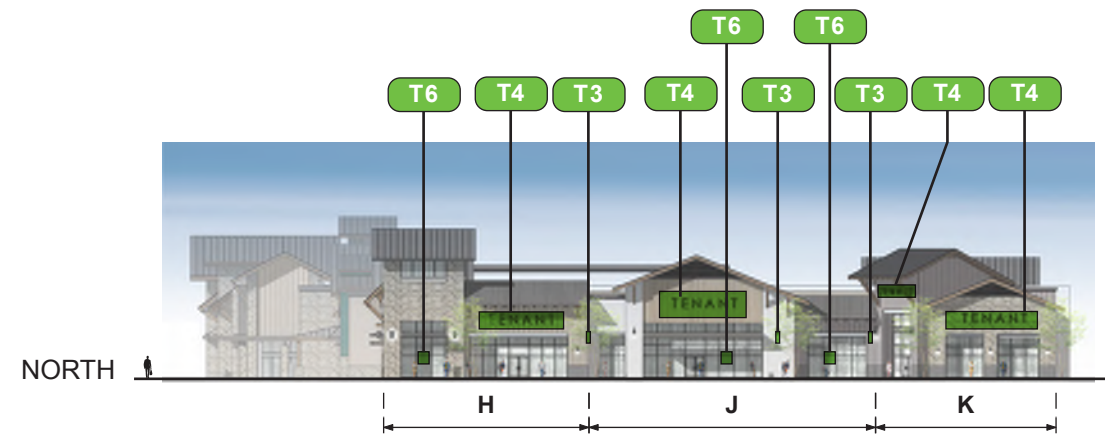
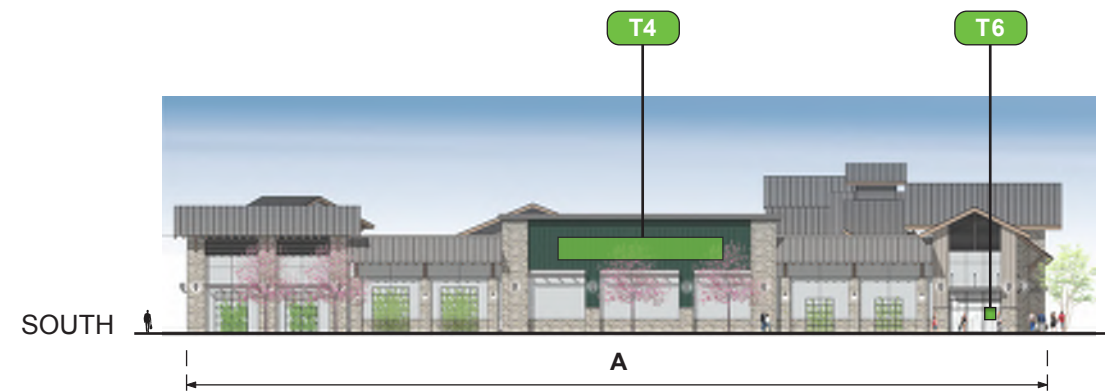
NTS

KEY PLAN



PROPOSED SIGNAGE

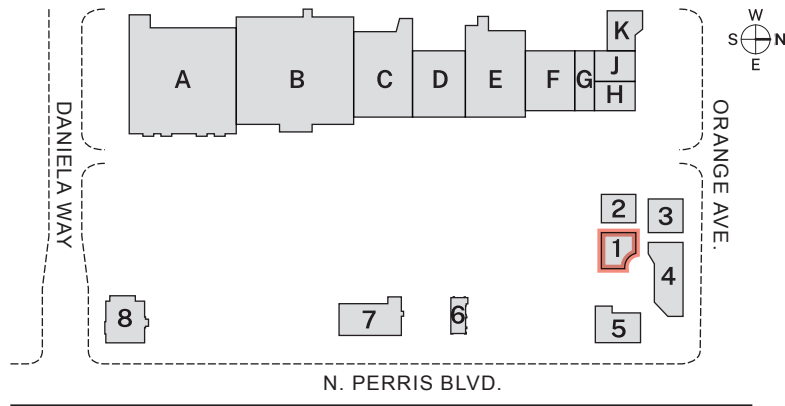
TENANT	FRONTAGE LINEAR FT	T3 BLADE (4 SF MAX.)	T4 WALL (SF FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
A	246	n/a	246	9
H	60	n/a	60	9
J	64	n/a	64	9
K	80	n/a	80	9



BUILDING A / NORTH & SOUTH ELEVATIONS

NTS

KEY PLAN



PROPOSED SIGNAGE – EAST

TENANT	FRONTAGE LINEAR FT	T1 AWNING (FACTOR 1:1)	T4 WALL (FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
A	39	n/a	39	9
B	35	n/a	n/a	

PROPOSED SIGNAGE – WEST

TENANT	FRONTAGE LINEAR FT	T1 AWNING (FACTOR 1:1)	T4 WALL (FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
A	38	n/a	38	9
B	38	n/a	38	9

PROPOSED SIGNAGE – NORTH

TENANT	FRONTAGE LINEAR FT	T1 AWNING (FACTOR 1:1)	T3 BLADE (4 SF MAX.)	T4 WALL (FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
B	80	80	4	80	9

PROPOSED SIGNAGE – SOUTH

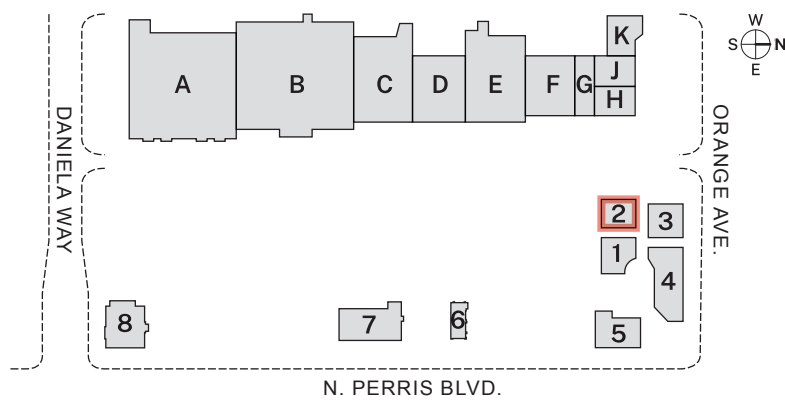
TENANT	FRONTAGE LINEAR FT	T1 AWNING (FACTOR 1:1)	T4 WALL (FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
A	78	78	78	9



PAD 1 ELEVATIONS

NTS

KEY PLAN



PROPOSED SIGNAGE – EAST

TENANT	FRONTAGE LINEAR FT	T3 BLADE (4 SF MAX.)	T4 WALL (FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
A	34	n/a	34	9
B	41	n/a	41	9

PROPOSED SIGNAGE – WEST

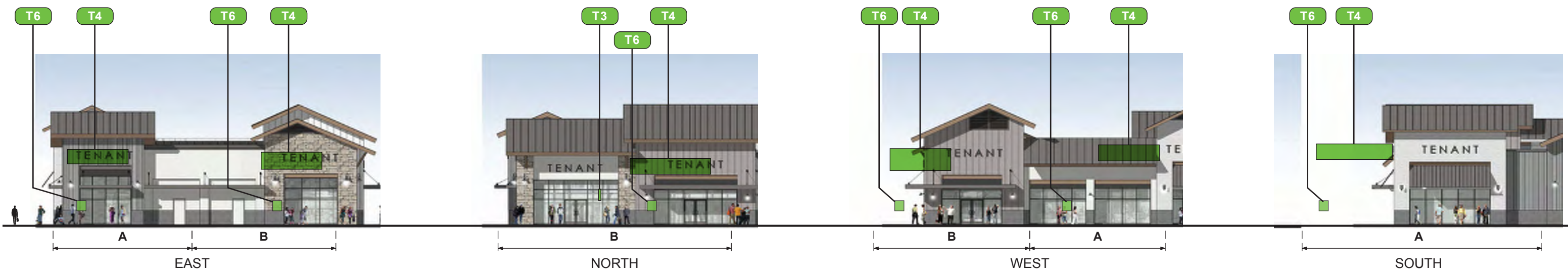
TENANT	FRONTAGE LINEAR FT	T3 BLADE (4 SF MAX.)	T4 WALL (FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
A	36	n/a	36	9
B	42	n/a	42	9

PROPOSED SIGNAGE – NORTH

TENANT	FRONTAGE LINEAR FT	T3 BLADE (4 SF MAX.)	T4 WALL (FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
B	65	4	65	9

PROPOSED SIGNAGE – SOUTH

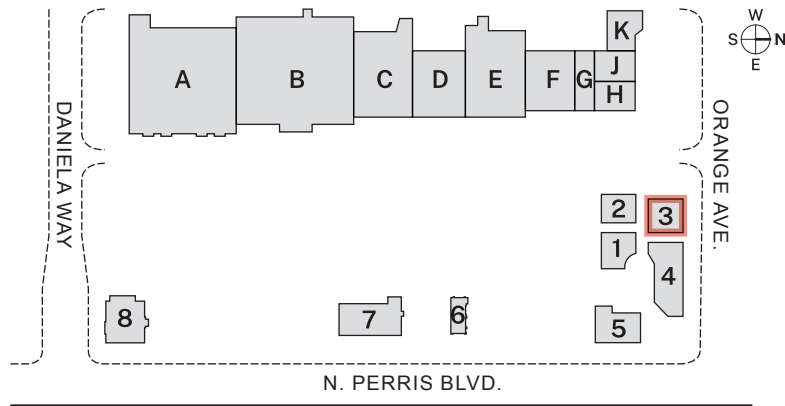
TENANT	FRONTAGE LINEAR FT	T3 BLADE (4 SF MAX.)	T4 WALL (FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
A	67	n/a	65	9



Note: signs face alley

PAD 2 ELEVATIONS

KEY PLAN



PROPOSED SIGNAGE – EAST

TENANT	FRONTAGE LINEAR FT	T4 WALL (FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
A	77	77	9

PROPOSED SIGNAGE – WEST

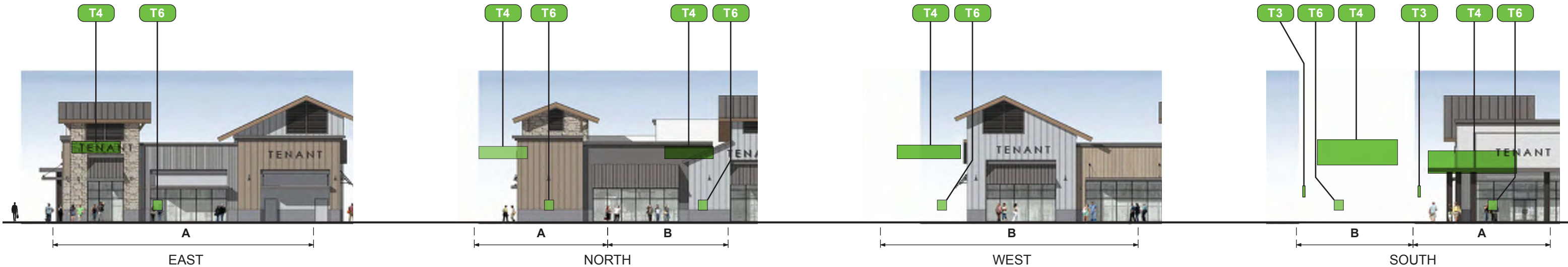
TENANT	FRONTAGE LINEAR FT	T4 WALL (FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
A	76	76	9

PROPOSED SIGNAGE – NORTH

TENANT	FRONTAGE LINEAR FT	T4 WALL (FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
A	39	39	9
B	36	36	9

PROPOSED SIGNAGE – SOUTH

TENANT	FRONTAGE LINEAR FT	T3 BLADE (4 SF MAX.)	T4 WALL (FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
A	39	4	39	9
B	36	4	36	9

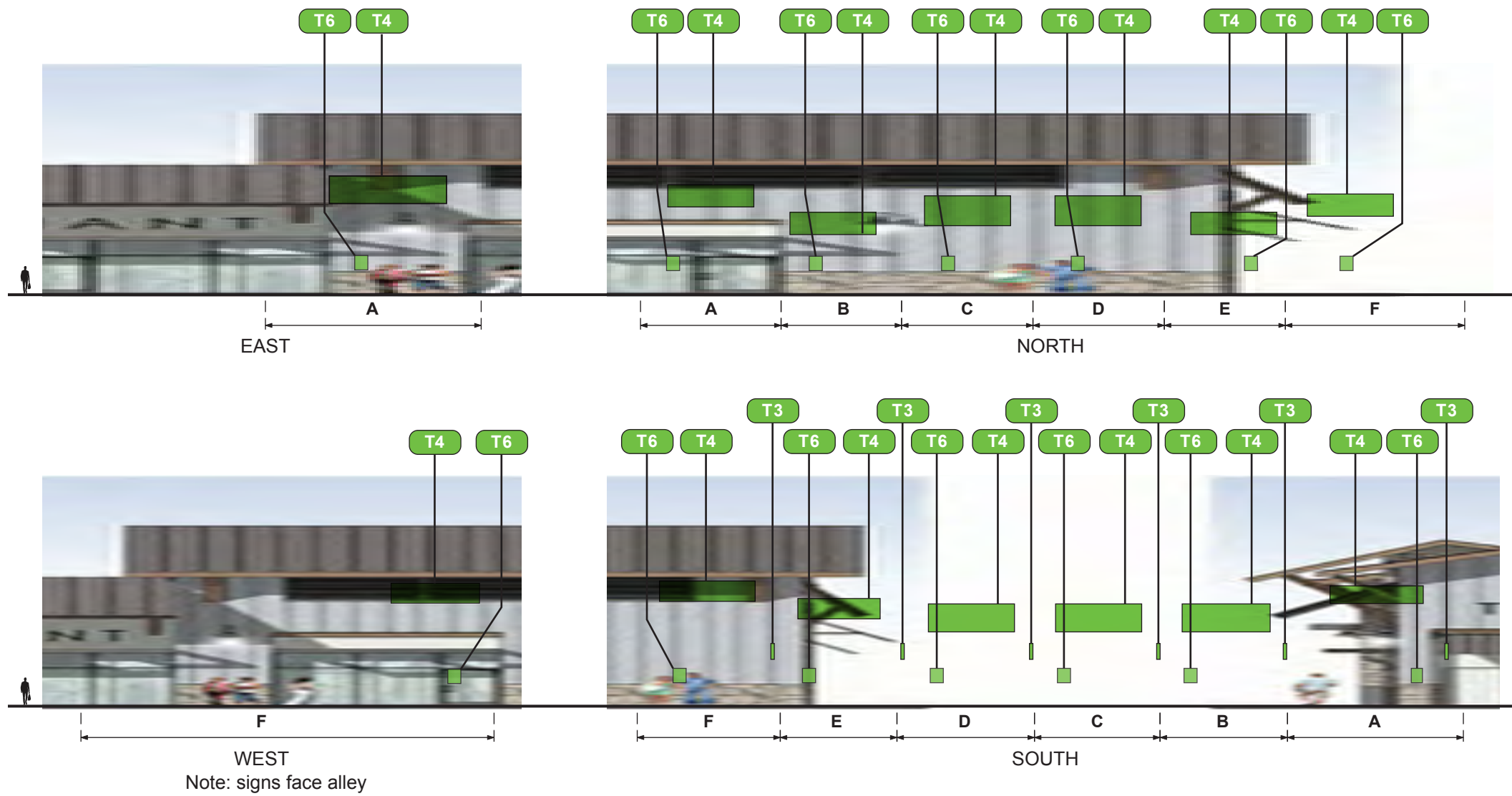
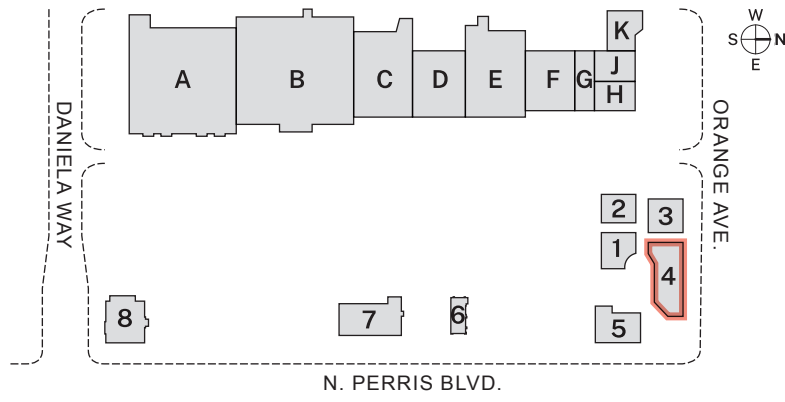


Note: signs face alley

PAD 3 ELEVATIONS

NTS

KEY PLAN



PAD 4 ELEVATIONS

NTS

PROPOSED SIGNAGE – EAST

TENANT	FRONTAGE LINEAR FT	T3 BLADE (4 SF MAX.)	T4 WALL (FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
A	37	n/a	37	9

PROPOSED SIGNAGE – WEST

TENANT	FRONTAGE LINEAR FT	T3 BLADE (4 SF MAX.)	T4 WALL (FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
A	75	n/a	75	9

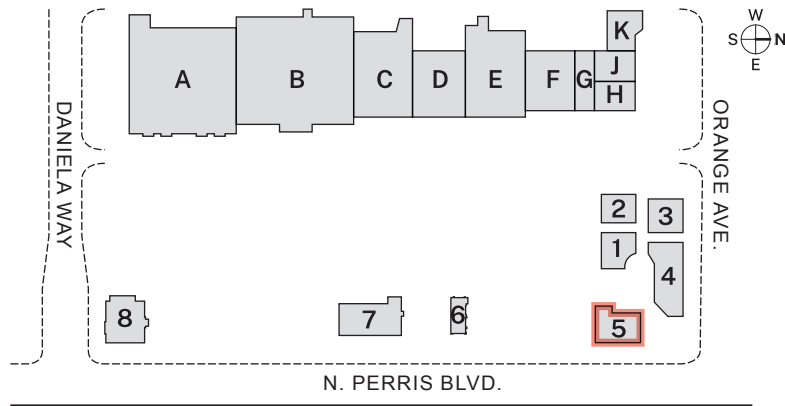
PROPOSED SIGNAGE – NORTH

TENANT	FRONTAGE LINEAR FT	T3 BLADE (4 SF MAX.)	T4 WALL (FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
A	33	n/a	33	9
B	24	n/a	24	9
C	24	n/a	24	9
D	24	n/a	24	9
E	23	n/a	23	9
F	26	n/a	26	9

PROPOSED SIGNAGE – SOUTH

TENANT	FRONTAGE LINEAR FT	T3 BLADE (4 SF MAX.)	T4 WALL (FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
A	41	4	41	9
B	24	4	24	9
C	24	4	24	9
D	24	4	24	9
E	23	4	23	9
F	27	4	27	9

KEY PLAN



PROPOSED SIGNAGE – EAST

TENANT	FRONTAGE LINEAR FT	T3 BLADE (4 SF MAX.)	T4 WALL (FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
	95	n/a	95	9

PROPOSED SIGNAGE – WEST

TENANT	FRONTAGE LINEAR FT	T3 BLADE (4 SF MAX.)	T4 WALL (FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
	95	n/a	95	9

PROPOSED SIGNAGE – NORTH

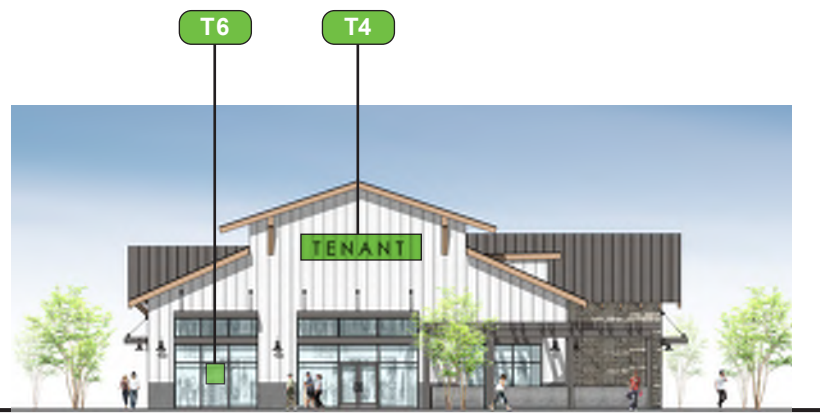
TENANT	FRONTAGE LINEAR FT	T3 BLADE (4 SF MAX.)	T4 WALL (FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
	77	n/a	77	9

PROPOSED SIGNAGE – SOUTH

TENANT	FRONTAGE LINEAR FT	T3 BLADE (4 SF MAX.)	T4 WALL (FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
	77	n/a	77	n/a



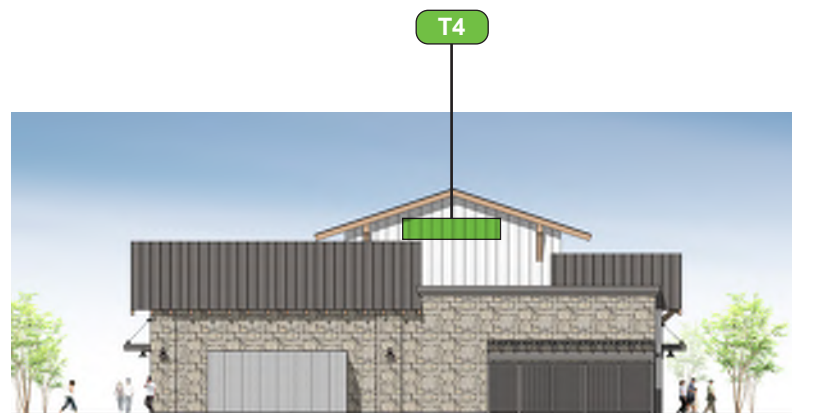
EAST



NORTH



WEST

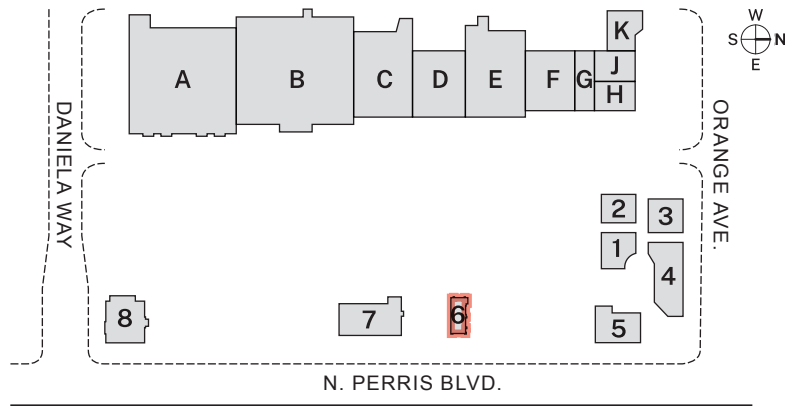


SOUTH

PAD 5 ELEVATIONS

NTS

KEY PLAN



PROPOSED SIGNAGE – EAST

TENANT	FRONTAGE LINEAR FT	T3 BLADE (4 SF MAX.)	T4 WALL (FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
	32	n/a	32	9

PROPOSED SIGNAGE – WEST

TENANT	FRONTAGE LINEAR FT	T3 BLADE (4 SF MAX.)	T4 WALL (FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
	32	n/a	32	n/a

PROPOSED SIGNAGE – NORTH

TENANT	FRONTAGE LINEAR FT	T3 BLADE (4 SF MAX.)	T4 WALL (FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
	75	n/a	75	n/a

PROPOSED SIGNAGE – SOUTH

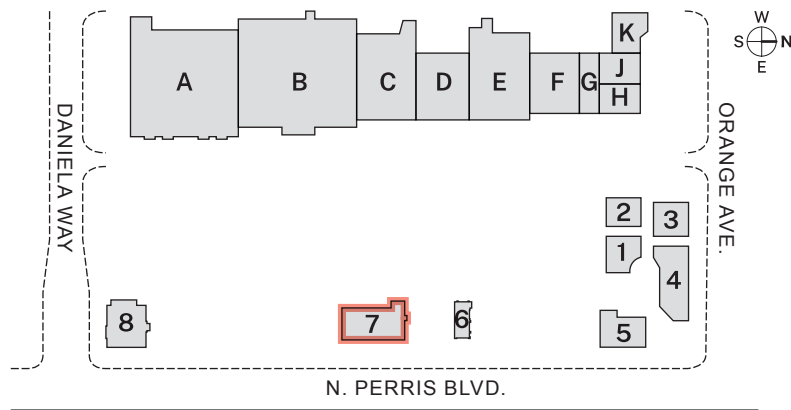
TENANT	FRONTAGE LINEAR FT	T3 BLADE (4 SF MAX.)	T4 WALL (FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
	75	n/a	75	9



PAD 6 ELEVATIONS

NTS

KEY PLAN



PROPOSED SIGNAGE – EAST

TENANT	FRONTAGE LINEAR FT	T3 BLADE (4 SF MAX.)	T4 WALL (FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
A	27	n/a	27	n/a
B	24	n/a	24	n/a
C	24	n/a	24	n/a
D	20	n/a	20	n/a
E	35	n/a	35	n/a

PROPOSED SIGNAGE – WEST

TENANT	FRONTAGE LINEAR FT	T3 BLADE (4 SF MAX.)	T4 WALL (FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
A	27	n/a	27	9
B	24	n/a	24	9
C	24	n/a	24	9
D	20	n/a	20	9
E	35	n/a	35	9

PROPOSED SIGNAGE – NORTH

TENANT	FRONTAGE LINEAR FT	T3 BLADE (4 SF MAX.)	T4 WALL (FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
E	80	n/a	80	9

PROPOSED SIGNAGE – SOUTH

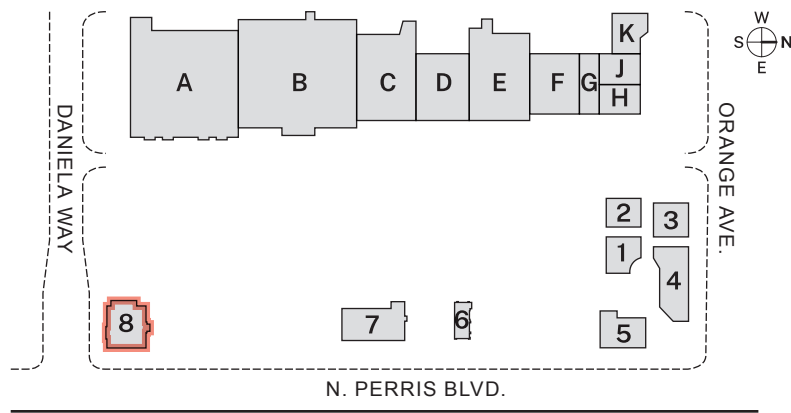
TENANT	FRONTAGE LINEAR FT	T3 BLADE (4 SF MAX.)	T4 WALL (FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
A	67	n/a	67	9



PAD 7 ELEVATIONS

NTS

KEY PLAN



PROPOSED SIGNAGE – EAST

TENANT	FRONTAGE LINEAR FT	T3 BLADE (4 SF MAX.)	T4 WALL (FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
	32	n/a	32	9

PROPOSED SIGNAGE – WEST

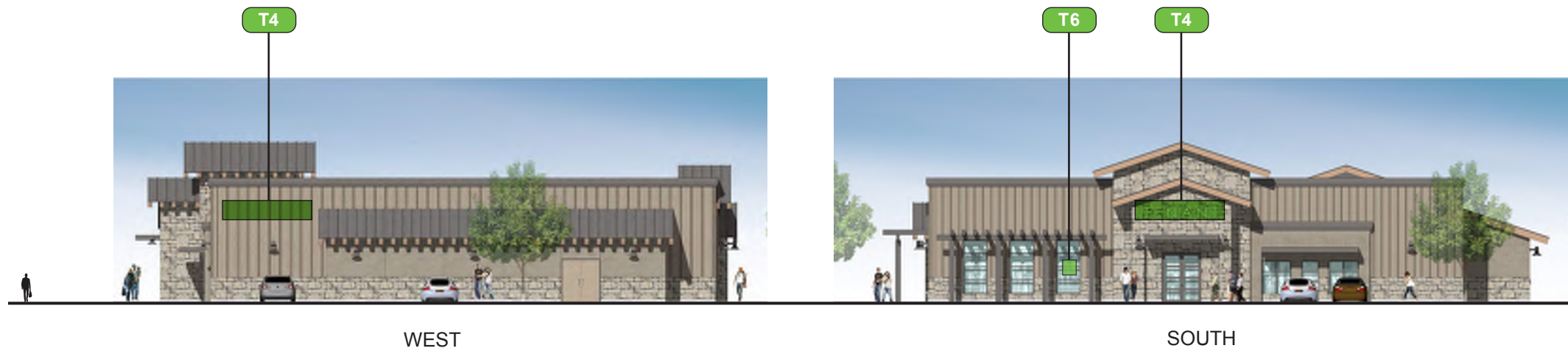
TENANT	FRONTAGE LINEAR FT	T3 BLADE (4 SF MAX.)	T4 WALL (FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
	32	n/a	32	n/a

PROPOSED SIGNAGE – NORTH

TENANT	FRONTAGE LINEAR FT	T3 BLADE (4 SF MAX.)	T4 WALL (FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
	75	n/a	75	9

PROPOSED SIGNAGE – SOUTH

TENANT	FRONTAGE LINEAR FT	T3 BLADE (4 SF MAX.)	T4 WALL (FACTOR 1:1)	T6 WINDOW (9 SF MAX.)
	75	n/a	75	9



PAD 8 ELEVATIONS

NTS

Signage Criteria & Reference

S1: FREEWAY MONUMENT/PYLON IDENTITY

Sec. 19.75.070 Defintion: FREEWAY SIGN

“Freeway sign means a sign located on the same parcel as the entity it identifies and such parcel is located within 660 feet of the nearest edge of a freeway right-of-way line.”

CODE CONSIDERATIONS (SEC. 19.75.100)

Sec. 19.75.100 (b4):

- 100 sf / 25’ height max
- 100 sf / 50’ height max within 300 ft off freeway

Sec. 19.75.100 (b4b):

“A maximum of one freeway sign shall be allowed per parcel or site.”

Sec. 19.75.100 (b4e):

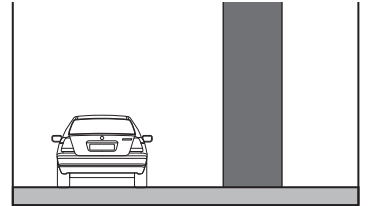
“Joint use freeway signs identifying more than one site/business may have a total sign area of 150 square feet . . .”

Sec. 19.75.100 (b4e2):

“Joint use signs may only identify businesses . . . not to exceed a total of two.”

Sec. 19.75.100 (b4e4):

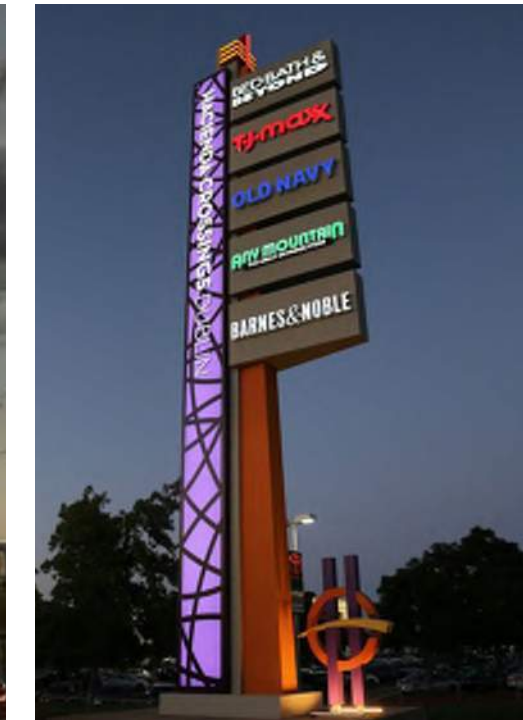
“Only one freeway sign shall be permitted per site/business irrespective of whether a singular or joint use freeway sign is used.”



S1 FREEWAY MONUMENT/PYLON IDENTITY



REFERENCE IMAGERY



S2: FREESTANDING MONUMENT/PYLON IDENTITY

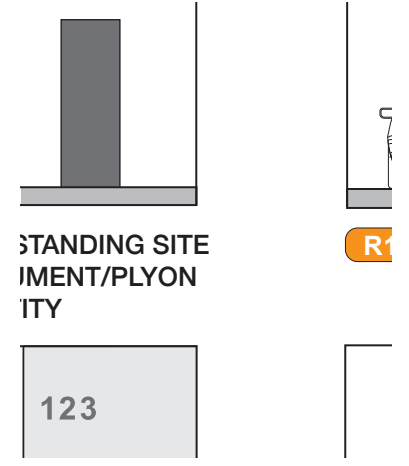
Sec. 19.75.070 Definition: FREESTANDING SIGN

“Freestanding sign means a sign permanently supported by one or more uprights, braces, poles, or other similar structural components attached to the ground or any foundation set in or upon the ground as a support base.”

CODE CONSIDERATIONS (SEC. 19.75.100)

Sec. 19.75.100 (b1c2): Complex Identification

- (i) “Small complex (less than 5 acres): Maximum of 2 monument signs, one per street frontage, with each sign area not to exceed 32 square feet and 5 feet in height from finished grade.”
- (ii) “Median complex (5–15 acres): Maximum of 2 monument or pylon signs, one per street frontage, with each sign area not to exceed 50 square feet and 6 feet in height for monument signs, and 100 square feet and 20 feet in height for pylon signs, as measured from finished grade.”
- (iii) “Large complex (15–25 acres): **Maximum of 2 monument or pylon signs, one per street frontage, with each sign area not to exceed 75 square feet and 8 feet in height for monument signs, and 150 square feet and 20 feet in height for pylon signs,** as measured from finished grade.”



REFERENCE IMAGERY



T1: TENANT AWNING/MARQUEE IDENTITY

Sec. 19.75.070 Defintion: AWNING SIGN

“Any sign copy or logo mounted or attached to, or painted, on an awning.”

CODE CONSIDERATIONS (SEC. 19.75.100)

Sec. 19.75.100 (a3b):

“Signs shall only be allowed on the front face or valance of the awning, but are allowed on all faces of marquees.”

Sec. 19.75.100 (a3c):

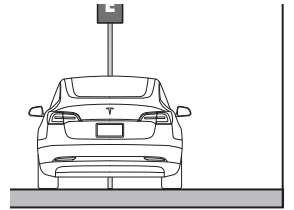
“Lettering shall not exceed a height of 8 inches and symbols/logos shall not exceed a height of 12 inches.”

Sec. 19.75.100 (a3e):

“Awning and marquee signs shall be illuminated by external lighting only.”

Sec. 19.75.100 (a3f):

“Signage area on an awning or marquee will be included in the total allowable sign area of the building frontage on which it is located.”



EV PARKING



REFERENCE IMAGERY



T2: TENANT FREESTANDING MONUMENT/PYLON IDENTITY

Sec. 19.75.070 Definitions: MONUMENT, PYLON

“Monument sign means a low profile, freestanding sign supported by a solid base as opposed to poles or open braces.”

“Pylon sign means a high profile sign over 6 feet in height having a solid support permanently attached directly into or upon the ground.”

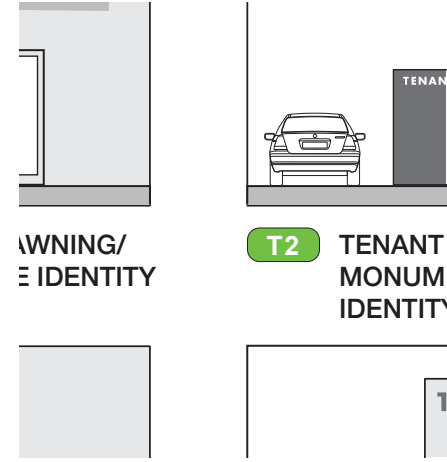
CODE CONSIDERATIONS (SEC. 19.75.100)

Sec. 19.75.100 (b):

“Monument or other types of freestanding signs such as pylon signs are permitted in addition to wall signs for **occupants of individual pad tenant and multi-tenant buildings**, based on site development and size criteria. Signs shall be located to be directed toward a parking lot, major pedestrian access, street, driveway or alley. . . **placed as near to the middle of the street frontage as practical or at a major driveway entrance to the complex from the street frontage**. Signs shall be separated by 300 feet on public streets.”

Sec. 19.75.100 (b1c1):

- 1–10,000 sf: max **20 sf / 4' ht**
- 10,000–30,000 sf: max **32 sf / 5' ht**
- 30,000+ sf: max **50 sf / 5' ht**



REFERENCE IMAGERY



T3: TENANT UNDER-CANOPY IDENTITY

Sec. 19.75.070 Defintion: UNDER-CANOPY sign not defined.

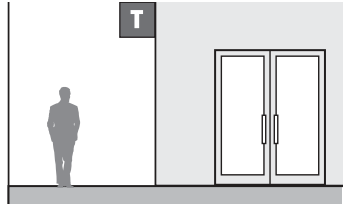
Sec. 19.75.070 Defintion: PROJECTING SIGN

“Sign, projecting, means a sign which protrudes from a building or structure.”

CODE CONSIDERATIONS (SEC. 19.75.100)

Sec. 19.75.100 (a4):

“For each use of occupancy, one under-canopy sign, and no sign exceeding 9 square feet shall be permitted.”



T3 TENANT PROJECTING IDENTITY

REFERENCE IMAGERY



T4: TENANT WALL IDENTITY

Sec. 19.75.070 Defintion: WALL SIGN

“Wall sign means a sign attached or erected upon a wall of a building or structure.”

T5: TENANT WALL IDENTITY: HIGH

Sec. 19.75.070 Defintion: WALL SIGN

“Wall sign means a sign attached or erected upon a wall of a building or structure.”

CODE CONSIDERATIONS (SEC. 19.75.100)

Sec. 19.75.100 (a1):

“. . . maximum of one square foot of sign per lineal foot of frontage of lease space . . . not to exceed 200 square feet of total sign area.”

Sec. 19.75.100 (a1b):

“The width of each wall sign shall not exceed 80 percent of the linear width of the building elevation.”

Sec. 19.75.100 (a1d):

“. . . in lieu of signs on the second and third floors, one sign may be placed as near the top of the building . . . on each frontage . . .”



ACCESSIBLE VAN PARKING



WALL



T5 TEN IDEI

SIGNS [E]



REFERENCE IMAGERY



T6: TENANT WINDOW SIGN

Sec. 19.75.070 Defintion: WINDOW SIGN

“Window sign means a sign painted or otherwise attached to a window for the purpose of outside display.”

CODE CONSIDERATIONS

Sec. 19.75.070:

“Window area shall be computed by calculating each window pane or panel. The area shall be separate for each window. A group of window panes or panels may be considered one window if they are adjoining and less than six inches apart.”

Sec. 19.75.100 (a2):

“For each use or occupancy, one window sign for each public entrance, and no sign exceeding 9 square feet shall be permitted.”

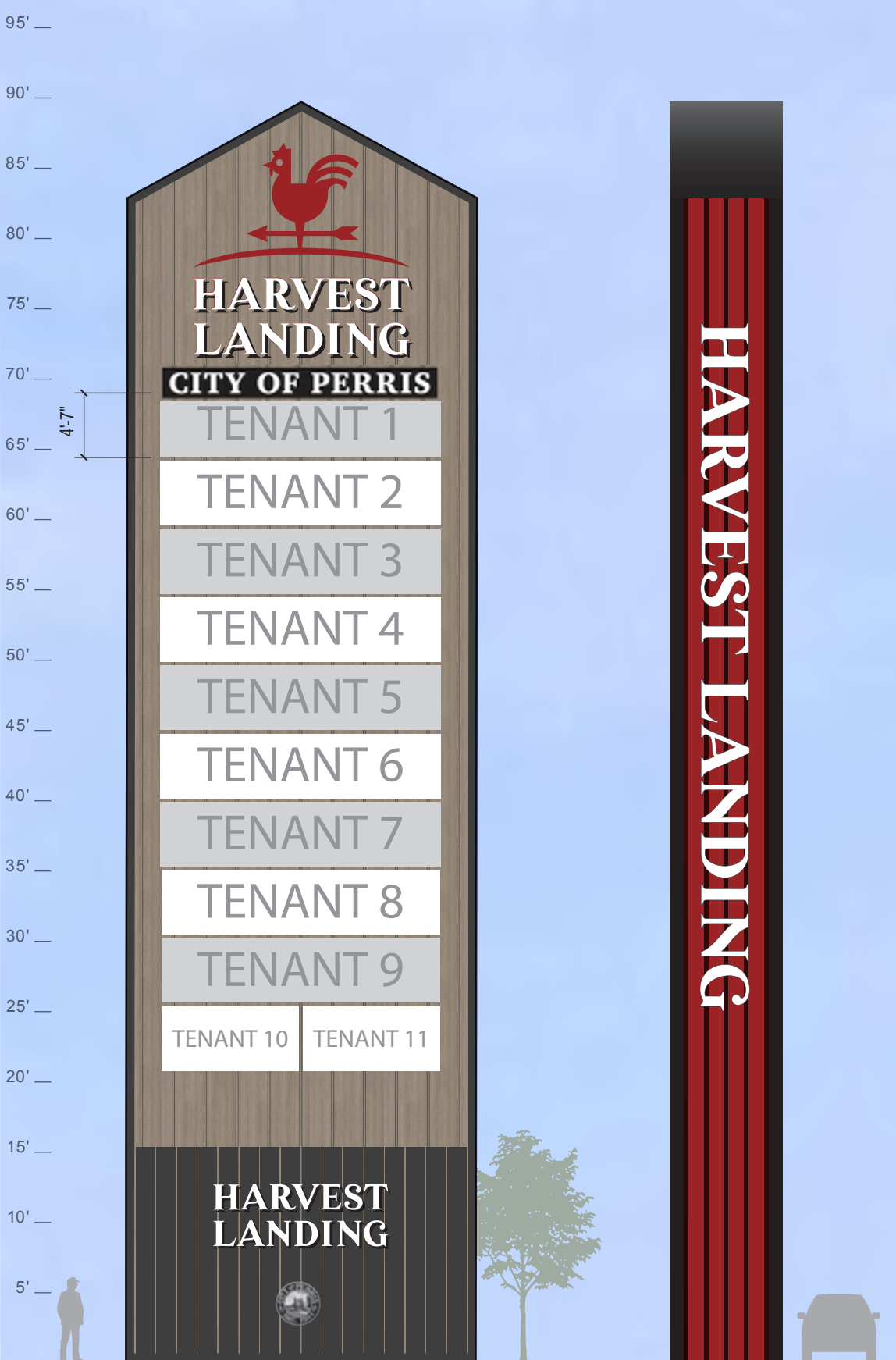


T6 TENANT WINDOW IDENTITY

REFERENCE IMAGERY



**Selbert
Perkins
Design.**



FRONT

SIDE

S1 FREEWAY MONUMENT IDENTITY

SCALE: 3/32" = 1'-0"



S2 SITE MONUMENT IDENTITY (OPTION A)

S3 MULTI-TENANT MONUMENT IDENTITY

SCALE: 3/16" = 1'-0"



D1 VEHICLE DIRECTION

D3 PEDESTRIAN DIRECTION

T3 TENANT BLADE IDENTITY

SCALE: 3/16" = 1'-0"



References

