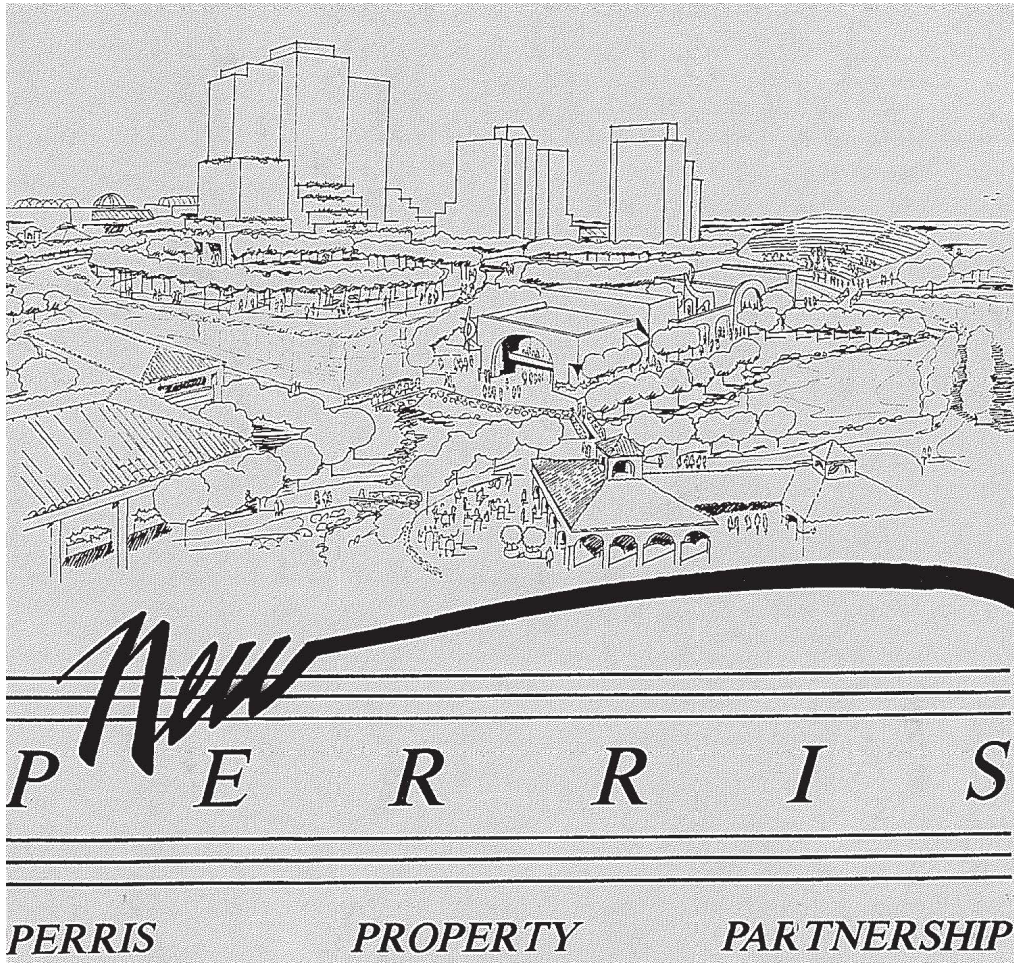

***SPECIFIC PLAN
AMENDMENT NO. 1***



Adopted: July 13, 2010

NEW PERRIS SPECIFIC PLAN AMENDMENT NO. 1 July 13, 2010

**Lead Agency
City of Perris
101 North D Street
Perris CA 92570
(951) 943-5003
Contact: Kenneth Phung, Planning Manager**

**Project Applicant:
IDI Logistics
840 Apollo Street, Suite 343
El Segundo CA 90245
(949) 351-7243
Contact: Steve Hollis**

**Prepared by:
Albert A. WEBB Associates
3788 McCray Street
Riverside CA 92506
(951)686-1070**

NEW PERRIS SPECIFIC PLAN

TABLE OF CONTENTS

Section		Page
i.	Table of Contents	1
ii.	List of Figures	6
iii.	List of Tables	7
1.0	Introduction.....	1-1
1.1	Purpose and Intent.....	1-1
1.2	Authority.....	1-2
1.3	Relationship to the City of Perris General Plan	1-2
1.4	Specific Plan Organization	1-2
1.5	Concurrent and Future Development.....	1-2
1.6	Project Summary.....	1-3
2.0	SUMMARY	2-1
2.1	Specific Plan Summary	2-1
	2.1.1 Project Location and Setting	2-1
	2.1.2 Project Characteristics	2-1
	2.1.3 Community Objectives	2-1
	2.1.4 Project Objectives.....	2-2
3.0	SPECIFIC PLAN.....	3-1
3.1	Comprehensive Land Use Plan.....	3-1
	3.1.1 Community Concept	3-1
	3.1.2 Plan Description.....	3-1
	3.1.3 Development Standards	3-4
3.2	Circulation Plan	3-7
	3.2.1 Approach	3-7
	3.2.2 Plan Description.....	3-7
	3.2.3 Development Standards	3-12
3.3	Landscaping Plan.....	3-16
	3.3.1 Approach	3-16
	3.3.2 Plan Description.....	3-16
	3.3.3 Development Standards	3-18

NEW PERRIS SPECIFIC PLAN

TABLE OF CONTENTS

Section		Page
3.4	Grading Plan	3-27
	3.4.1 Approach	3-27
	3.4.2 Plan Description.....	3-27
	3.4.3 Development Standards	3-27
3.5	Drainage Plan	3-29
	3.5.1 Approach	3-29
	3.5.2 Plan Description.....	3-29
	3.5.3 Development Standards	3-30
3.6	Water and Sewer Plan	3-33
	3.6.1 Approach	3-33
	3.6.2 Plan Description.....	3-33
	3.6.3 Development Standards	3-38
3.7	Project Phasing.....	3-40
	3.7.1 Approach	3-40
	3.7.2 Plan Description.....	3-40
	3.7.3 Development Standards	3-40
4.0	PLANNING AREA DEVELOPMENT STANDARDS	4-1
4.1	Planning Area 9 – Retail Commercial	4-1
	4.1.1 Land Use	4-1
	4.1.2 Zoning Ordinance Regulations	4-1
4.2	Planning Area 10 – Research and Development	4-6
	4.2.1 Land Use	4-6
	4.2.2 Zoning Ordinance Regulations	4-6
4.3	Planning Area 11 – Office/Commercial	4-10
	4.3.1 Land Use	4-10
	4.3.2 Zoning Ordinance Regulations	4-10
4.4	Planning Area 12 – Single-Family Detached Residential.....	4-16
	4.4.1 Land Use	4-16
	4.4.2 Zoning Ordinance Regulation	4-16
4.5	Planning Area 13 – Golf Course.....	4-20
	4.5.1 Land Use	4-20
	4.5.2 Zoning Ordinance Regulations	4-20

NEW PERRIS SPECIFIC PLAN

TABLE OF CONTENTS

Section	Page
4.6	Planning Area 14 – Office/Commercial 4-23
4.6.1	Land Use 4-23
4.6.2	Zoning Ordinance Regulations 4-23
4.7	Planning Area 15 – Hotel 4-28
4.7.1	Land Use 4-28
4.7.2	Zoning Ordinance Regulations 4-28
4.8	Planning Area 16 – Retail Commercial 4-32
4.8.1	Land Use 4-32
4.8.2	Zoning Ordinance Regulations 4-32
4.9	Planning Area 17 – Single-Family Detached Residential 4-37
4.9.1	Land Use 4-37
4.9.2	Zoning Ordinance Regulations 4-37
4.10	Planning Area 18 – Golf Course..... 4-41
4.10.1	Land Use 4-41
4.10.2	Zoning Ordinance Regulations 4-41
4.11	Planning Area 19 – Single-Family Detached Residential (10,000 SF)..... 4-44
4.11.1	Land Use 4-44
4.11.2	Zoning Ordinance Regulations 4-44
5.0	GENERAL DEVELOPMENT STANDARDS5-1
5.1	Site Planning and Architectural Guidelines 5-1
5.1.1	Architecture Guidelines..... 5-1
5.1.2	Office 5-1
5.1.3	Commercial..... 5-2
5.1.4	Single Family Detached 5-4
5.2	Parking Regulation and Standards..... 5-5
5.2.1	Intent and Purpose..... 5-5
5.2.2	Applicability of Regulations 5-5
5.2.3	Vehicle Off-Street Parking Requirements 5-5
5.2.4	Motorcycle Parking Requirements 5-6
5.2.5	Handicapped Parking Requirements 5-7
5.2.6	Bicycle Parking Requirements 5-7
5.2.7	Uses Not Listed..... 5-8

NEW PERRIS SPECIFIC PLAN

TABLE OF CONTENTS

Section	Page
5.2.8	Dimensions of Parking Spaces 5-8
5.2.9	Design Parking Standards for Parking Areas..... 5-8
5.2.10	Parking Area Landscaping..... 5-9
5.2.11	Off-Street Loading and Unloading Areas 5-9
5.2.12	Drive-Thru Facilities 5-10
5.2.13	Off-Site Parking Facilities..... 5-11
5.2.14	Shared Parking 5-11
5.2.15	Parking Structures 5-11
5.3	Transportation Management Standards..... 5-13
5.3.1	Intent and Purpose..... 5-13
5.3.2	Objectives 5-13
5.3.3	Parking..... 5-13
5.3.4	Transportation Management Programs 5-14
5.3.5	Pedestrian Circulation..... 5-15
5.4	Signage..... 5-16
5.4.1	Intent..... 5-16
5.4.2	Sign Standards 5-16
5.4.3	Sign Materials 5-17
5.4.4	Prohibited Signs..... 5-17
5.4.5	Temporary Signage 5-18
5.5	Outdoor Storage Areas and Reuse Collection Areas..... 5-19
5.6	Mechanical Equipment Screening..... 5-21
5.7	Energy Conservation Guidelines..... 5-22
5.7.1	Water 5-22
5.7.2	Energy 5-22
5.8	Public Arts Program 5-23
5.8.1	Intent..... 5-23
5.8.2	The Program..... 5-23
5.8.3	Guidelines..... 5-23
6.0	SPECIFIC PLAN IMPLEMENTATION 6-1
6.1	Intent 6-1
6.2	Site Plan and Building Design Review Procedures 6-1
6.3	Subdivision Procedures 6-2
6.4	Specific Plan Amendment Procedures 6-2

LIST OF FIGURES

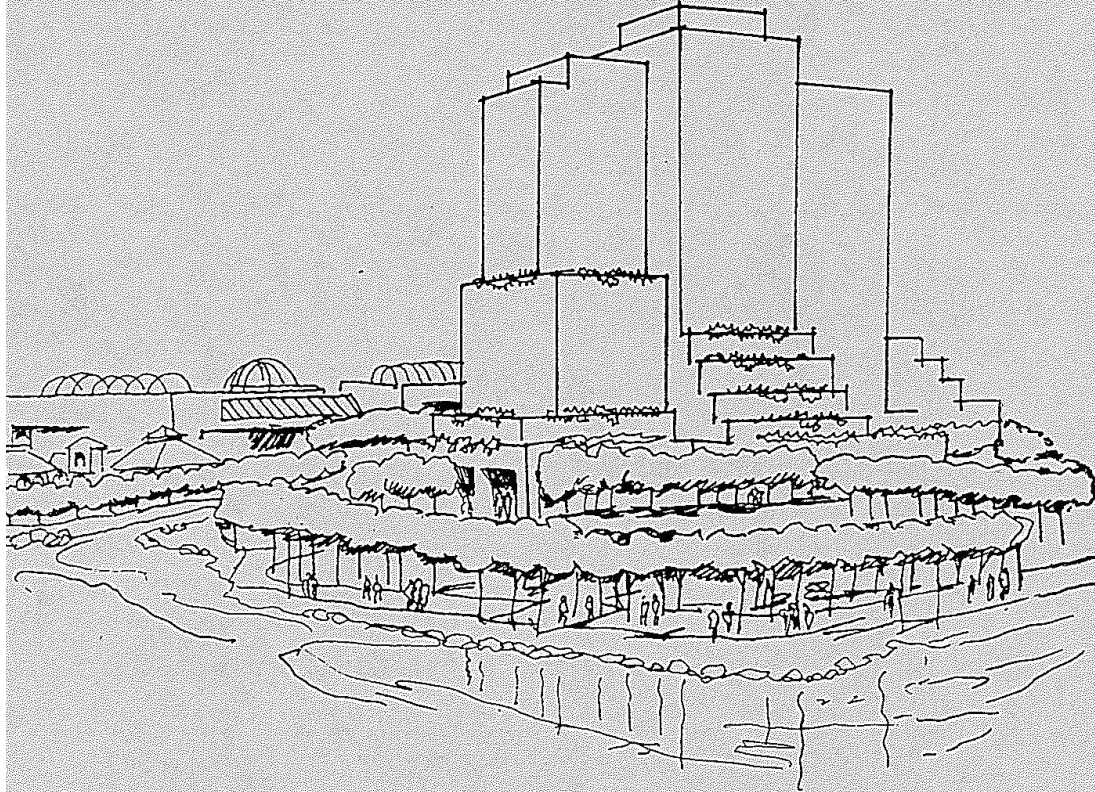
Number Figure	Page
Figure 1, Regional Map.....	2-3
Figure 2, Site Location Map	2-4
Figure 3, Aerial Photo.....	2-5
Figure 4, Vicinity Map.....	2-6
Figure 5, Land Use Plan.....	3-2
Figure 6, Illustrative Plan.....	3-5
Figure 7, Bird’s Eye View	3-6
Figure 8, Circulation Plan	3-9
Figure 9, Circulation Plan (Pedestrian/Equestrian Trails).....	3-10
Figure 10, Circulation Plan (Bicycle Trails).....	3-11
Figure 11, Typical Street Section – Collector (Ellis Avenue)	3-13
Figure 12, Typical Street Section – Collector (Murrieta Road)	3-14
Figure 13, Typical Street Section – Local	3-15
Figure 14, Overall Landscape Plan	3-17
Figure 15, Village and Primary Entries	3-19
Figure 16, Secondary Entries	3-20
Figure 17, Entry Elevations	3-21
Figure 18, Grading Plan	3-28
Figure 19, Drainage Plan	3-31
Figure 20, Channel Cross-Sections	3-32
Figure 21, Water Plan	3-35
Figure 22, Sewer Plan.....	3-37
Figure 23, Reclaimed Water Plan	3-39
Figure 24, Phasing Plan	3-41
Figure 25, Planning Area 9.....	4-4
Figure 26, Planning Area 9 Detail	4-5
Figure 27, Planning Area 10.....	4-8
Figure 28, Planning Area 10 Detail	4-9
Figure 29, Planning Area 11.....	4-14
Figure 30, Planning Area 11 Detail	4-15
Figure 31, Planning Area 12.....	4-18
Figure 32, Planning Area 12 Detail	4-19
Figure 33, Planning Area 13.....	4-21
Figure 34, Planning Area 13 Detail	4-22
Figure 35, Planning Area 14.....	4-26
Figure 36, Planning Area 14 Detail	4-27
Figure 37, Planning Area 15.....	4-30
Figure 38, Planning Area 15 Detail	4-31
Figure 39, Planning Area 16.....	4-35
Figure 40, Planning Area 16 Detail	4-36
Figure 41, Planning Area 17.....	4-39
Figure 42, Planning Area 17 Detail	4-40
Figure 43, Planning Area 18.....	4-42

LIST OF FIGURES

Number Figure	Page
Figure 44, Planning Area 18 Detail	4-43
Figure 45, Planning Area 19.....	4-46
Figure 46, Planning Area 19 Detail	4-47
Figure 47, Art in Public Places	5-24

LIST OF TABLES

Number Table	Page
Table 3-1, Statistical Summary	3-3
Table 3-2, Land Use Summary	3-4
Table 3-3, Plant Palette.....	3-22
Table 3-4, Water Demand (Gallons).....	3-34
Table 3-5, Wastewater Generation (Gallons)	3-36
Table 3-6, Irrigation Demand (Gallons)	3-38
Table 5-1, Vehicle Off-Street Parking Requirements.....	5-5
Table 5-2, Handicapped Parking Spaces	5-7
Table 5-3, Bicycle Parking Requirements.....	5-8
Table 5-4, Loading Space Requirements	5-10
Table 5-5, Stacking Requirements	5-11



1.0 INTRODUCTION

1.0 INTRODUCTION

1.1 Purpose and Intent

This Specific Plan provides the City of Perris with a comprehensive set of plans, regulations, conditions and programs for guiding the orderly development of New Perris, a proposed 596-acre project incorporating multiple land uses and implements each applicable element of the City of Perris General Plan. Approval of this Specific Plan is the first-step to allow the development of the New Perris project. The Specific Plan was approved by a unanimous vote of the City Council on October 30, 1989 (case GPA 88-57, Resolution #1740).

New Perris Specific Plan represents consistency of development with the General Plan. The Specific Plan assures the ordinary development with the General Plan. The Specific Plan assures the orderly development of land uses within the City and within the City's ability to supply essential urban services. The Specific Plan further assures coordinated development of the project area as a comprehensive project comprised of land use designations including commercial, office, multi-family, single-family and recreational/open space designations. A General Plan Amendment to change the previous land use designations from agriculture to "Specific Plan" and to incorporate both the proposed Ellis/Evans Avenue interchange at I-215 and the Evans Avenue Corridor into the circulation element was approved by the City Council on October 30, 1989 (GPA 89-33, Resolution #1739).

The New Perris Specific Plan has been prepared in accordance with the State specific plan requirements as delineated within the California Planning Zoning and Development Laws 1988 (Government code sections 65450 through 65457) and addresses all issues and topics specified within those provisions.

A key function of the Specific Plan is to reduce the need for further detailed planning and environmental review procedures as the project is developed. In accordance with Section 65453 of the California Planning, Zoning and Development Laws 1988, the accompanying Environmental Impact Report (EIR) was prepared under the City's authority and is in compliance with the California Environmental Quality Act and City of Perris local guidelines. The EIR was certified as adequate by the City of Perris on October 30, 1989 (EIR No. 88-57, Resolution #1742). The EIR is intended to serve as the sole environmental document for the Specific Plan and all individual projects which are undertaken pursuant to, and in conformance with, the New Perris Specific Plan. The EIR is an informational document designed to provide the general public and appropriate governmental agency decision-makers with a full understanding of the potential environmental effects of the proposed.

The EIR process is intended to enable public agencies to: 1) evaluate a project for determination of the significance of its effect on the environment; 2) examine and institute methods of reducing adverse impacts; and 3) consider alternatives to the project as proposed. Pursuant to Condition of Approval #8, an environmental assessment shall be conducted with each filing for tentative map, plot plan, specific plan amendment, or any other discretionary permit required to implement the Specific Plan.

1.2 Authority

California Government Code Section 65450 gives a legislative body authority to prepare specific plans for the systematic implementation of a general plan for all or part of the area covered by the general plan. As outlined in Government Code Sections 65451-65452, specific plans are to contain a text and diagrams specifying: (1) the distribution, location, and extent of land uses; (2) the distribution, location, and extent of major infrastructure improvements needed to support the land uses described in the plan; (3) development standards and criteria; (4) a program of implementation measures; (5) a state of the relationship of the specific plan to the general plan; and (6) any other subjects which are necessary or desirable for implementation of the general plan.

After it is adopted, a Specific Plan has an effect similar to the local general plan. For example, the State's Subdivision Map Act requires the legislative body to deny approval of a final or tentative subdivision map if it is not in substantial conformance with applicable specific plans. In addition, a development agreement between a municipal body and a developer/applicant cannot be approved unless the legislative body finds that the agreement is consistent with the general plan and any applicable specific plan.

1.3 Relationship to the City of Perris General Plan

The City of Perris General Plan establishes broad goals which are designed to improve the quality of life by preserving, maintaining, or enhancing the human, built, or natural environments. Taken as a whole, the General Plan represents a long-range planning document which determines where the City presently is in terms of development, where the City is going, and what guidelines and policies are being used to direct the future character of the City.

The New Perris Specific Plan carries out the goals and objectives of the City of Perris General Plan in an orderly and aesthetically pleasing manner. Special consideration was given to ensuring that the specific plan is consistent with all elements of the City of Perris General Plan. The project applicant has worked with the City of Perris to ensure that the New Perris Specific Plan conforms to all policies and goals of the City's General Plan.

1.4 Specific Plan Organization

In providing for the systematic implementation of the General Plan, this Specific Plan is organized into six primary sections, all of which relate to and support the other sections include:

- Introduction
- Summary
- Specific Plan Description
- Planning Area Development Standards
- General Development Standards
- Specific Plan Implementation

1.5 Concurrent and Future Development

The New Perris Specific Plan establishes a comprehensive picture of the proposed development, highlighting important issues, and includes all the information necessary to obtain a clear understanding of the project. As stated

above, it is the intent of the New Perris Specific Plan to address project-wide issues relating to the general character of land use, general circulation systems, overall landscaping themes, general infrastructure systems and zoning. The Specific Plan also serves to set parameters and general policy direction for subsequent development applications.

The next level of project review for the New Perris project will be the subdivision maps. Along with the Specific Plan, the City has agreed to the concurrent development processing of the New Perris Master Tentative Tract Map. The master tract map will subdivide the 596-acre site into 19 numbered lots for development and various lettered lots for landscape purposes. The master tentative tract map will also provide for future infrastructure improvements, such as the I-215 interchange, roadways, overcrossings, flood control improvements, all of which are needed for the development of the New Perris Project.

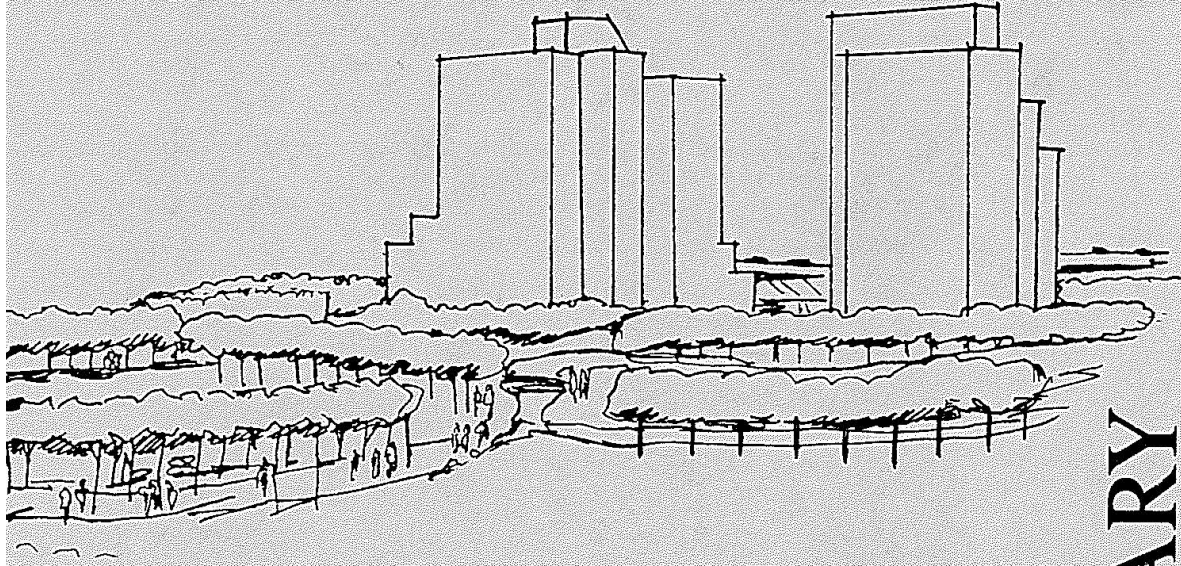
For the New Perris project, the project applicant also proposes the formation of a Mello-Roos Community Facilities District and will be entering into a Development Agreement with the City of Perris to ensure that vesting rights are maintained through project build-out.

Subsequent City of Perris review of future proposals for individual site plans and buildings within the New Perris development will be based on the consistency of each proposal with the development standards provided in this Specific Plan. This may be achieved through the City of Perris Development Plan Review Process (also refer to Section 6.0, Implementation).

1.6

Project Summary

Amendment No. 1 to the New Perris Specific Plan removes Planning Areas 1 through 8; all located west of Interstate-215 (I-215).



2.0 SUMMARY

2.0 SUMMARY

2.1 Specific Plan Summary

2.1.1 Project Location and Setting

The 389-acre New Perris site is located adjacent to I-215 which bisects the site from the north to the south. The project site is located immediately south of the Fourth Street offramp and in the City of Perris and is bounded by San Jacinto Avenue to the north, Ellis Avenue to the south, and I-215 to the west. The Perris Valley Storm Drain and the San Jacinto River transect the eastern portion of the project site and meet at this location. There are no significant topographic features on the site. The project site had previously been used extensively for agricultural production and, as a result, there is little natural vegetation remaining onsite. The project site is depicted in **Figure 1, Regional Map, Figure 2, Site Location Map, Figure 3, Aerial Photograph** and **Figure 4, Vicinity Map**.

2.1.2 Project Characteristics

The New Perris development incorporates a multi-use concept that reflects urban trends in the Perris Valley. The mixed-use development concept comprises residential, commercial, office and industrial uses, a town-center, open space recreational areas, a flood control channel and an extensive circulation network within a comprehensive land use plan. Specific land use designations include the proposed development of research and development centers, hotel, restaurants, and a golf course.

The land uses will be accessed by a curvilinear circulation system and will feature major water amenities, art in public places, extensive landscaping, and greenbelts and walkways to encourage pedestrian circulation within the project site. A master infrastructure program will include a coordinated program for engineering, financing, and the construction of water, sewer, storm drain, and roadway improvements.

2.1.3 Community Objectives

The goal of the New Perris project is the creation of a high-quality working and living environment, integrated with a balance of commercial and recreational uses and public facilities to serve as a “community within a community.” The land uses are complemented and balanced by a number of supporting services and cultural attractions designed to increase the commercial and cultural importance of the New Perris project within the City of Perris and the surrounding areas within the Perris Valley, Sitting adjacent to I-215, the New Perris project will serve as a landmark at one of the major entrances to the City. The location offers excellent visibility, convenience and vehicular accessibility.

The surrounding residential community will benefit from the centralized, consolidated location of retail, office, entertainment, cultural and recreational uses provided in close proximity to homes in the City of Perris.

2.1.4

Project Objectives

The New Perris project is uniquely located within the Perris community. More than any other existing or potential development area in the City, this site offers an opportunity to create a unique “gateway” to the city and a significant focal point of community life which will be a primary source of community identity and pride and will bring substantial revenue to the City. It is important that the area be developed for the long-term benefit of the community in a manner which is reasonable and equitable to the project proponents. Thus, the specific objectives are:

- Create a development concept which provides for a balanced and integrated mix of commercial, residential, entertainment, cultural, and recreational uses on-site.
- Coordinate the orderly development of land uses within the City and within the City’s ability to supply essential public support services.
- Establish . facilities for office-professional, governmental, institutional, retail, and recreational activities that should be selectively integrated with the surrounding land uses in order to form an active focal point of community life.
- Provide an area of outstanding environmental character, including a high quality of visual design, suppression of noise, protection of health and safety, and promotion of the people, city and community.
- Provide additional employment opportunities for the residents of the City of Perris and surrounding communities.
- Minimize conflicts with surrounding land uses through sensitive land use planning and site development standards.
- Minimize impacts on the natural and urban environments through the incorporation of appropriate mitigation measures.
- Realize additional tax revenues that can be used to improve the quality of public service for all the citizens of Perris.
- Provide vertical mixing of key project components around centrally located focal points.
- Promote the development of a safe, effective and efficient circulation and transportation network.
- Ensure adequate consideration for urban design in the development process so that the new development enhances the City of Perris as it matures.
- Develop in conformance with a coherent community plan that will provide overall unifying elements and provide an attractive environment to motorists from I-215. These include: vehicular and pedestrian circulation corridors; visual buffering between land uses; and project-wide design themes such as architecture, landscaping, color palette of building materials, paving, walls, fences, signage and entry treatments.

REGIONAL MAP

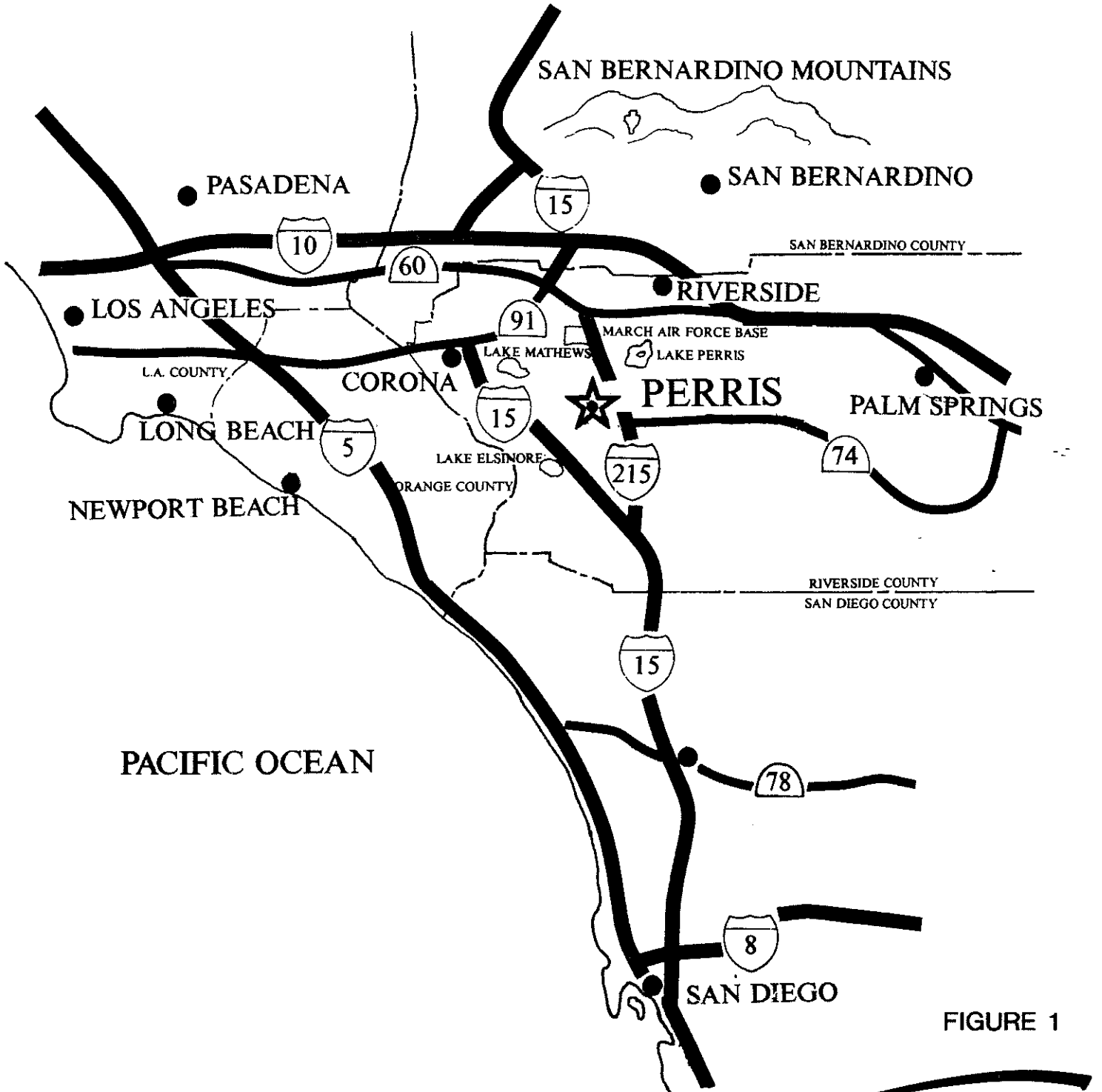


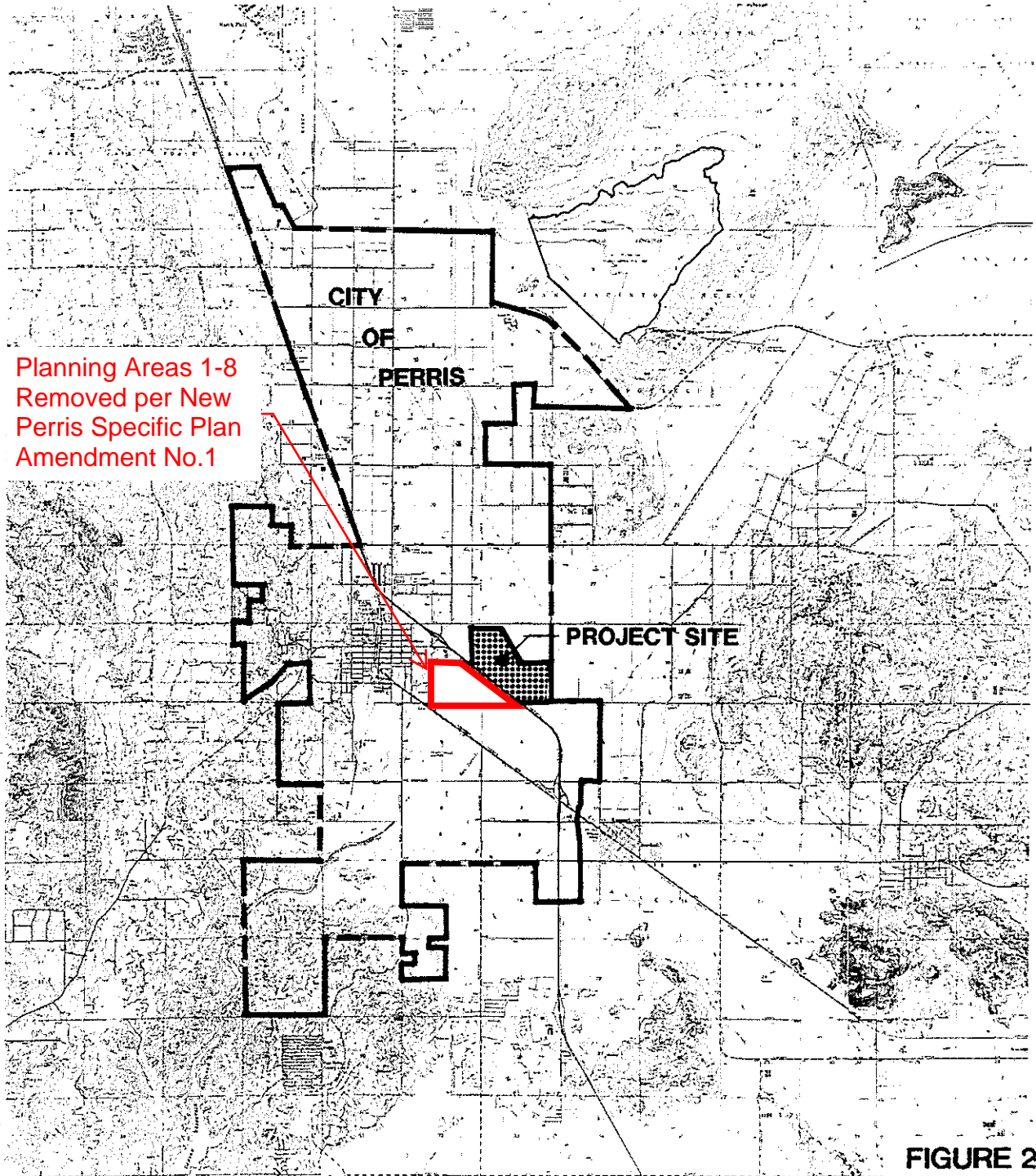
FIGURE 1



FM
FLORIAN MARTINEZ ASSOCIATES

New
P E R R I S
PERRIS PROPERTY PARTNERSHIP

SITE LOCATION MAP



AERIAL PHOTO



FIGURE 3



FMA
FLORIAN MARTINEZ ASSOCIATES

New
P E R R I S

PERRIS PROPERTY PARTNERSHIP

VICINITY MAP

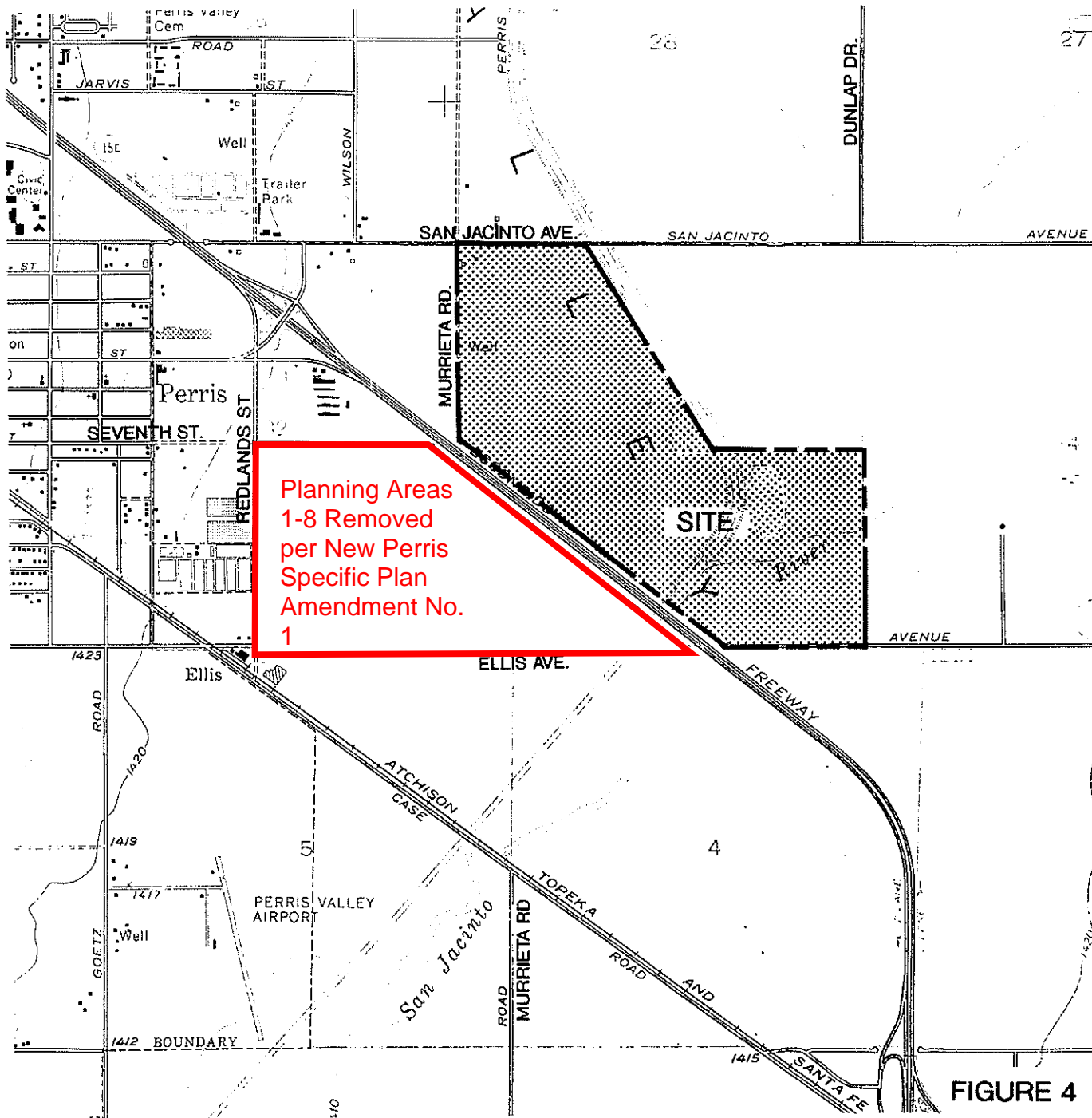
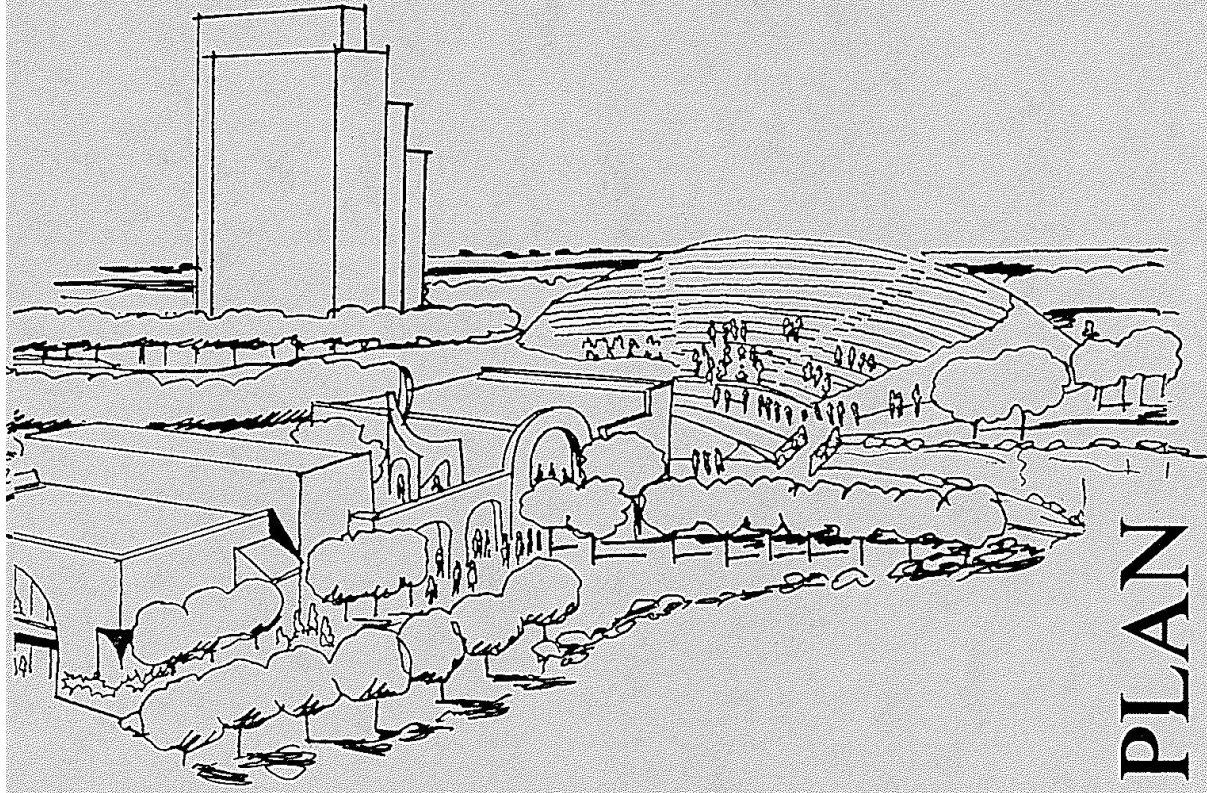


FIGURE 4



New
P E R R I S

PERRIS PROPERTY PARTNERSHIP



3.0 SPECIFIC PLAN

3.0 SPECIFIC PLAN

3.1 Comprehensive Land Use Plan

3.1.1 Community Concept

The underlying community concept for New Perris incorporates strong and cohesive design standards to create a high-quality, multi-use, master-planned community. Through strong, cohesive community planning and design, the New Perris Specific Plan will offer a diverse, convenient living and business environment for the citizens of Perris. The New Perris Specific Plan provides for overall unifying elements in each Planning Area. The project's vehicular and pedestrian circulation corridors, landscaping and entry elements, and project-wide design theme will provide the unifying characteristics. Additionally, the project will create a community identity through control of elements such as architecture, landscaping, color palette of building materials, paving, walls, fencing, signage and entry treatments.

The New Perris development establishes a strong, positive image for the southern gateway into the City of Perris. The New Perris Specific Plan implements a harmonious pattern of land uses, provides an aesthetic and functional open space system that responds to site conditions, and creates a community identity for the entire project.

3.1.2 Plan Description

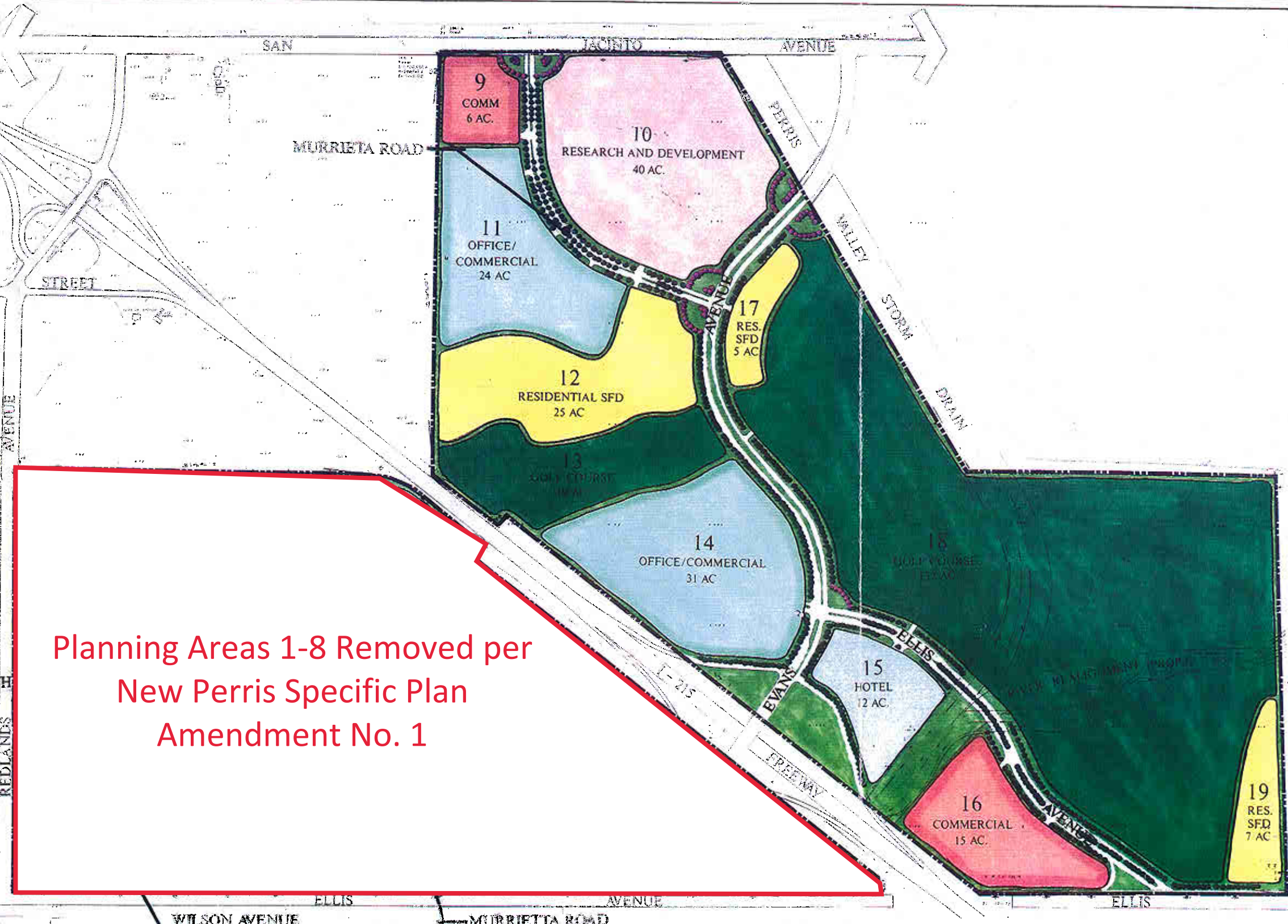
The comprehensive land use plan, reflected in **Figure 5, Land Use Plan**, defines the general location of each type of development, the type of development allowed on the site, and the conceptual site design by Planning Area. The maximum intensity allowed for the development of the site is intended to achieve a functional integration of the proposed uses. In addition, the location and intensity of development allowed within the different planning areas are intended to create an urban design concept which will provide a unique identity and prominence for the New Perris development through the creation of a harmonious mixture of low-, mid-, and high-rise building clusters on the project site.

In order to promote a functional integration of uses and to provide for a stimulating urban environment, the New Perris development has been divided into eleven (11) major Planning Areas as illustrated in Section 4.0. The size, in acres, for each of the Planning Areas, is summarized below in **Table 3-1, Statistical Summary**. The statistical summary by land use is provided in **Table 3-2, Land Use Summary**.

LAND USE PLAN

SUMMARY:

PLANNING AREA	LAND USE/ DESIGNATION	TOTAL ACRES
1	MULTI-FAMILY RESIDENTIAL	25
2	HOTEL	12
3	TOWN CENTER	11
4	OFFICE/COMMERCIAL	36
5	OFFICE	47
6	OFFICE/COMMERCIAL	20
7	REGIONAL COMMERCIAL	54
8	COMMERCIAL	9
9	COMMERCIAL	6
10	RESEARCH AND DEVELOPMENT	40
11	OFFICE/COMMERCIAL	24
12	RESIDENTIAL - SFD	25
13	GOLF COURSE	19
14	OFFICE/COMMERCIAL	31
15	HOTEL	12
16	COMMERCIAL	15
17	RESIDENTIAL - SFD	5
18	GOLF COURSE	15
19	RESIDENTIAL - SFD	7
TOTAL PROJECT ACRES		596



Planning Areas 1-8 Removed per
New Perris Specific Plan
Amendment No. 1

FIGURE 5



Table 3-1, Statistical Summary¹

Planning Area	Land Use	Acres	Dwelling Units	Total Building Square Footage²
9	Commercial	6		80,000
10	Research and Development	40		435,100
11	Office/Commercial	24		272,250
12	Residential (SFD)	25	79	
13	Golf Course	19		
14	Office/Commercial	31		340,000
15	Hotel	12		250,000
16	Commercial	15		150,000
17	Residential (SFD)	5	18	
18	Golf Course	152		50,000
19	Residential (SFD)	7	17	
	Roadways	53		
	TOTAL	389	114	1,577,350

Notes:

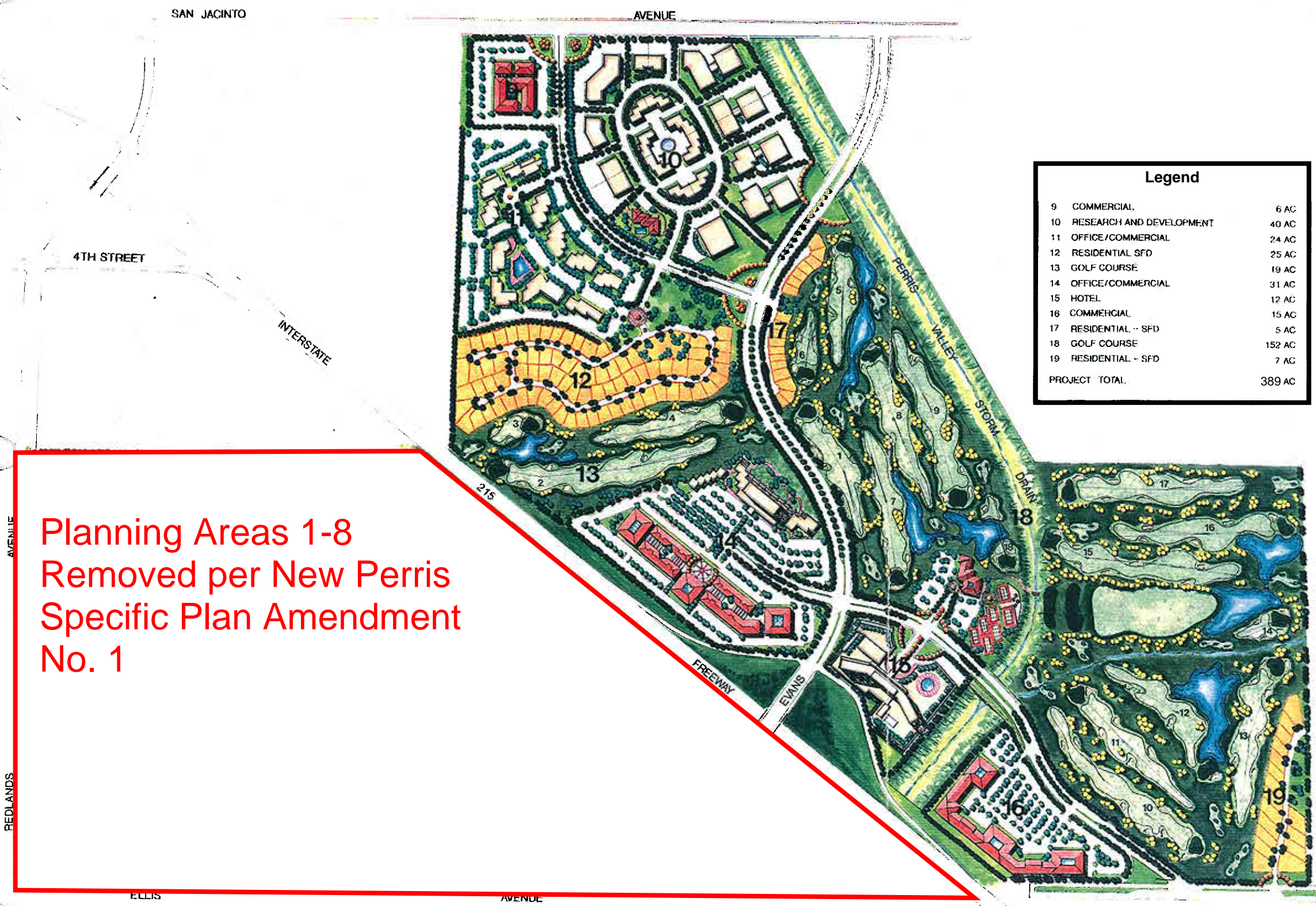
1. Planning Areas 1-8 have been removed through Specific Plan Amendment No. 1.
2. Does not include parking structures.

Table 3-2, Land Use Summary¹					
Land Use	Planning Areas	Acres	Dwelling Units	Density	Percent of Project
Residential (SFD)	12, 17, 19	37	114	2-5 DU/AC	9.5
Retail Commercial	9, 16	21			5.4
Office Commercial	11, 14	55			14.1
Hotel	15	12			3.1
Research and Development	10	40			10.3
Golf Course	13, 18	171			44.0
Roadways; Circulation		53			13.6
TOTAL		389	114		
Notes:					
1. Planning Areas 1-8 have been removed through Specific Plan Amendment No. 1.					

3.1.3 Development Standards

- A. The development of the property shall be in accordance with the mandatory requirements all City of Perris ordinances and state laws and shall conform substantially with the approved New Perris Specific Plan (88-57) as filed in the office of the City of Perris Department of Community Development, unless otherwise amended.
- B. Prior to the issuance of a building permit for construction of any use contemplated by this approval, the applicant shall first obtain clearance from the City of Perris Department of Community Development to ensure that all pertinent conditions of approval have been satisfied for the particular development phase of the Specific Plan.
- C. Development of the Specific Plan shall be in conformance with the Planning Area Development Standards contained within the adopted Specific Plan, or as amended by these conditions of approval.
- D. Lots created pursuant to this Specific plan shall be in conformance with the development standards as provided in the adopted specific plan and as amended by the conditions of approval.

ILLUSTRATIVE PLAN



Planning Areas 1-8
 Removed per New Perris
 Specific Plan Amendment
 No. 1

FIGURE 6



Perris
 P E R R I S

PERRIS PROPERTY PARTNERSHIP

BIRD'S EYE VIEW

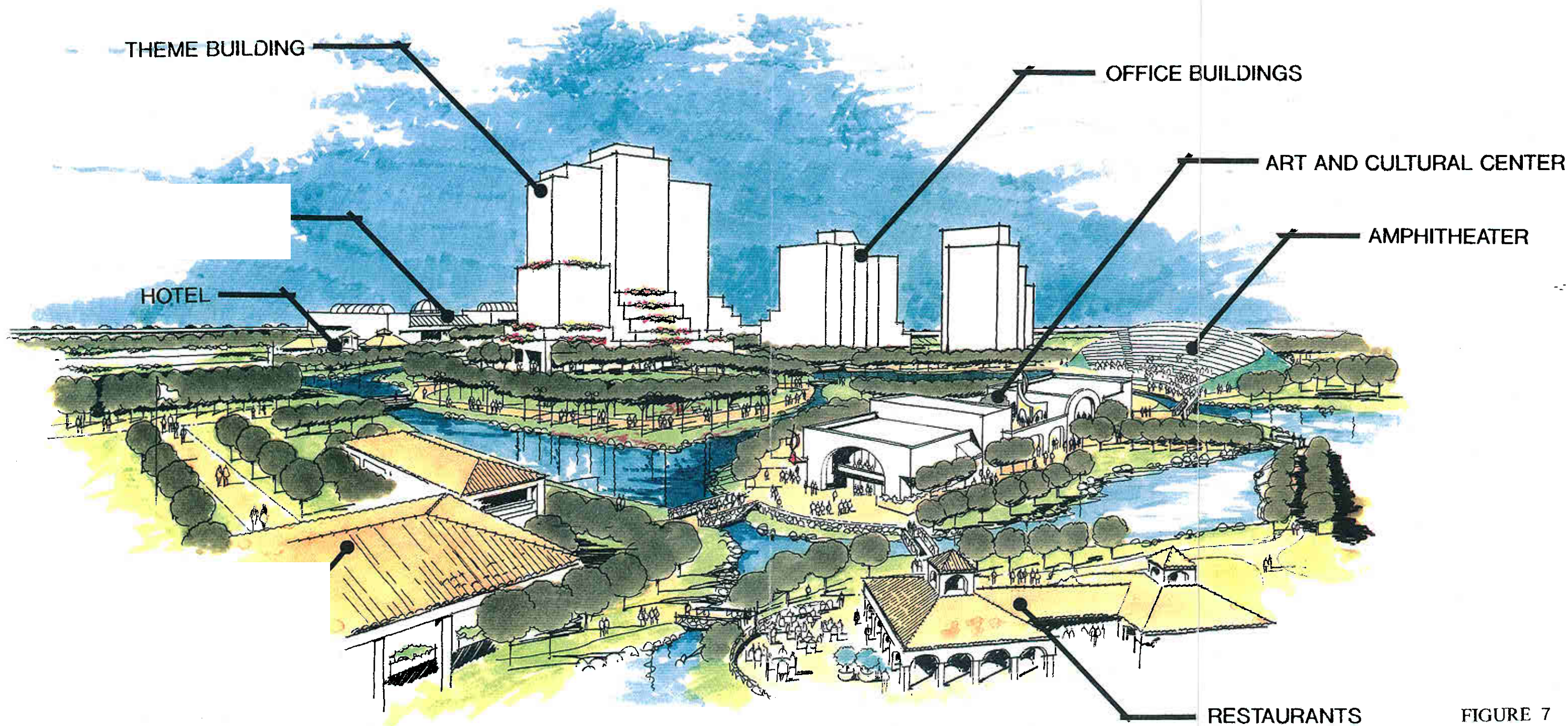


FIGURE 7

3.2 Circulation Plan

3.2.1 Approach

The Circulation Plan for New Perris is based on a design which will allow safe and efficient internal circulation and access throughout the project. The land uses will be accessed via a curvilinear circulation system accented by bike lanes, major water features, art in public places, extensive landscaping and greenbelts and walkways to encourage pedestrian circulation. The primary access points will provide safe ingress and egress while enhancing the aesthetics of the project. Evans/Ellis Avenue will be developed as a major arterial traveling in a north-south direction and will serve as the project's main source of access into the separate planning areas. A major project goal of designing a circulation system that allows for the consistent flow of automobile, bicycle and pedestrian traffic is achieved through the development of planning areas with an inward orientation.

The existing street serving the New Perris project site is as follows:

Ellis Avenue is a two-lane east/west street which does not presently connect with nor cross with I-215. The existing traffic volume on Ellis Avenue within the vicinity of this project is negligible since Ellis Avenue does not cross I-215 and since it provides access to vacant properties which have been previously used for agriculture.

Redlands Avenue is a two-lane north/south street which intersects 4th Street to the north and intersects Ellis Avenue to the south. The existing traffic volume on Redlands Avenue is also considered negligible.

San Jacinto Avenue is a two-lane east/west street that serves as a connector from the easterly foothills to I-215. San Jacinto Avenue also carries a very low traffic volume at the present time.

Interstate 215 is a four-lane divided freeway with an average daily traffic of 35,000 vpd (1987). There are two interchanges within the general area of 4th Street at I-215 and Case Road at I-215.

3.2.2 Plan Description

Figure 8, Circulation Plan, shows the basic circulation system and proposed Ellis/Evans Avenue interchange at I-215. All roads within the development will be public streets and will conform with City standards while providing positive aesthetic project statements. The system is designed to provide efficient movement of traffic with minimal disturbance to the internal and external developments.

A traffic study has been prepared for the New Perris project which addresses the major transportation network requirements, such as the number of lanes and signalized intersections, needed to ensure a smooth traffic flow in the immediate area for the projected build-out year 2010. The project applicant and traffic consultants have worked extensively with the City of Perris to incorporate the New Perris Specific Plan land uses into future traffic and population models and the Circulation Element of the General Plan.

The New Perris project also takes into consideration the proposed “Ellis/Evans Avenue Interchange” at I-215. As part of the Circulation Element update, a new major north-south arterial, Evans Avenue, is required between the Ramona Expressway and I-215. This same arterial is currently being planned to continue northerly from Ramona Expressway through the City of Moreno Valley to connect with State-Route 60 (SR-60) at Moreno Beach Boulevard as an intercity arterial serving the central and eastern portions of the Moreno-Perris Valley area.




Extending Evans Avenue southerly from San Jacinto Avenue to connect with I-215 (via the interchange) and southerly to Ellis Avenue will relieve the current and future traffic congestion at 4th Street, provide better distribution to traffic, improve access to the Perris central business district, and improve overall traffic circulation in the area. Ellis Avenue with the proposed interchange would further provide a parallel east/west traffic arterial to 4th Street.

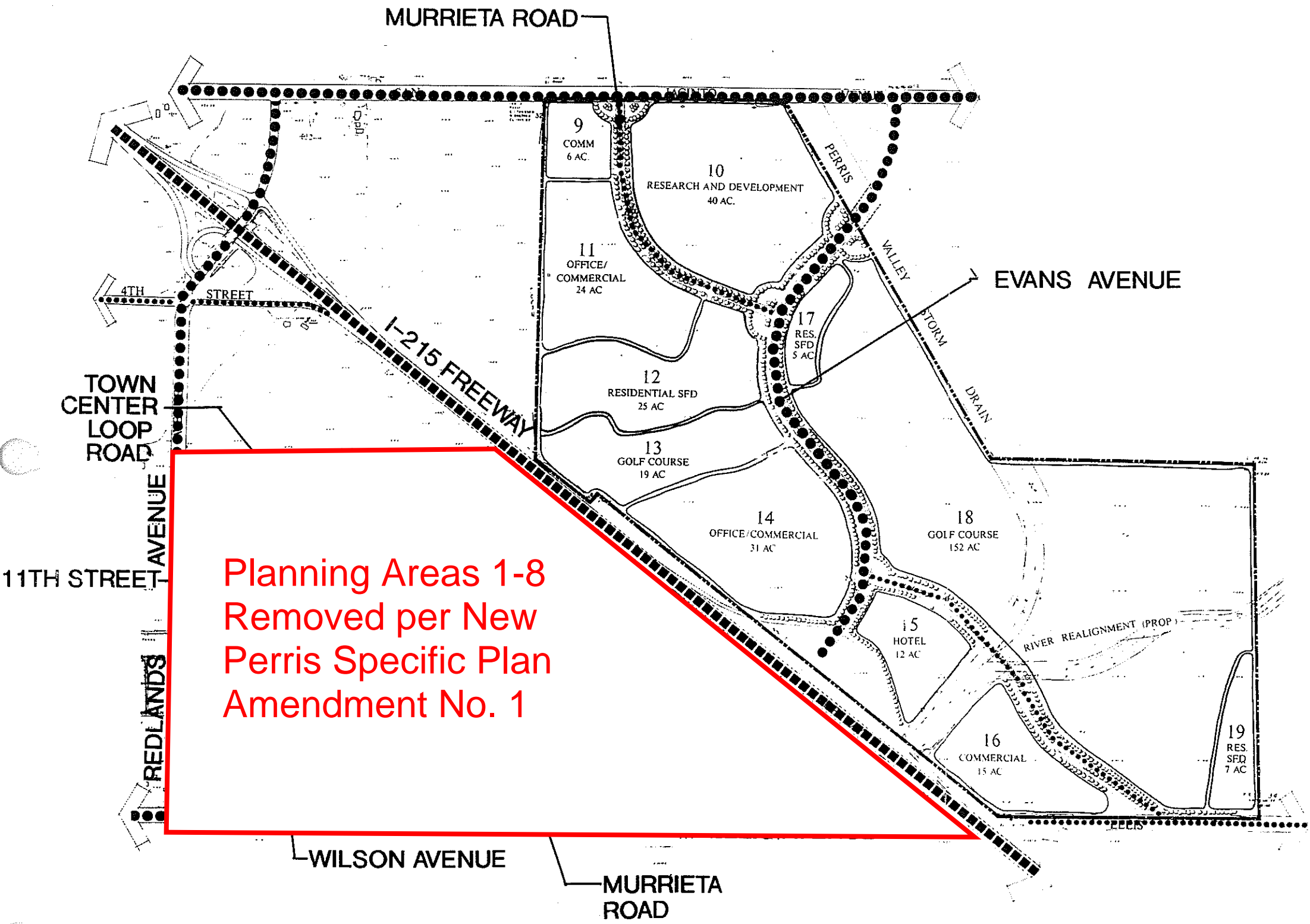
Vehicular access to the arterial roadways will be restricted to controlled points of ingress and egress. Conceptual access points to each individual planning area are illustrated within the individual planning area exhibits provided in Section 5.0 and within **Figure 8**. Final access point locations to each planning area will be identified at the time of tentative tract submittal.

Bicycle and pedestrian circulation will be an integral part of the New Perris overall circulation plan encouraging the use of walkways and bicycle paths as reflected in **Figure 9, Circulation Plan (Pedestrian/Equestrian Trails)** and **Figure 10, Circulation (Bicycle Trails)**. Bicycle and pedestrian paths also tie into regional systems via the San Jacinto River and Perris Valley Storm Drain Channel.

CIRCULATION PLAN

LEGEND

-  FREEWAY
-  ARTERIAL
-  COLLECTOR



Planning Areas 1-8
Removed per New
Perris Specific Plan
Amendment No. 1

FIGURE 8



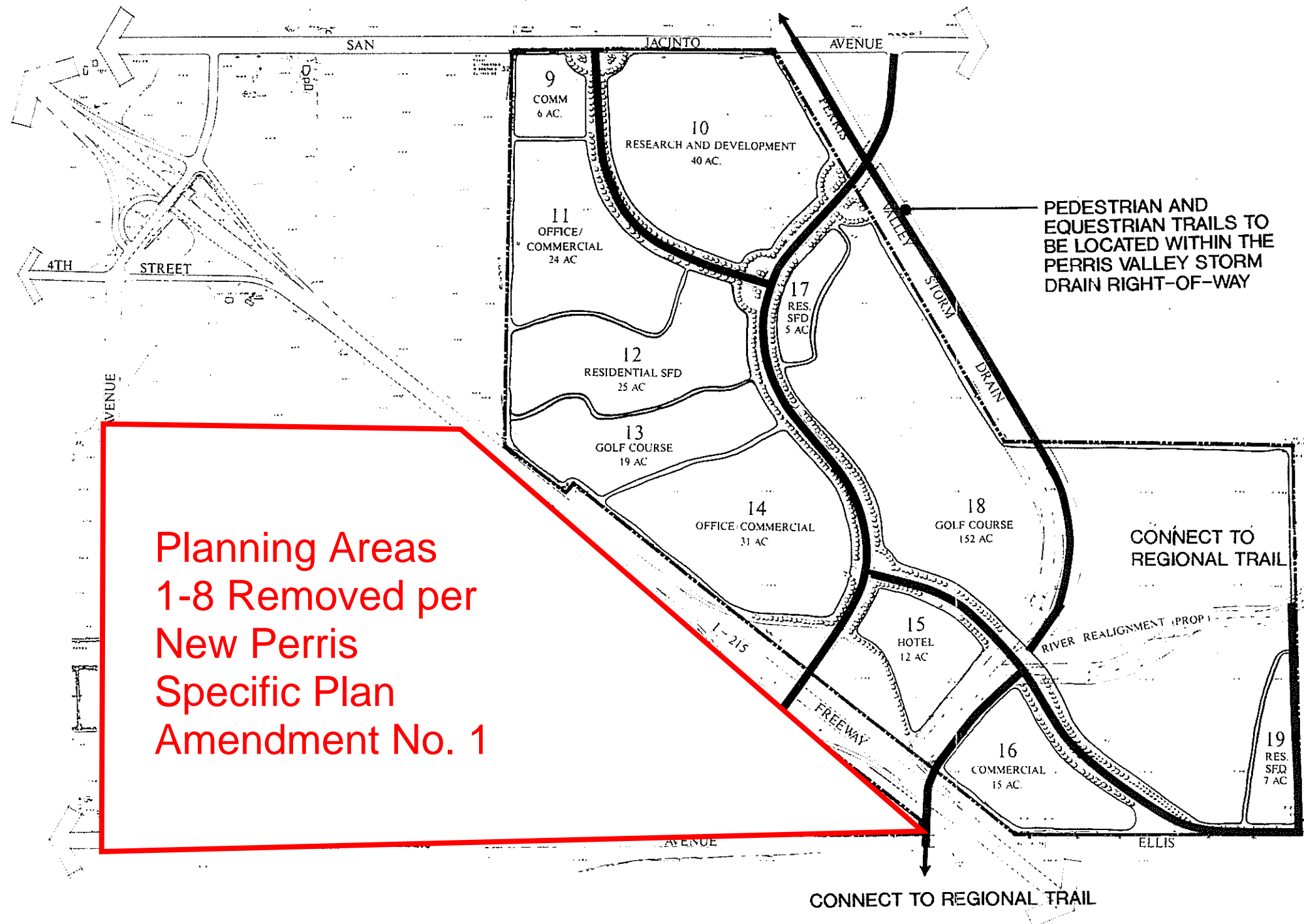
NEW
P E R R I S

PERRIS PROPERTY PARTNERSHIP

CIRCULATION PLAN

PEDESTRIAN TRAILS

EQUESTRIAN TRAILS



PEDESTRIAN AND EQUESTRIAN TRAILS TO BE LOCATED WITHIN THE PERRIS VALLEY STORM DRAIN RIGHT-OF-WAY

LEGEND

PEDESTRIAN TRAILS

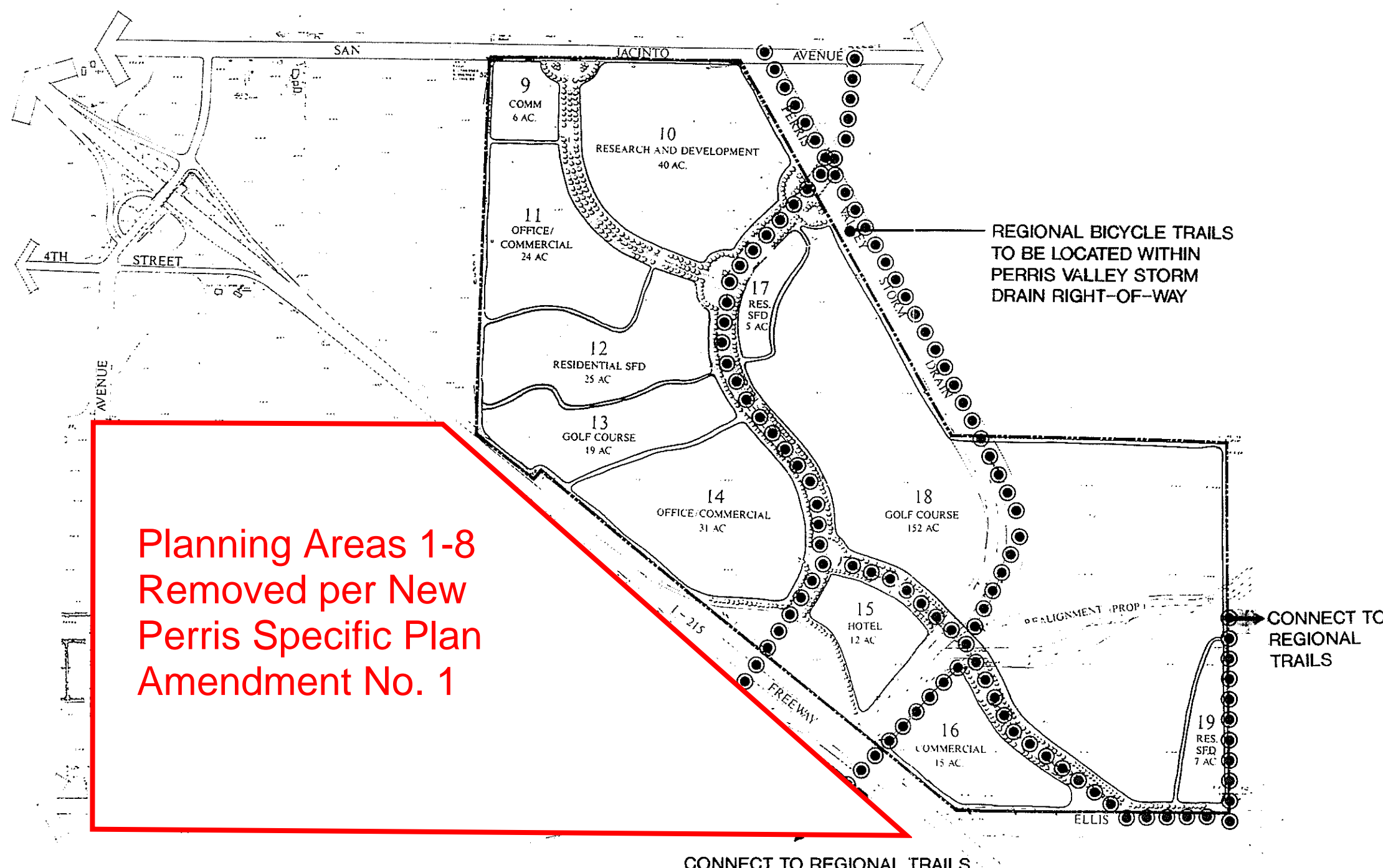
Planning Areas
1-8 Removed per
New Perris
Specific Plan
Amendment No. 1

NOTE:

INTERNAL PEDESTRIAN CIRCULATION WILL BE PROVIDED WITHIN EACH PLANNING AREA OR WILL BE PROVIDED ADJACENT TO THE STREETS AND WILL PROVIDE CONNECTIONS WITHIN THE DEVELOPMENT.

FIGURE 9

CIRCULATION PLAN BICYCLE TRAILS



LEGEND

BICYCLE TRAILS

FIGURE 10

3.2.3

Development Standards

There is a proposed freeway interchange and two public street classifications in the New Perris Specific Plan project.

I-215 Interchange

At I-215, a diamond interchange is proposed for Ellis/Evans Avenue utilizing an overcrossing structure approximately 5,400 feet southerly of 4th Street, and 1-3/4 miles northerly of Case Road. Ellis Avenue will be realigned to provide a perpendicular crossing of I-215. The ramps will be of conventional design with the off ramps widened at the signals to provide vehicle storage.

The overcrossing structure will provide for four through traffic lanes, two median turn lanes and four-foot shoulders. The structure span length and the ramp terminal design will provide for the addition of two future freeway lanes in each direction. A sidewalk will be provided on the northerly side of the structure. Auxiliary lanes will be provided on each side of I-215 between Evans Avenue and 4th Street to provide safe merge and weaving operations.

The ramps will be carried over the San Jacinto River on structures matching the existing I-215 bridges. The Riverside County Flood Control District's plan for improvement of the San Jacinto River is currently under design. The proposed interchange improvements will be coordinated with the Riverside County Flood Control District.

Collector Roadways

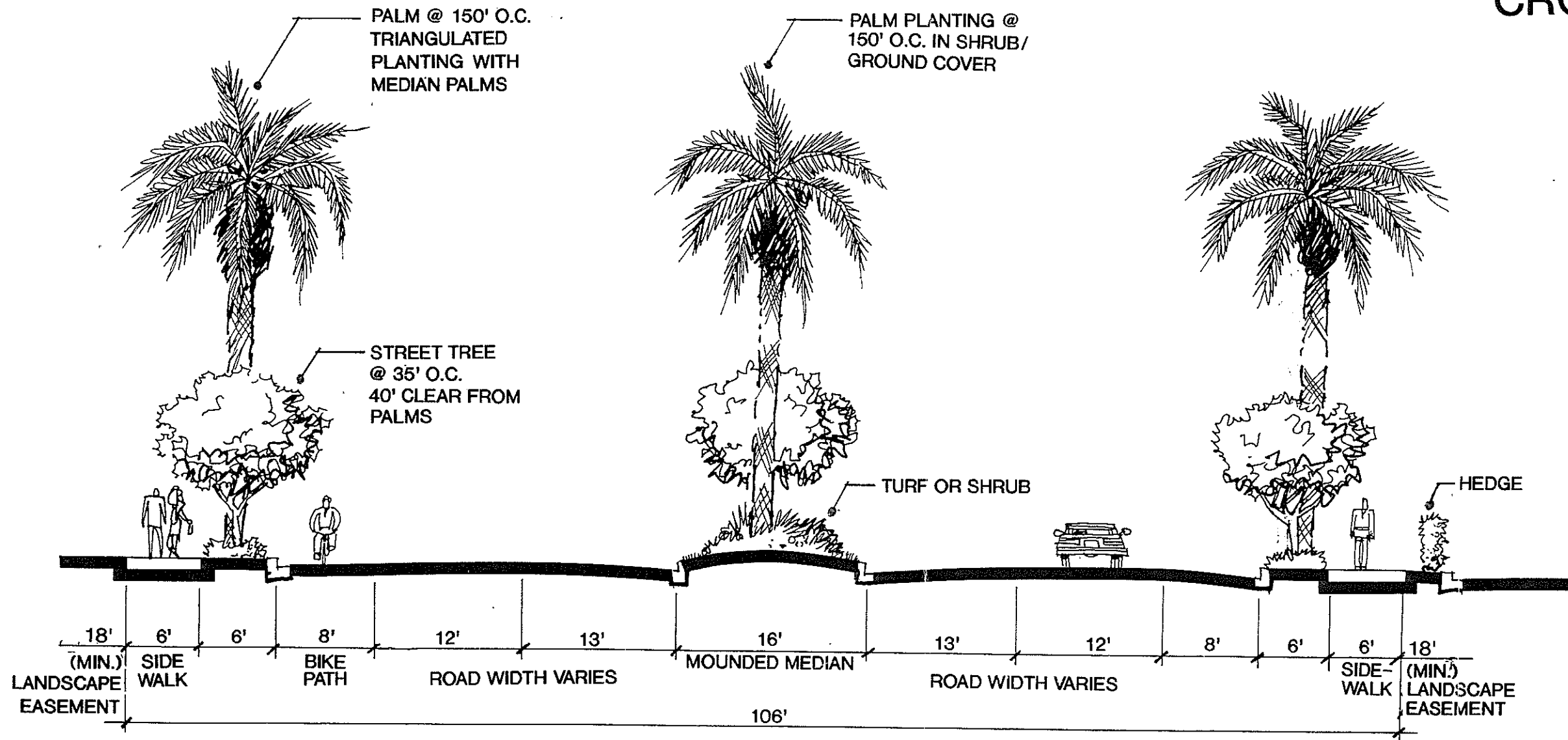
Ellis Avenue – 106-foot right-of-way. This design allows for two travel lanes and a bike lane in each direction. A 16-foot mounded median will allow for complete auto separation. There will be 30-feet of street tree landscaping and walkway on each side of the roadway as reflected on **Figure 11, Typical Street Section – Collector (Ellis Avenue)**.

Murrieta Road – 90-foot right-of-way. This design allows for two travel lanes in each direction. A 16-foot mounded median will allow for complete automobile separation. There will be 30-feet of street tree landscaping and walkway on each side of Murrieta as reflected on **Figure 12, Typical Street Section – Collector (Murrieta Rd)**.

Local Roadways

Local Streets – 60-foot right-of-way. This design allows for one travel lane in each direction, a 10-foot street tree and walkway easement on both sides of the road as reflected on **Figure 13, Typical Street Section – Local**.

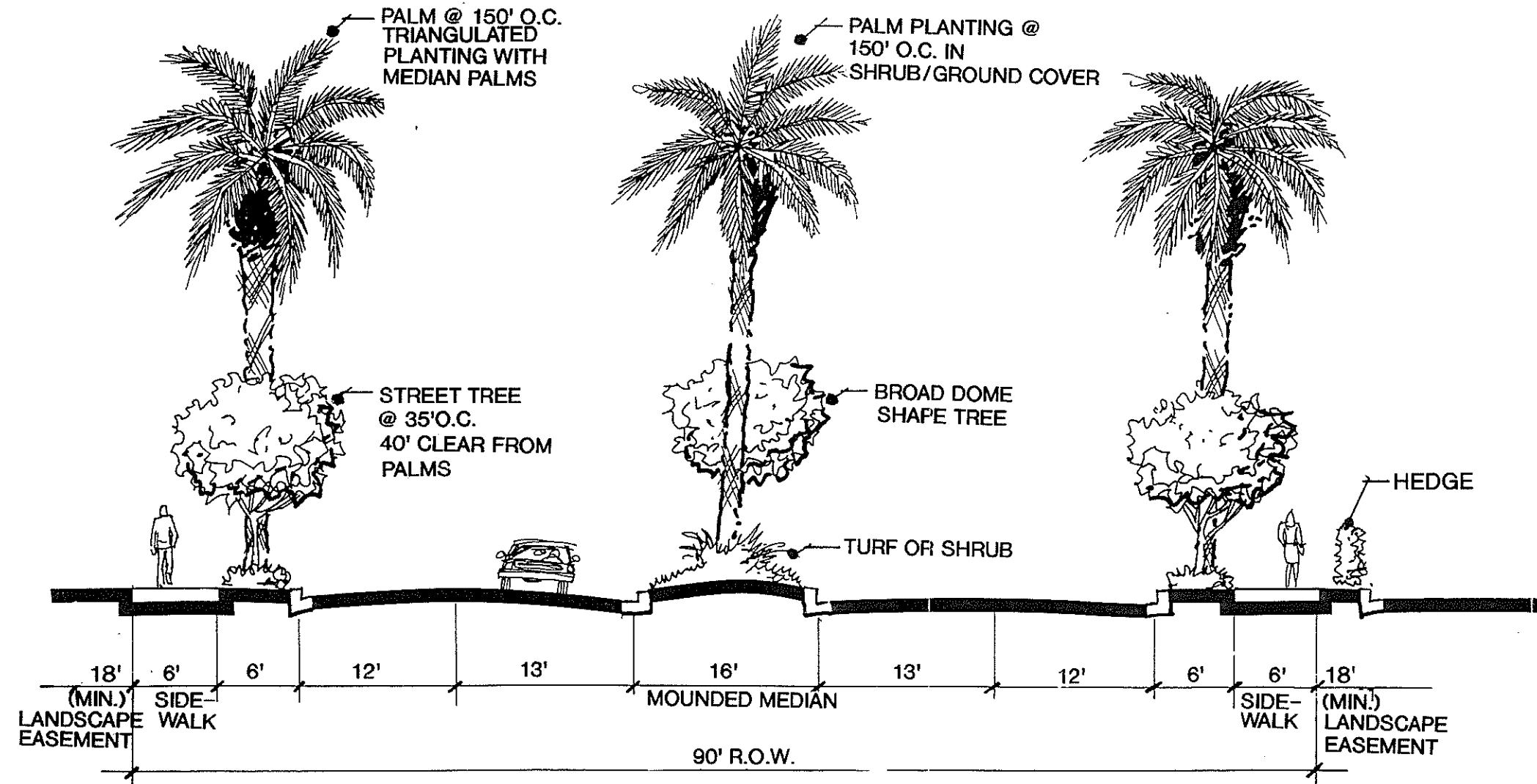
TYPICAL STREET CROSS-SECTIONS



COLLECTOR
ELLIS AVENUE

FIGURE 11

TYPICAL STREET CROSS-SECTIONS



COLLECTOR
MURRIETA ROAD

FIGURE 12

TYPICAL LOCAL STREET

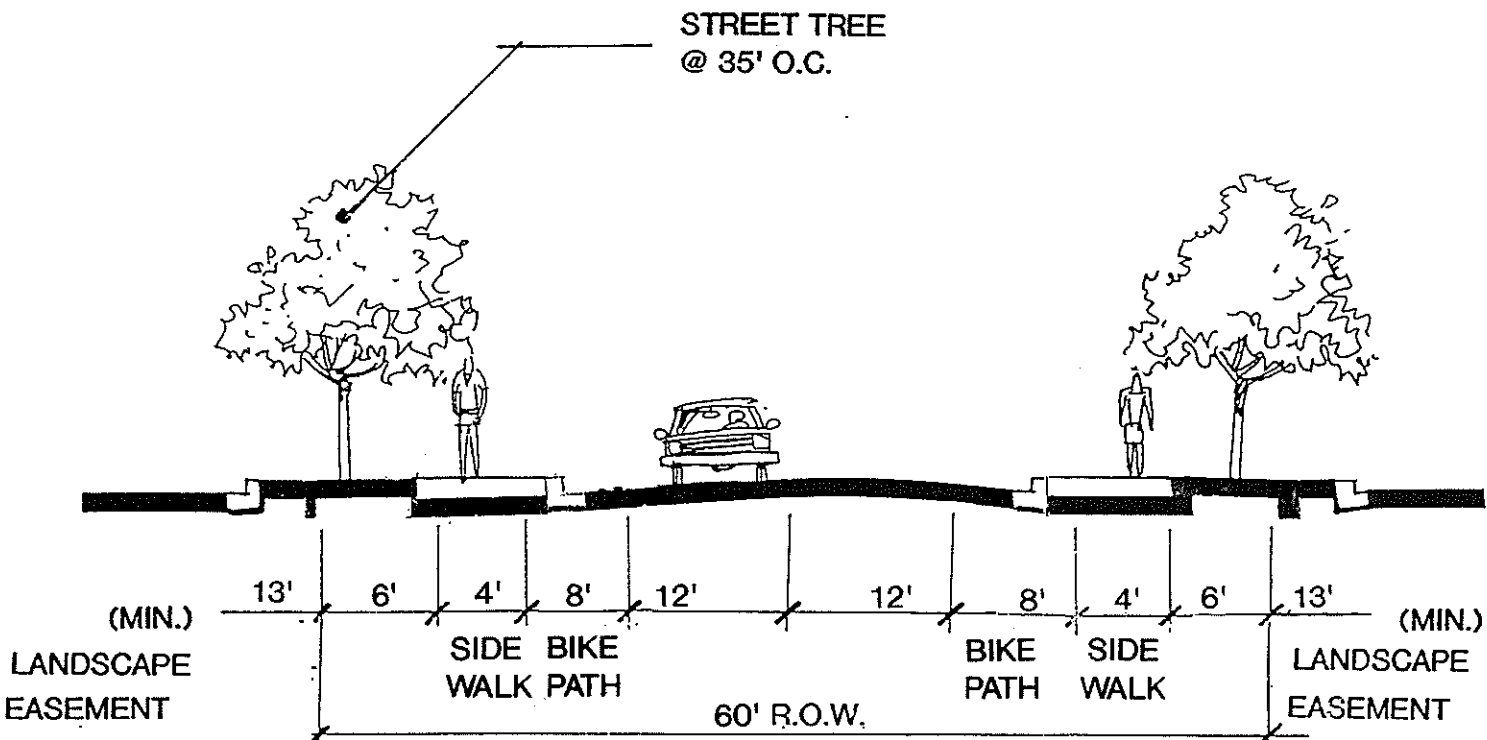


FIGURE 13

3.3 Landscaping Plan

3.3.1 Approach

The landscape design for the New Perris project will serve to enhance the overall image and character of the project. The landscape plan is intended to provide a project identity and cohesiveness throughout the development, visually soften the hardscape and urban core environment, and enhance unique features and primary architectural design through the harmonizing of landscape textures, forms, colors, mass, and geography. Landscaping is to be used to create screens and buffers between areas of differing land use intensity, compliment architectural elevations and roof lines and provide shade, shelter, texture color, form, and visual relief.

The landscape design shall have consideration of the environment and climatology of the Perris area so as to have proper plant growth and conservation of water resources. As illustrated in **Figure 14, Landscape Concept**, project landscaping will play an important role in maintaining overall project design themes while emphasizing community continuity.

3.3.2 Plan Description

This section of the Specific Plan will provide a general description and development standards for the New Perris landscape concept. Entry monumentation will provide initial definition for the site and will be viewed when approaching the project site from any direction. Once within the site, entry monumentation will continue to be present at any key intersections. Monumentation will be developed in a hierarchical format in size and texture from major village entry to low key village identity and will provide initial identification for each residential planning area, and the community as a whole.

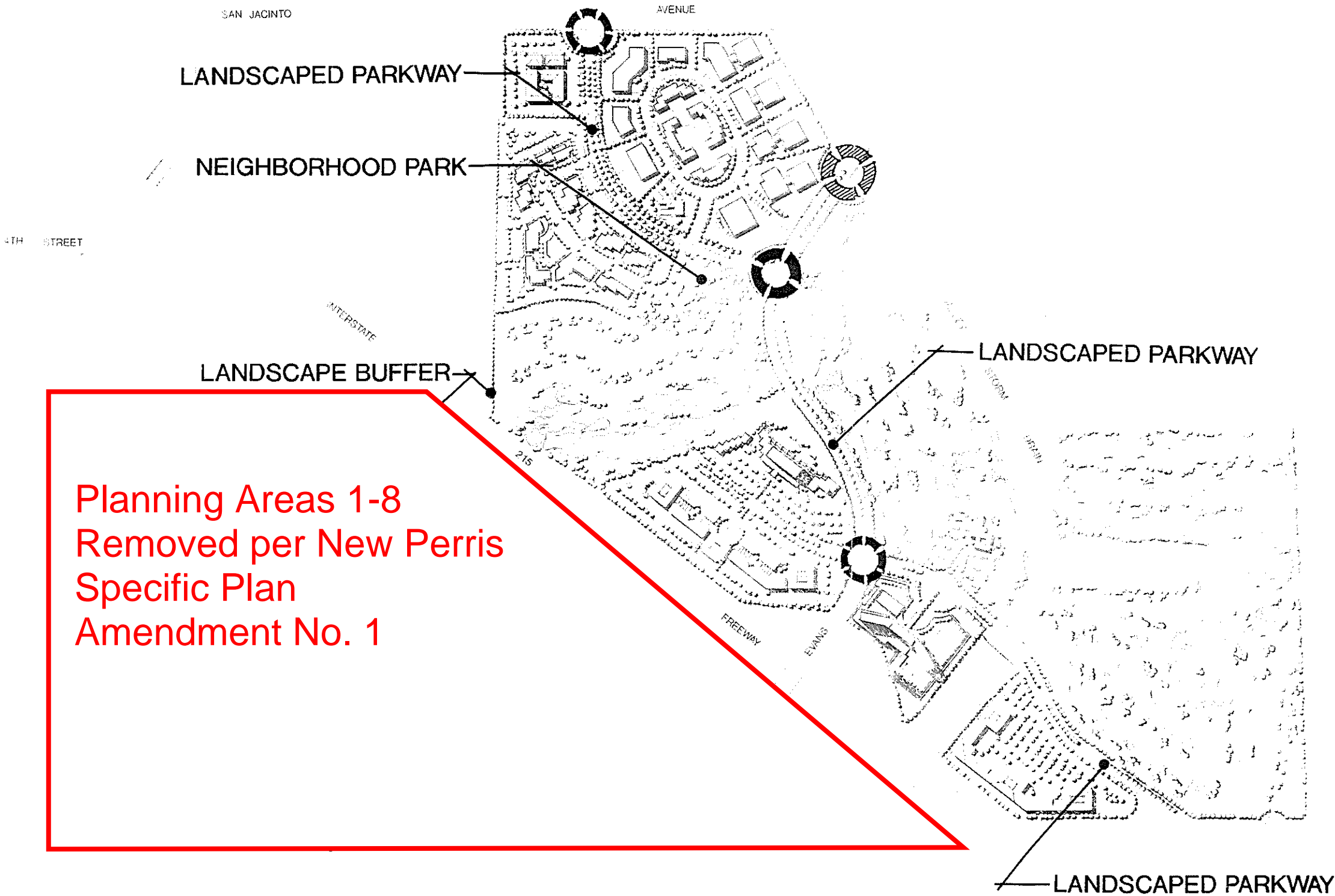
Landscaping will be used to identify the hierarchy of the New Perris street system, from major access corridors utilizing primary landscape treatments to local streets. This will be achieved through the creation of definitive landscape corridors and the careful consideration of the relationship between street size and plant material criteria such as size, form, texture and color.

Primary landscaping treatment shall occur on Evans Road at this major project entry. This use of landscaping will provide project identity and will create the feeling of arrival at a destination.




Secondary landscaping treatment will be designed for Murrieta Road and Ellis Avenue. The secondary treatment is designed to add to the overall design theme and provide unity throughout the project.

Major buffer treatments shall occur along I-215 while allowing view corridors to highlight various open space and architectural features of the project site.

OVERALL LANDSCAPE PLAN



LEGEND

-  VILLAGE ENTRY
-  PRIMARY ENTRY
-  SECONDARY ENTRY

Planning Areas 1-8
 Removed per New Perris
 Specific Plan
 Amendment No. 1

FIGURE 14



MM
P E R R I S

Landscape buffers will be creatively designed to incorporate and enhance all proposed land uses within the New Perris project site. Landscaping may be used to screen and provide aesthetic relief of parking structures, storage areas and open parking areas. Further, landscaping and trees will be utilized in open parking areas to reduce heat gain.

Each planning area shall have its own unique landscape design so that it is characterized by a distinct land use while providing a consistency and harmony with the New Perris overall landscape design concept.

3.3.3 Development Standards

The Landscape Plan consists of landscape components that individually identify specific conditions of the plan and their coordinated landscape treatment.

Collectively, all components are coordinated with the section of building and plant materials that serve to reinforce the overall unified appearance of the community.

A. Entry Themes

The village entry and primary entry monumentation as depicted in **Figure 15, Village and Primary Entries**, proposes enhanced entry signage walls utilizing materials that are reflective of the colors, forms, and textures of the area. The landscaping will consist of turf, berms, flowering shrubs, groundcover, canopy trees, and vertical accent trees. The primary and entry monumentation may be subject to refinements as development occurs.

The secondary entry monumentation as identified in **Figure 16, Secondary Entries**, continues the landscaping themes throughout New Perris and provide consistency in design as streets transition to local levels throughout the project site.

Figure 17, Entry Elevations provides conceptual level detail for all entry themes.

B. Landscape Parkways

The Landscape parkway design proposes a hierarchy of landscape treatment to identify the specific image of importance of roads servicing the project. Theme street trees will be planted to highlight the primary loop and collector roads serving the project along with flowering shrubs, groundcover, and canopy trees.

C. New Perris Plant Palette

The plant palette for New Perris may comprise an assortment of some of the following as reflected in **Table 3-3, Plant Palette**:

VILLAGE ENTRY

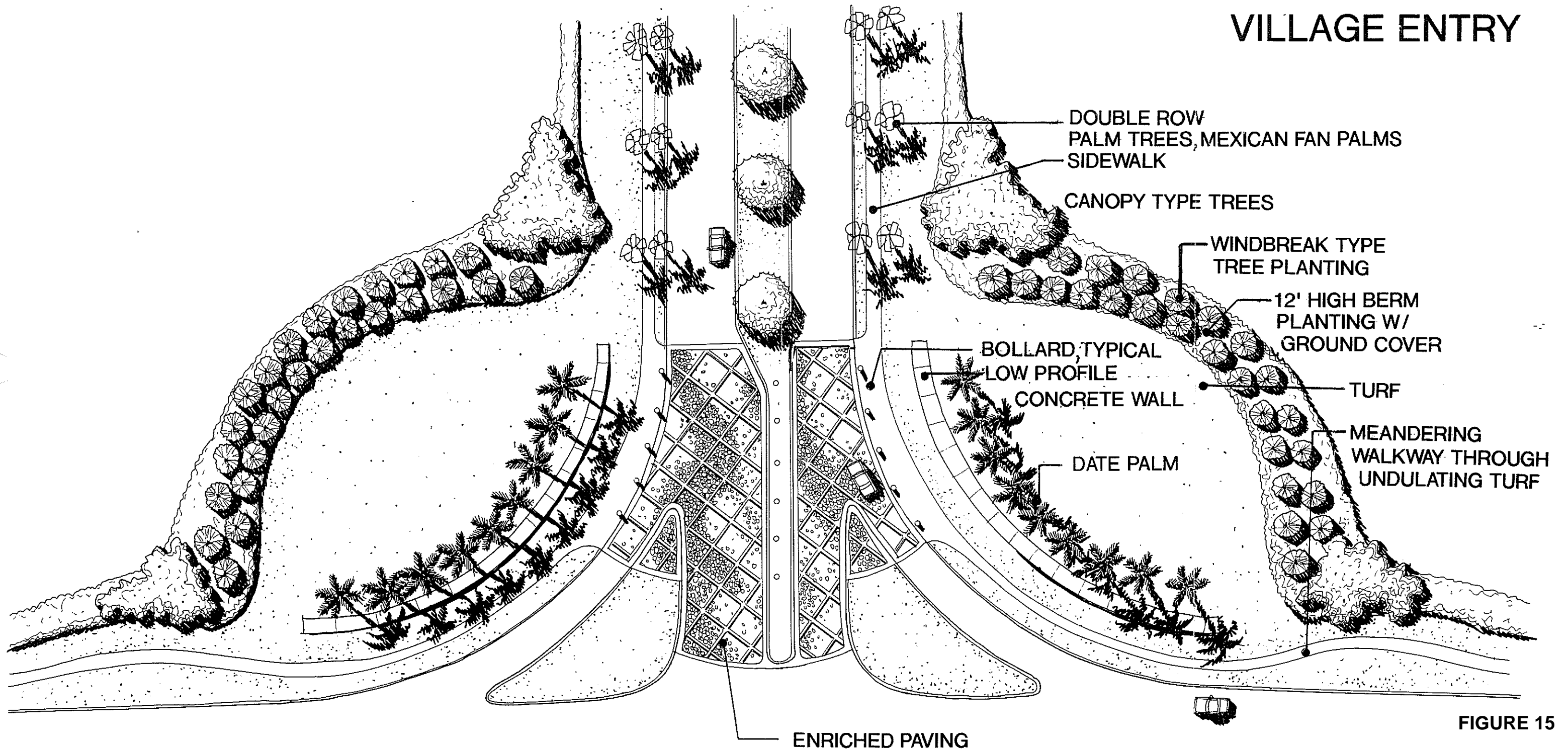


FIGURE 15

TYPICAL ENTRY

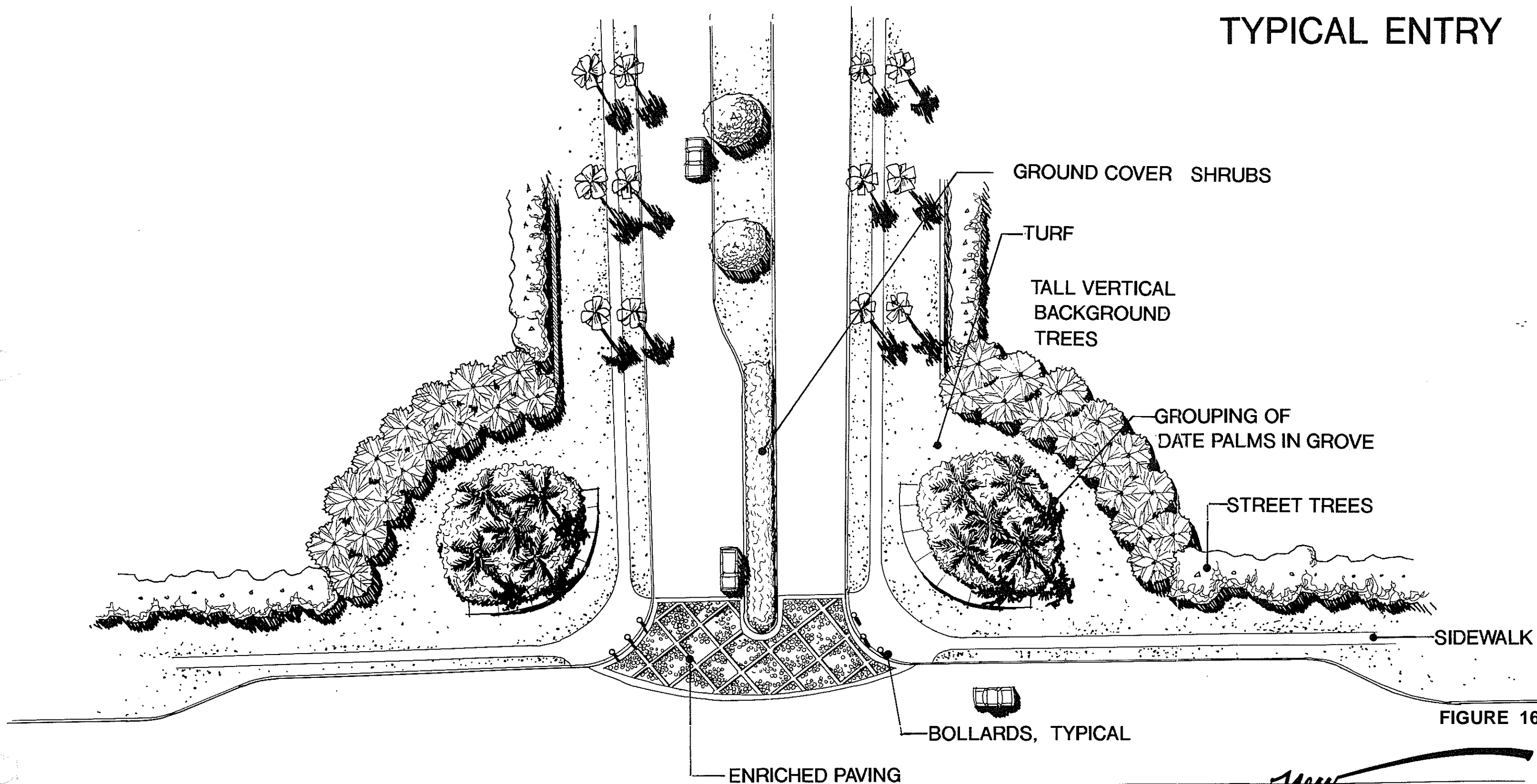


FIGURE 16

ENTRY ELEVATIONS

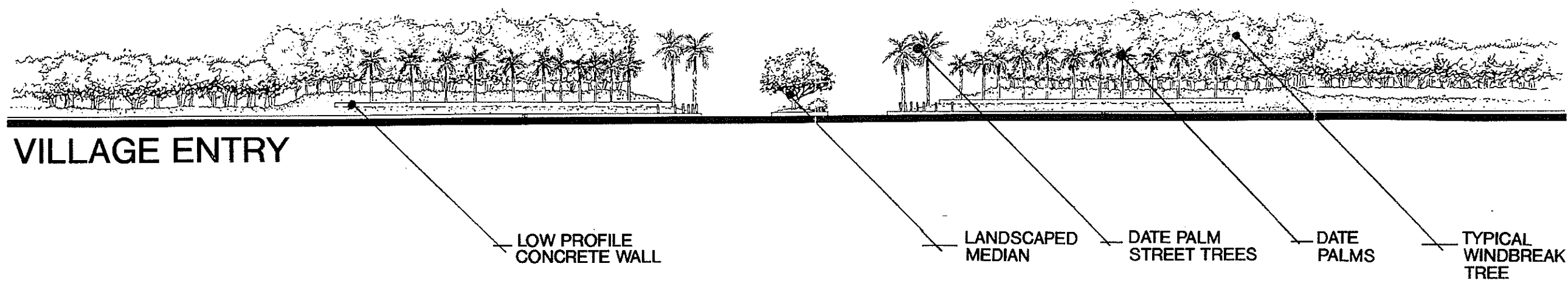
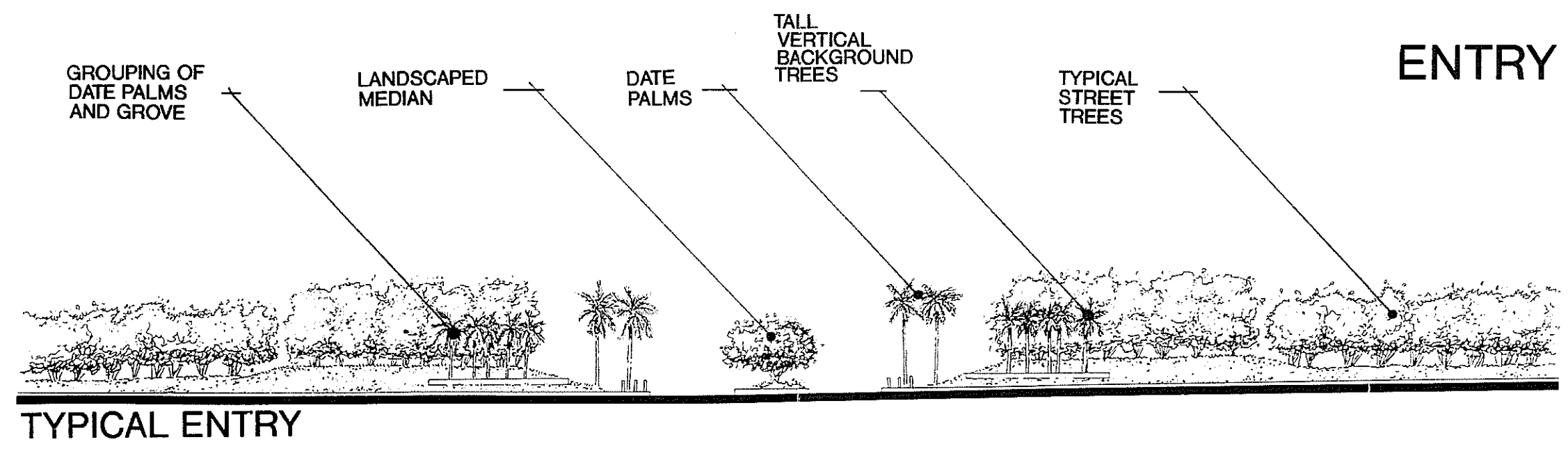


FIGURE 17

Table 3-3, Plant Palette

TREES - EVERGREEN	
Botanical Name	Common Name
Acacia baileyana	Bailey Acacia
Arbutus menziesii	Madrone
Brachychiton populneus	Bottle Tree
Casuarina stricta	Mountain She-Oak
*Cedrus species	Cedar
*Cinnamomum camphora	Camphor Tree
Eucalyptus camaldulensis	Red Gum
Eucalyptus leucoxyton "Rosea"	White Ironbark
Eucalyptus rudis	Desert Gum
Eucalyptus sideroxyton	Red Ironbark
Nerium oleander	Oleander Standard
Olea europaea 'Fruitless'	Fruitless Olive
Pinus edulis	Pinon Pine
Pinus halepensis	Allepo Pine
Pinus pinea	Italian Stone Pine
Podocarpus gracilior	Fern Pine
Quercus agrifolia	Coast Live Oak
Quercus engelmannii	Mesa Oak
Quercus ilex	Holly Oak
*Rhus lancea	African Sumac
Schinus molle	California Pepper
TREES - DECIDUOUS	
Botanical Name	Common Name
*Albizia julibrissin	Silk Tree
*Alnus rhombifolia	White Alder
Brachychiton acerifolius	Australian Flame Tree
*Chorisia speciosa	Silk Floss Tree
*Fraxinus uhdei	Evergreen Ash
*Fraxinus velutina 'Modesto'	Modesto Ash
*Fraxinus velutina 'Tomlinson'	Tomlinson Ash
*Ginkgo biloba	Maidenhair Tree
Gleditsia triacanthos 'Shademaster'	Honey Locust
Koelreuteria bipinnata	Chinese Flame Tree
Koelreuteria paniculata	Golden Rain Tree
*Lagersoemia indica	Crape Myrtle
*Liquidambar styraciflua	Sweet Gum
*Liquidambar orientalis	Oriental Sweet Gum
*Morus alba 'Fruitless'	Fruitless Mulberry
*Pistacia chinensis	Chinese Pistache
*Platanus acerifolia	London Plane Tree
*Platanus occidentalis	American Sycamore

Table 3-3, Plant Palette

*Platanus racemose	California Sycamore
*Populus fremontii	Fremont Cottonwood
*Populus nigra 'Italica'	Lombardy Poplar
Quercus conninea	Scarlet Oak
*Quercus painstrus	Pin Oak
*Robina idahoensis	Idaho Locust
*Sophora japonica	Japanese Pagoda Tree
*Ulmus parvifolia 'Drake'	Evergreen Elm
PALM	
Botanical Name	Common Name
*Phoenix canariensis	Canary Island Date Palm
Washingtonia robusta	Mexican Fan Palm
Washingtonia filifera	California Fan Palm
SHRUBS	
Bontanical Name	Common Name
*Abelia 'Edward Goucher'	Edward Goucher Abelia
Acacia species	No Common Name
Cercis occidentalis	Western Redbud
*Cistus species	Rockrose
Cotoneaster species	Cotoneaster
Eleagnus pungens	Silver Berry
Eriogonum giganteum	St. Catherine's Lace
Escallonia fradesii	No Common Name
*Euonymus japonica	Evergreen Euonymus
Feijoa sellowiana	Pineapple Guava
Ilex aquifolium	English Holly
Juniperus species	Juniper
Leptospermum scoparium	New Zealand Tea Tree
Ligustrum japonicum	Japanese Privet
*Mahonia aquifolium	Oregon Grape
*Nandian domestica	Heavenly Bamboo
Nerium oleander	Oleander
Osmanthus fragrans	Sweet Olive
*Phormium tenax	Flax
Pittosporum tobira	Tobira
*Plumbago capensis	Cape Plumbago
Podocarpus macrophyllus	Yew Pine
*Prunus caroliniana	Carolina Laurel Cherry
Pyracantha species	Firethorn
Raphiolepis species	Indian Hawthorn
*Rhus ovata	Sugar Bush
*Ribes sanguinum	Pink Winter Currant
Tecomaria capensis	Cape Honeysuckle

Table 3-3, Plant Palette	
Viburnum tinus 'Spring Bouquet'	Laurustinus
Xylosma congestum	Xylosma
SUB SHRUBS	
Botanical Name	Common Name
*Agapanthus africanus	Lily of the Nile
*Clivia minata	Clivia
*Hemerocallis species	Day Lily
Juniperus species	Juniper
*Lonicera japonica 'Halliana'	Hall's Honeysuckle
*Mahonia aquifolium 'Compacta'	Oregon Grape
*Moraea bicolor	Fortnight Lily
*Nandina domestica 'Compacta'	Compact Heavenly Bamboo
Nandina domestica 'Nan Compacta'	Dwarf Heavenly Bamboo
Nerium oldeander 'Petite Pink'	Dwarf Oleander
Pittosporum tobira 'Wheeler's	Dwarf Tobira
*Ribes viburnifolium	Catalina Perfume
Tulbaghia violacea	Society Garlic
Xylosma congestum 'Compacta'	Compact Xylosma
VINES	
Botanical Name	Common Name
*Cissus Antarctica	Kangaroo Treevine
Clytostoma callistegioides	Violet Trumpet Vine
Doxantha unguis-cati	Cat's Claw Vine
Gelseium sempervirens	Carolina Jasmine
*Jasminum mesnyi	Primrose Jasmine
*Lonicera japonica	Japanese Honeysuckle
*Parthenocissus tricuspidata	Boston Ivy
*Wisteria floribunda	Japanese Wisteria
GROUNDCOVERS	
Botanical Name	Common Name
*Ajuga reptans	Carpet Bugle
*Baccharis pilularis 'Twin Peaks'	Coyote Bush
*Cuchesnea indica	Indian Mock Strawberry
Gazania splendens 'Mitsuwa Yellow'	Gazania
Hypericum calycinum	Aarons Beard
Juniperus species	Juniper
*Lonicera japonica 'Halliana'	Honeysuckle
Myoporum parvifolium	Myoporum
*Potentilla species	Cinquefoil
*Vinca minor	Dwarf Periwinkle
Notes: *Plant materials sensitive with reclaimed water. Irrigation water to be kept away from leaves to avoid possible salt burn.	

D. Standards

- 1) A plant palette was developed with emphasis being placed on plants which are suitable to the Perris Valley area and the New Perris project theme. Selections were made with respect to growth factors such as reclaimed water, climate, soils and maintenance concerns. Plants were selected to promote and complement the New Perris project and provide it with its own identity. Plant variety is encouraged to add to the community theme. Where feasible. The use of plants compatible with the use of reclaimed water will be used.
- 2) The plant palette should be utilized to create a complement to the architectural style of each particular project.
- 3) Plants will establish themselves easier if certain precautions are taken. Standard landscape practices should be utilized for plantings during various seasons of the year.
- 4) All landscaped areas shall be planted with plant materials chosen from the plant palette contained within the section. Smooth transitions from landscaped common areas to adjoining properties are encouraged.
- 5) Horticultural soils tests are highly recommended and can be obtained prior to landscape planting from a qualified agricultural laboratory. The test summaries should include analysis of soil fertility and agricultural suitability and provide recommendations for pre-plant amendments, backfill mix and post plant maintenance fertilization.
- 6) A minimum 15 percent of all parking lots shall be landscaped. The landscaping material shall include but not be limited to trees, shrubs, and ground cover using approved plant materials from the landscape plan plant palette. There shall be no less than one tree per seven parking spaces.
- 7) Setback areas shall be landscaped with live plant material taken from the approved plant palette for the New Perris landscape plan.
- 8) All landscaped areas shall be provided with permanent irrigation facilities. Automatic irrigation is required as a part of landscape development. Irrigation systems shall be designed with head to head coverage with additional necessary to accommodate summer and winter patterns. Pop ups shall be used whenever there is a safety concern, such as near sidewalks, curbs, drives, wheel stops, or other hardscape. All additional equipment such as controllers should be located to minimize their view impacts.
- 9) The climate constraints have been taken into consideration in the plant palette and only materials that are suited to the prevailing climate have been chosen.
- 10) The landscape maintenance of each parcel is the responsibility of the owner or as agreed upon by the developer and the City of Perris. This maintenance shall be inclusive of all practices necessary to all landscape areas healthy, well-groomed, and clean. Diseased or dead plants shall be removed and replaced promptly. Hardscape shall be kept free of debris.

- 11) Dual irrigation lines shall be required so that reclaimed water can be used when it becomes available from the Eastern Metropolitan Water District.
- 12) All landscaping plans shall be done by a certified landscape architect.

3.4 Grading Plan

3.4.1 Approach

The proposed project is located in a relatively flat area. Grading for the proposed site is designed to accomplish the following:

- Provide for a balanced cut and fill situation requiring approximately 2,000,000 cubic yards of excavation so that neither import nor export of dirt is required;
- Provide floodproofing of the area north of I-215; and
- Allow adequate drainage of the project site.

3.4.2 Plan Description

The Grading Plan, as illustrated in **Figure 18, Grading Plan** provides a depiction of the grading concept for the proposed project site. Grading as proposed will result in a balanced earthwork situation and is designed to facilitate drainage from the site into the planned storm drain facilities.

Grading of the area north of I-215 is planned to raise pads of Planning Areas 9, 10, 11, 12, 14, 15, 16 and 19 by removing dirt from Planning Areas 13 and 18. Planning Areas 13 and 18 would remain subject to flooding after project completion; however, they are proposed for use as recreational open space, a use that will not be adversely impacted by flooding during a storm.

3.4.3 Development Standards

Grading will be performed in accordance with the appropriate City of Perris policies and guidelines.

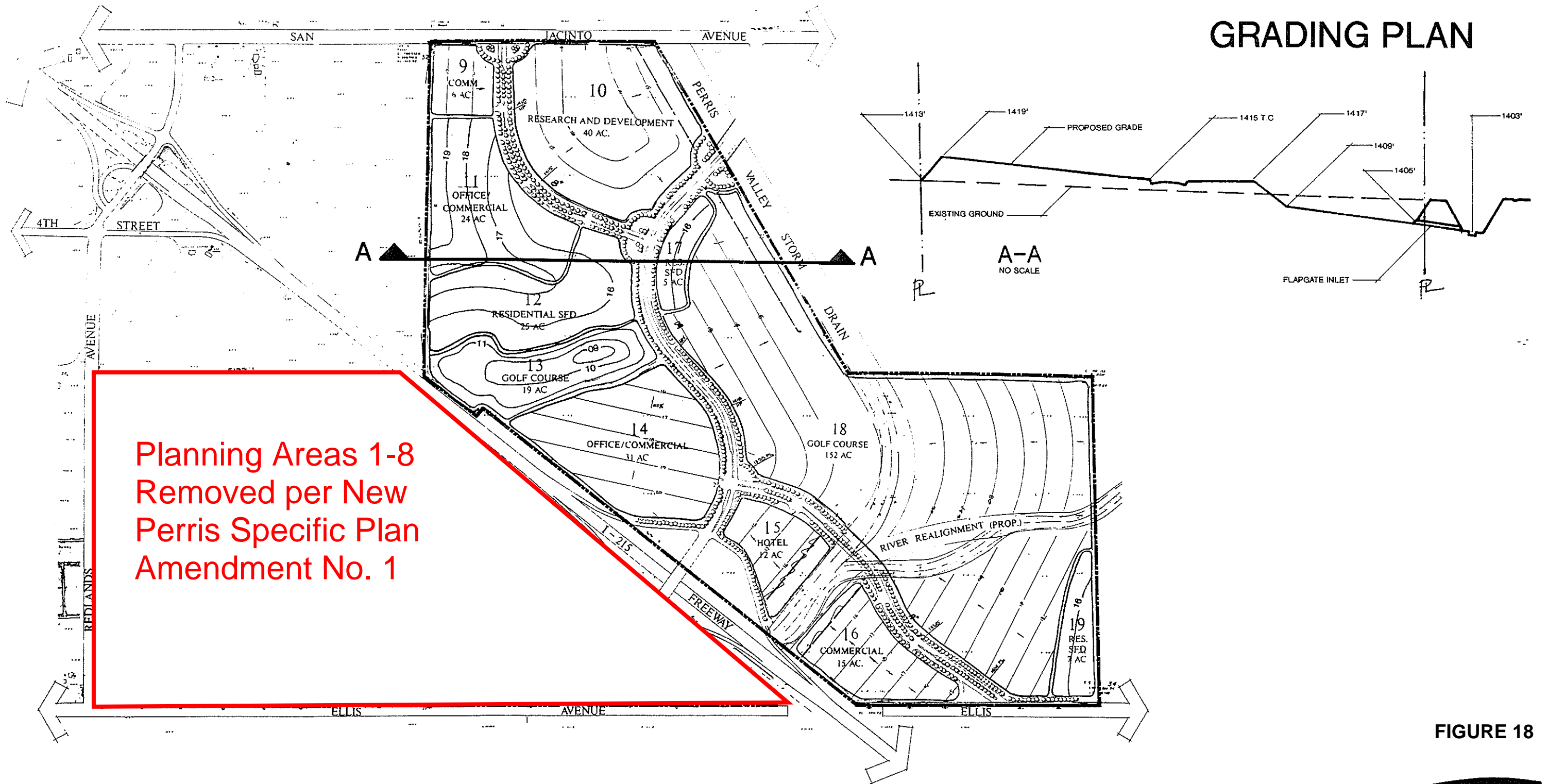


FIGURE 18

3.5 Drainage Plan

3.5.1 Approach

The project site is subject to inundation by the 100 year storm runoff. Improvements which are planned by the Riverside County Flood Control and Water Conservation District (Flood Control District) for channeling storm flow in the area include the San Jacinto Storm Drain Channel, which transects the southeastern portion of the project site, and the Perris Valley Storm Drain, located in the eastern portion of the site. There are currently on-going studies between the City of Perris and the Flood Control District relative to the design concepts of the Perris Valley Channel. The results of such studies may impact the design of the Perris Valley project as it passes through the project site.

The property is within the San Jacinto River Improvement District 4-2 and has been assessed a fee by the Flood Control District for the construction of the San Jacinto Storm Drain Channel. Construction of the channel by the Flood Control District will remove areas south of I-215 from the 100 year floodplain of the San Jacinto River. However, the portion of the site north of I-215 will remain subject to inundation during the 100 year storms.

3.5.2 Plan Description

The Drainage Plan, as illustrated in **Figure 19, Drainage Plan** and **Figure 20, Channel Cross-Sections**, present the improvements necessary for flood control on the project site. Elements of the drainage system include:

- A. The San Jacinto Channel and the Perris Valley Channel, which will be constructed by the Flood Control District, and the existing Perris Valley Storm Drain Channel.
- B. Within Planning Areas 13 and 18, earth shall be evacuated to obtain material to fill Planning Area 9 through 12, 14, 15, 16 and 19. The grading shall anticipate the construction of the Perris Valley Storm Drain and San Jacinto River channel. These channels will carry the design flows to the channel crossing at I-215. When the design flows are exceeded, the excess flows will spill over the channel levees into Planning Areas 13 and 18 which will be drained by the use of flap gate structures as the flows in the channels recede.
- C. Construction of storm drain systems on the project site including the portion of the project north of I-215 that will drain toward the Perris Valley Storm Drain channel. A channel and subchannel is proposed north of I-215 west of the Perris Valley Storm Channel to channel a portion of the storm water runoff to the Perris Valley Channel.
- D. Grading of the project will allow for proper drainage from the site to the proposed storm drains. The grading proposed will allow the area north of I-215 to drain to the Perris Valley Storm Drain.
- E. Grading of the area north of I-215 to raise the pads of Planning Areas 9, 10, 11, 12, 14, 15, 16 and 19 by removing dirt from Planning Areas 13

and 18. Planning Areas 13 and 18 would remain subject to flooding after project completion; however, these areas are proposed for use as recreational open space; a use that will not be adversely impacted by flooding during a storm.

- F. The project is subject to modifications as result of final design of the Perris Valley Storm Drain Channel and San Jacinto River. Currently, the project is subject to a building moratorium within a 750-foot right-of-way for the Perris Valley Storm Drain Channel and San Jacinto River.

3.5.3

Development Standards






Development standards will be used in detailed engineering design of the storm drain system in the New Perris project so that proposed facilities will:

- Protect homes, schools, commercial and industrial areas;
- Meet the requirements of the City of Perris; and
- Meet the requirements of the Riverside County Flood Control and Water Conservation District.

DRAINAGE PLAN

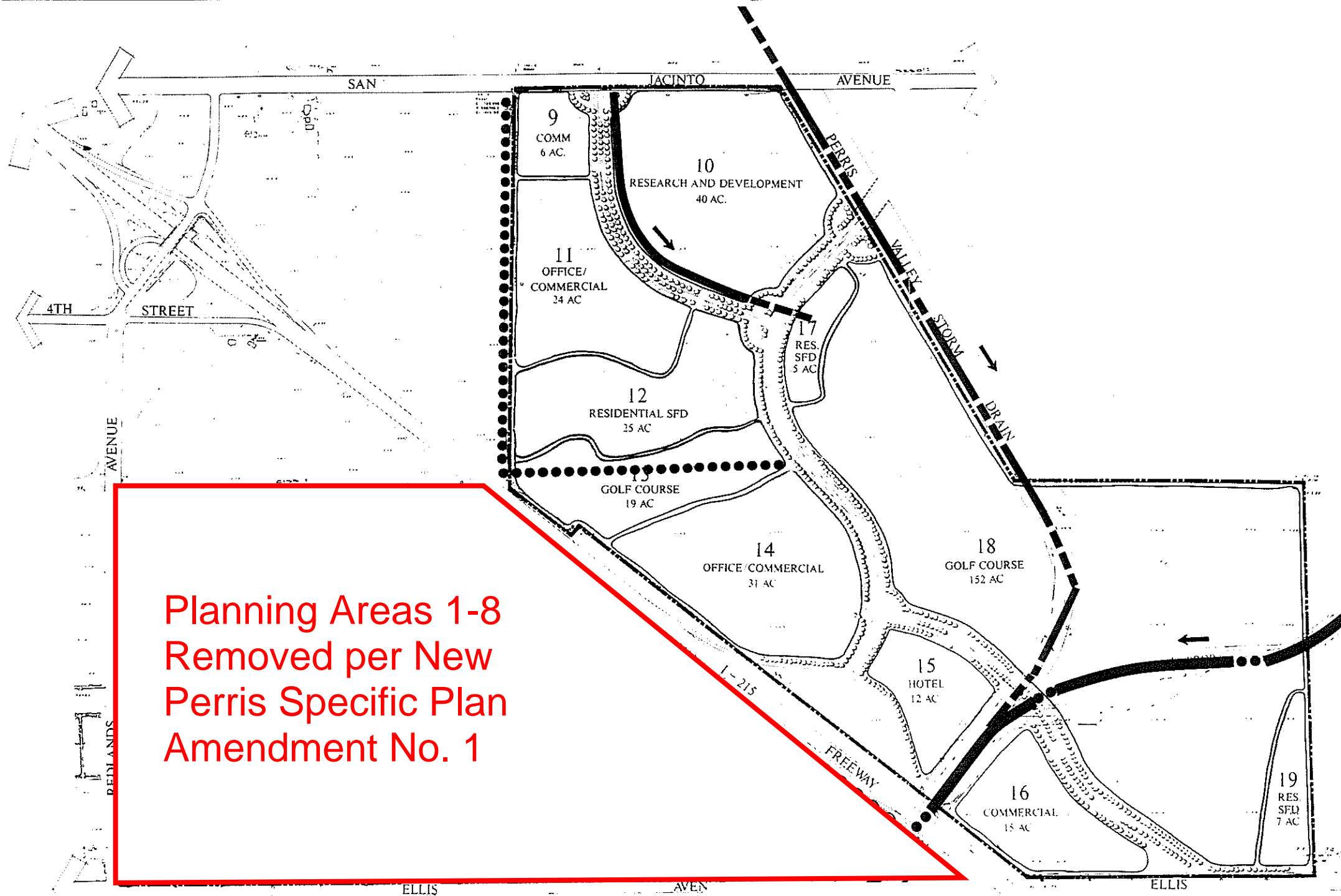
LEGEND

ON-SITE IMPROVEMENTS

-  FLOW DIRECTION
-  PROPOSED CHANNEL
-  PROPOSED SUB-CHANNEL
-  EXISTING PERRIS CITY STORM DRAIN CHANNEL 78' R.W.
-  PROPOSED STORM DRAIN (48")

OFF-SITE IMPROVEMENTS

-  SAN JACINTO RIVER
-  PERRIS VALLEY STORM DRAIN



Planning Areas 1-8
Removed per New
Perris Specific Plan
Amendment No. 1

FIGURE 19

CHANNEL CROSS-SECTIONS

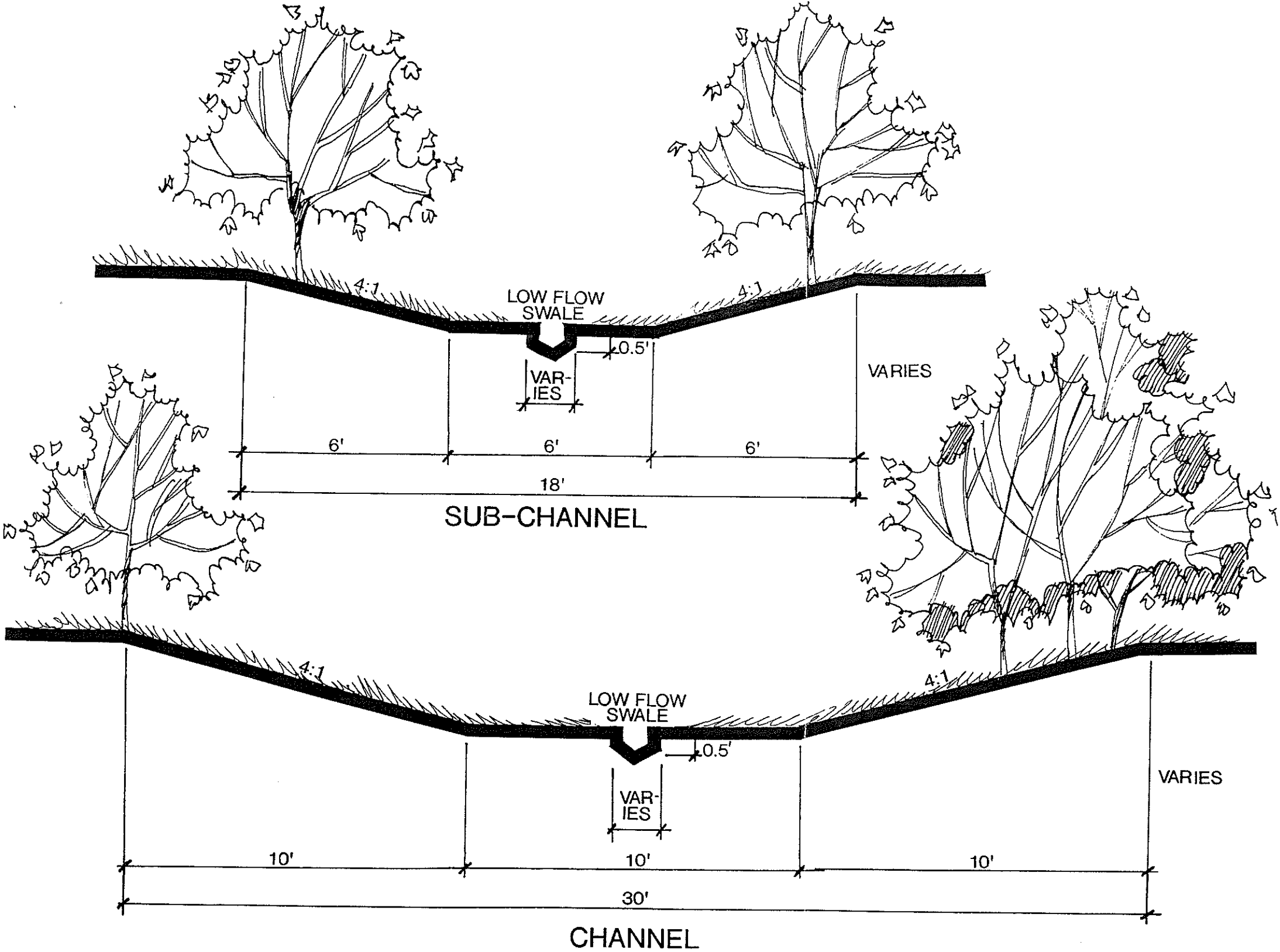


FIGURE 20

3.6 Water and Sewer Plan

3.6.1 Approach

The New Perris project site is located within the Eastern Municipal Water District (EMWD), the public agency responsible for the provision of both water and sewer service to the project site.

Water

The project site is within the 1,627 foot service zone of EMWD. Waterlines which exist in the vicinity of the project site include a 36" diameter line in Murrieta Road and a 12" to 36" diameter line in Ellis Avenue.

Sewer

Wastewater generated within the project area would receive treatment at EMWD's Perris Valley Treatment Plant which is located south of the project site off of Case Road. No sewer lines exist in the vicinity of the project site; however, a trunk sewer line is proposed by EMWD south of the project site.

Reclaimed Wastewater

Eastern Municipal Water District discharges treated effluent from the Perris Valley Treatment Plant into a 36" diameter effluent line in Dunlap Road. The reclaimed wastewater is then used for irrigational purposes within EMWD's service area.

3.6.2 Plan Description

Water

Average day and peak day water demands for the project site were determined based upon proposed land uses reflected in **Table 3-4, Water Demand (Gallons)**. A fire flow of 5,000 gallons per minute (gpm) with a 30 pound residual pressure were used in the calculations. The average day water demand is estimated to be approximately 1.7 million gallons; peak day demand is estimated to be approximately 2.3 million gallons. Based upon expected water needs, the conceptual water distribution facilities were determined and are illustrated within **Figure 21, Water Plan**.

It is anticipated that the proposed project will connect with the 36" waterline which crosses the project site at Murrieta Road. The existing 36" waterline will be relocated on-site so that it is within the proposed road network. An internal loop system of 16" waterlines is proposed for the majority of the site.



Eastern Municipal Water District is currently in the process of preparing a Master Water Plan which will take approximately one year to complete. When the master plan is completed, modifications to the improvements proposed within the New Perris Specific Plan may be necessary.

Table 3-4, Water Demand (Gallons)				
Land Use	Total Acres	Dwelling Units	Average Day Demand¹	Peak Demand²
Hotel	12	-	43,200	86,400
Residential (SFD)	37	114	29,600	59,200
Office/Commercial	55	-	198,000	396,000
Commercial	21	-	75,600	151,200
Research and Development	40		144,000	288,000
Golf Course ³	171		61,600	123,200
Subtotal			552,000	1,104,000
Fire Flow ⁴			1,200,000	1,200,000
TOTALS			1,752,000	2,304,000
Notes:				
1. Average Day Demand (ADD) Residential: ADD = 200 gal/cap/day and 4 people/dwelling unit Commercial: ADD = 3,600 gal/ac/day				
2. Peak Factor = 2 (Residential and Commercial)				
3. Assumes water demand over 10 percent of total acreage				
4. Fire Flow = 5,000 gpm x 240 min				

Planning Areas 1-8
 Removed per New
 Perris Specific Plan
 Amendment No. 1

WATER PLAN

LEGEND

-  EXISTING WATERLINE (SIZE NOTED)
-  PROPOSED WATERLINE (SIZE NOTED)

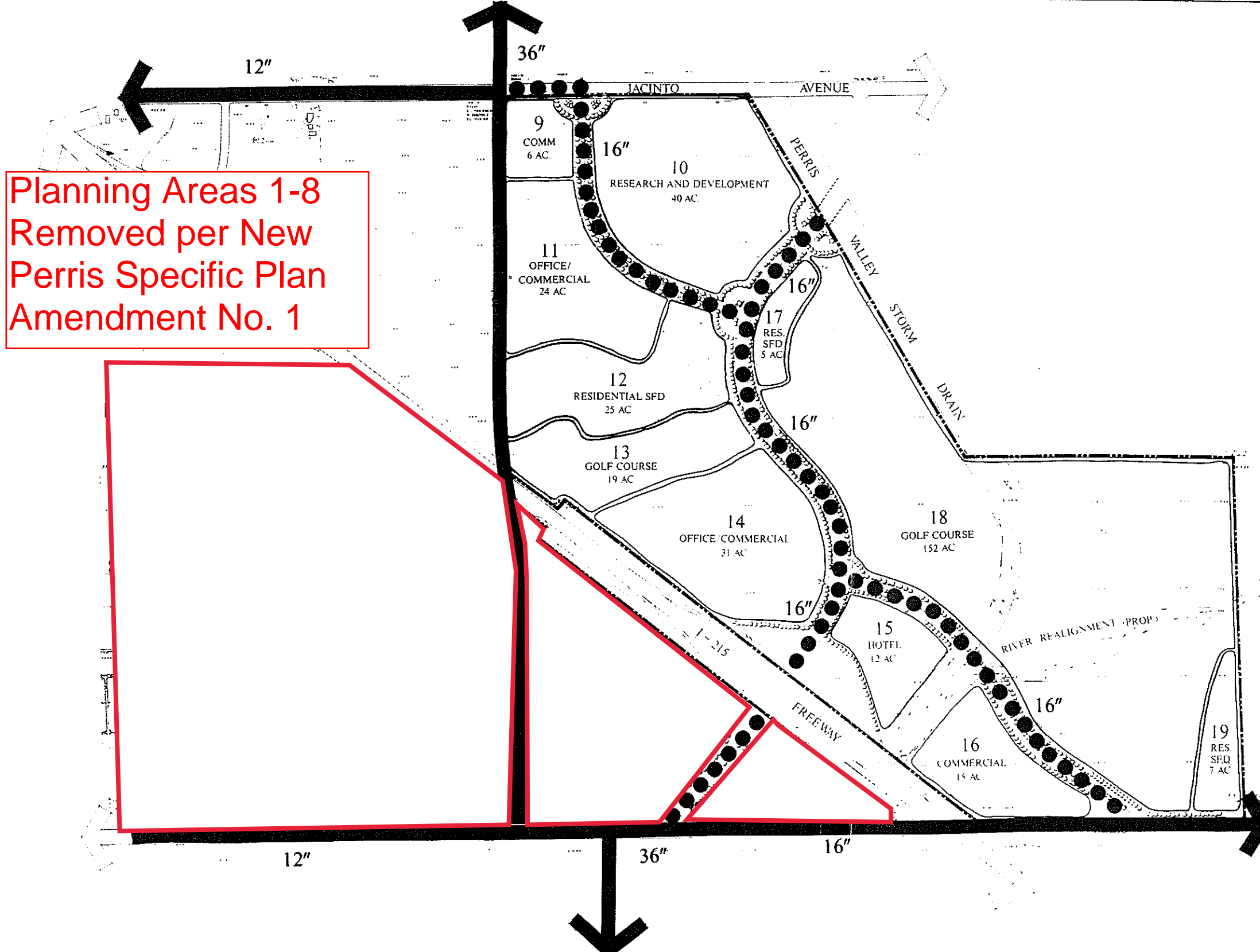


FIGURE 21

Sewer

The quantity of wastewater expected from the proposed project was estimated based on the proposed land uses for the area as reflected in **Table 3-5, Wastewater Generation (Gallons)**. A total of approximately 450,100 gallons per day of wastewater is expected to be generated by development within the project area. Facilities to serve the project are shown within **Figure 22, Sewer Plan**.

Table 3-5, Wastewater Generation (Gallons)			
Land Use	Total Acres	Dwelling Units	Average Day Generation¹
Hotel	12	-	36,000
Office/Commercial	55	-	165,000
Commercial	21	-	63,000
Research and Development	40	-	120,000
Residential (SFD)	37	114	14,800
Golf Course ²	171		51,300
TOTAL			450,100
Notes:			
1. Average Day Demand (ADD) Residential: Average Day Demand = 100 gal/cap/day and 4 people/dwelling unit Commercial: Average Day Demand = 3,000 gal/ac/day			
2. Assumes wastewater generation from 10 percent of total acreage.			

I-215




The project site will be served by:

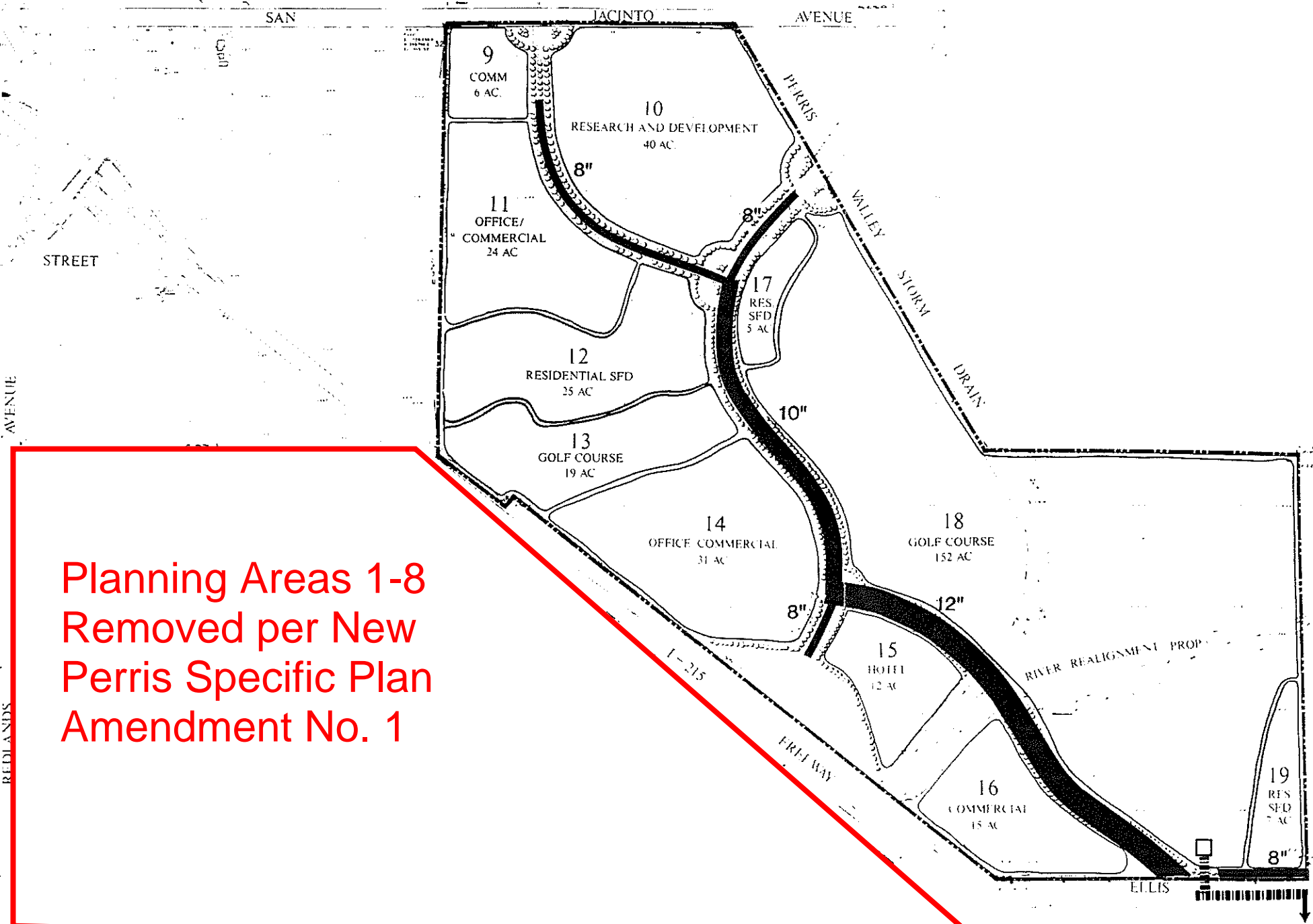
- A system of 8" to 10" lines which will naturally flow with gravity to a 12" line to a lift station on Ellis Road at the southeast corner of the project site. The wastewater will be lifted to a force main which will connect to a future trunk sewer line flowing to the Perris Valley Treatment Plant.

The wastewater collected on the project site will be discharged into the Perris Valley Treatment Plant. Eastern Municipal Water District is currently in the process of preparing a Master Sewer Plan which will take approximately one year to complete. When the master plan is completed, modifications to the improvements proposed within the New Perris Specific Plan may be necessary.

SEWER PLAN

LEGEND

-  PROPOSED SEWER
-  LIFT STATION
-  PROPOSED FORCE MAIN



Planning Areas 1-8
Removed per New
Perris Specific Plan
Amendment No. 1

JOIN FUTURE TRUNK
LINE TO PERRIS VALLEY
TREATMENT PLANT

FIGURE 22

Reclaimed Wastewater

It is anticipated that reclaimed wastewater will be used for the maintenance of parks, medians, schools and open space areas within the project area. The quantity of wastewater to be used for irrigation purposes was estimated based upon the proposed land uses for the area as reflected in **Table 3-6, Irrigation Demand (Gallons)**. The quantity was determined on the basis of applying 2.25 inches per acre during the peak week. A total of approximately 2.6 million gallons per day of wastewater will be required to irrigate the project site. Facilities to serve the project include the installation of 8-inch, 12-inch and 16-inch pipeline as reflected in **Figure 23, Reclaimed Water**. The source of supply will originate from a proposed 1,627 foot Eastern Municipal Water District wastewater service zone. This service zone has not been established by EMWD as of this date. Therefore, the installation of a ground storage reservoir and booster pump facilities may be required to provide the necessary operating pressure to irrigate the project site.

Table 3-6, Irrigation Demand (Gallons)		
Land Use	Total Acres	Peak Day Application¹
Hotel	12	104,740
Office/Commercial	55	480,040
Commercial	21	183,290
Research and Development	40	349,120
Golf Course	171	1,492,490
TOTAL		2,609,680
Notes:		
1. Peak Day Application = Acres x 8,728 gallons. Rounded to nearest whole number.		

3.6.3



Development Standards

Standards will be used in the detailed engineering design of the water distribution and wastewater collection systems so that proposed facilities:

- Will be adequately sized to meet the requirements of landowners within the project area based upon the proposed land uses;
- Will meet Eastern Municipal District requirements;
- Will meet the requirements of the City of Perris; and
- Are installed in accordance with the requirements and specifications of the Riverside County Health Department.

RECLAIMED WATER PLAN

LEGEND

-  EXISTING RECLAIMED WATERLINE (Size noted)
-  PROPOSED RECLAIMED WATERLINE (Size noted)

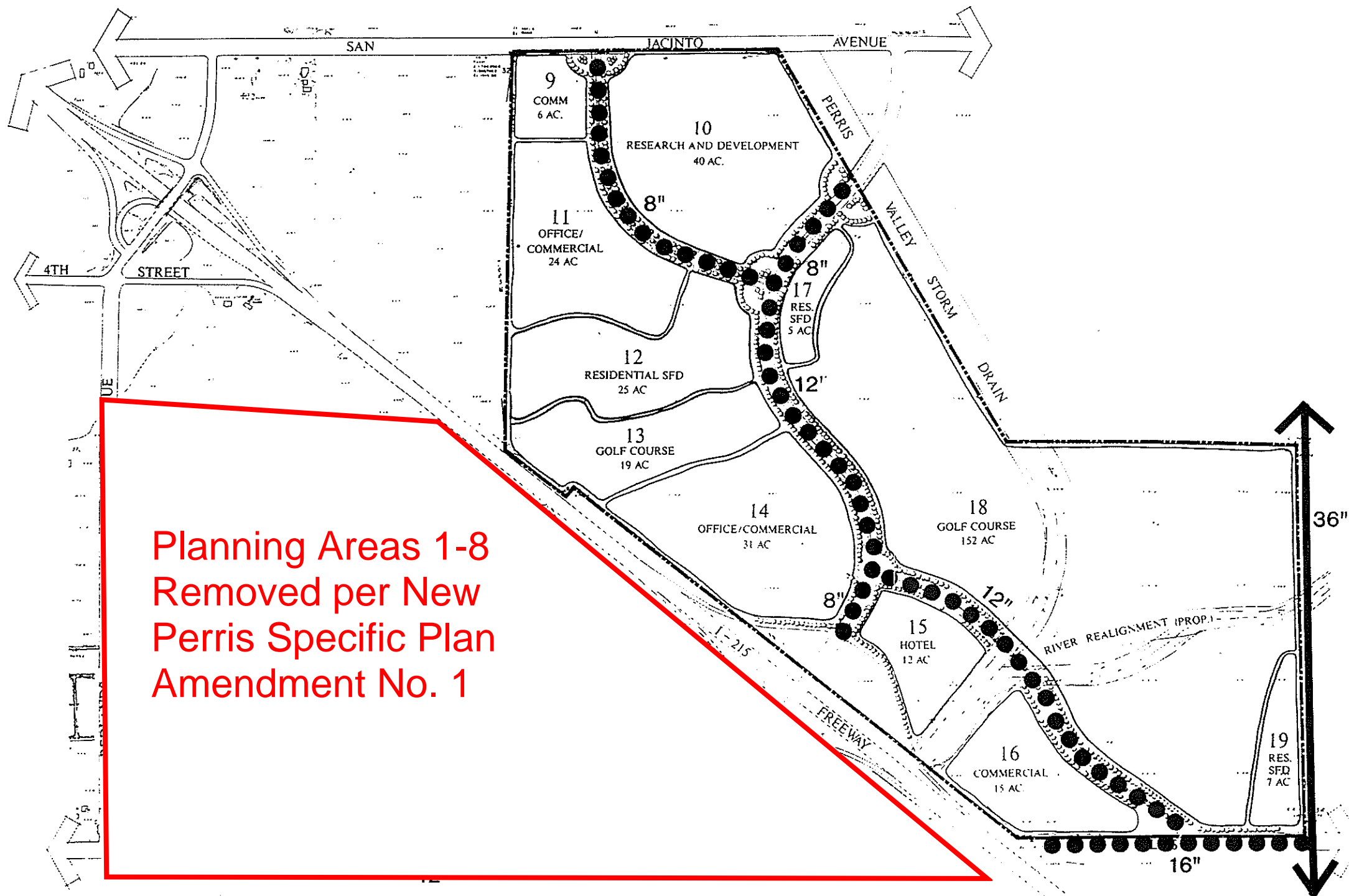


FIGURE 23

3.7 Project Phasing

3.7.1 Approach

The Phasing Plan is designed with the purpose of developing the property in a timely manner in response to market demands, economic concerns and a logical extension of roadways, public utilities and infrastructure. It is anticipated that buildout will occur in approximately 10- 15 years. The overall phasing program is illustrated in **Figure 24, Phasing Plan**.

3.7.2 Plan Description

- A. **Phase One:** Phase One shall incorporate Planning Areas 9, 10 and 11. Phase One will be built with supporting infrastructure. As noted, on the Drainage Plan, on-site channel and sub-channel improvements shall be constructed as part of Phase One. Buildout for Phase One will occur in approximately 5-10 years once construction commenced.
- B. **Phase Two:** Phase Two will consist of Planning Areas 12, 13, 17 and 18, which includes single-family residential and the golf course. Supporting infrastructure will also be provided. Buildout will occur in approximately 5-10 years once construction commenced.
- C. **Phase Three:** Phase Three incorporates Planning Areas 15 and 19. These areas include a hotel and single-family residential. Supporting infrastructure for the area will coincide with development. Buildout will occur in approximately five years once construction commenced.
- D. **Phase 4:** Phase Four incorporates Planning Areas 14 and 16 which consists of office and commercial uses. Supporting infrastructure will be constructed at the same time. Buildout will occur in approximately five years once construction commenced.

3.7.3 Development Standards





- A. Each planning area shall include development of common open space areas and infrastructure.
- B. Construction of the development permitted hereby, including recordation of final subdivision maps, may be done progressively in stages, provided adequate vehicular access is constructed for all dwelling units in each stage of development and further provided that such phase of development conforms substantially with the intent and purpose of the specific plan phasing program.
- C. Ensuing phasing may require the need for an alternative phasing plan. This may result from changes in regional and physical plans. Should this occur, an alternate phasing plan may be filed for administrative review concurrent with the master tentative map submittal, or as approved by the Director of Planning and Community Development and/or the City Engineer.

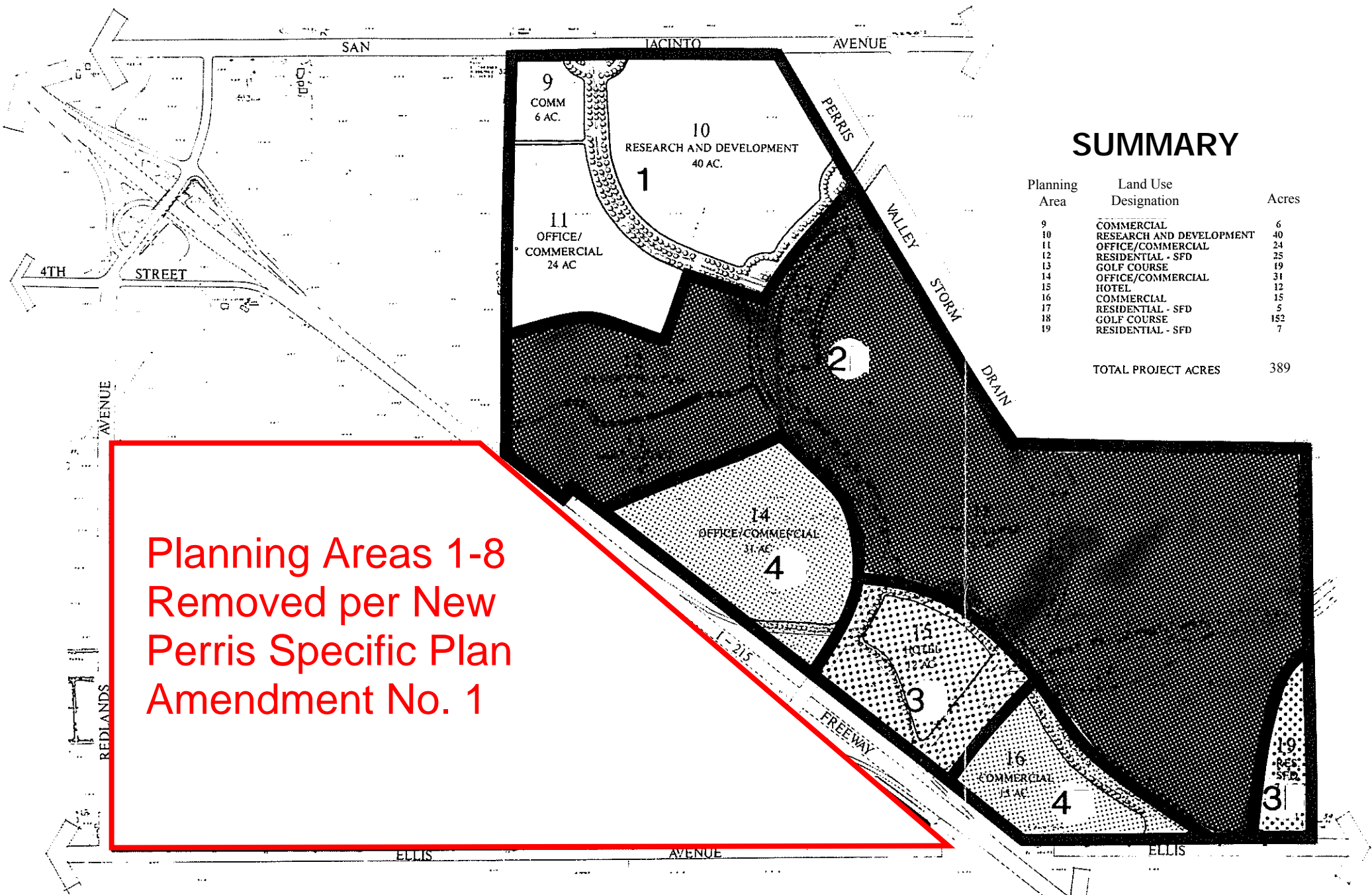
PHASING PLAN

SUMMARY

Planning Area	Land Use Designation	Acres
9	COMMERCIAL	6
10	RESEARCH AND DEVELOPMENT	40
11	OFFICE/COMMERCIAL	24
12	RESIDENTIAL - SFD	25
13	GOLF COURSE	19
14	OFFICE/COMMERCIAL	31
15	HOTEL	12
16	COMMERCIAL	15
17	RESIDENTIAL - SFD	5
18	GOLF COURSE	152
19	RESIDENTIAL - SFD	7
TOTAL PROJECT ACRES		389

LEGEND

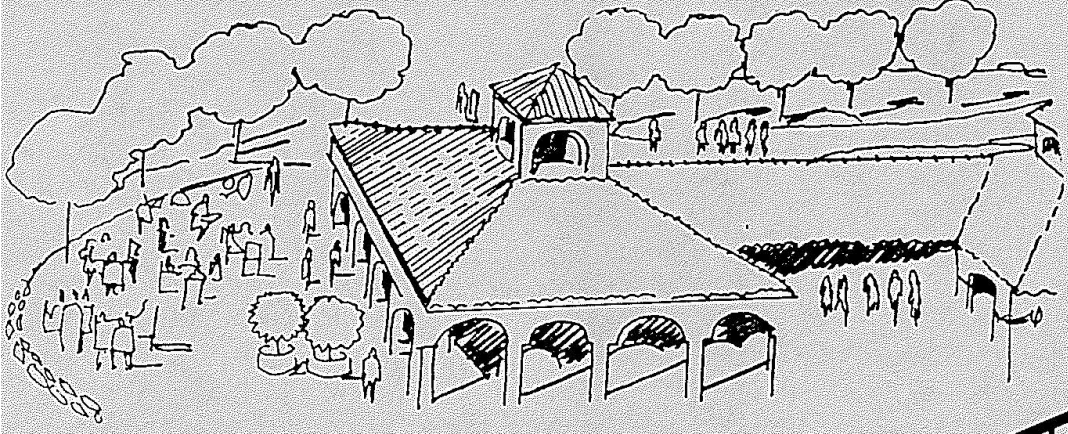
-  PHASE 1
-  PHASE 2
-  PHASE 3
-  PHASE 4



Planning Areas 1-8
Removed per New
Perris Specific Plan
Amendment No. 1

NOTE:
CHANNELS AND SUBCHANNELS TO
BE CONSTRUCTED CONCURRENT
WITH PHASE 1 DEVELOPMENT.

FIGURE 24



4.0 PLANNING AREA DEVELOPMENT STANDARDS

4.0 PLANNING AREA DEVELOPMENT STANDARDS

Planning Areas 1 through 8 have been removed by the New Perris Specific Plan, Amendment No. 1.

4.1 Planning Area 9 – Retail Commercial

4.1.1 Land Use

Planning Area 9 is a 6-acre site that will consist of retail commercial land uses and located in the northern portion of the property, west of Murrieta Road and adjacent to San Jacinto Avenue. Planning Area 9 will consist of retail commercial land uses intended to provide goods and services to the local community and is depicted in **Figure 25, Planning Area 9** and **Figure 26, Planning Area 9 Detail**.

4.1.2 Zoning Ordinance Regulations

A. Permitted Uses. The following general uses shall be permitted within Planning Area 9:

Retail Related

- 1) Apparel stores
- 2) Appliance stores
- 3) Art galleries, frame shops
- 4) Automotive accessories
- 5) Baker sales shops
- 6) Bicycle shops
- 7) Book stores
- 8) Camera stores
- 9) Candy, nut and confectionery stores
- 10) Department stores
- 11) Drug stores
- 12) Florist shops
- 13) Gift stores
- 14) Grocery stores
- 15) Furniture stores
- 16) Hardware stores
- 17) Optical goods stores
- 18) Package liquor stores
- 19) Paint, glass and wallpaper stores
- 20) Pet shops (completely enclosed)
- 21) Radio, television and music sound system stores
- 22) Shoe shops
- 23) Specialty shops
- 24) Sporting goods stores
- 25) Stationary stores
- 26) Theaters, not including drive-ins
- 27) Toy stores
- 28) Variety stores

Service Related

- 1) Apparel repair, alteration, and cleaning
- 2) Barber and beauty shops
- 3) Banks and savings and loan institutions
- 4) Day care centers
- 5) TV stations
- 6) Instant printing and duplicating services
- 7) Shoe repair
- 8) Travel service

Food Related

- 1) Bars and cocktail lounges, provided such uses are a part of a bona fide restaurant
- 2) Delicatessens and dairy product sales
- 3) Restaurants and sidewalk cafes
- 4) Fast food and carry out restaurants with drive-thru's

B. Other Uses. Other uses as deemed appropriate by the Director of Planning and Community Development.

C. Accessory Uses. The following accessory building and uses customarily incident to any of the above shall be permitted within Planning Area 9:

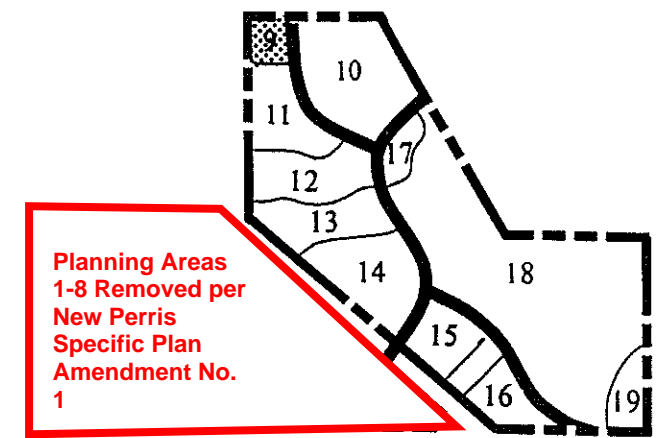
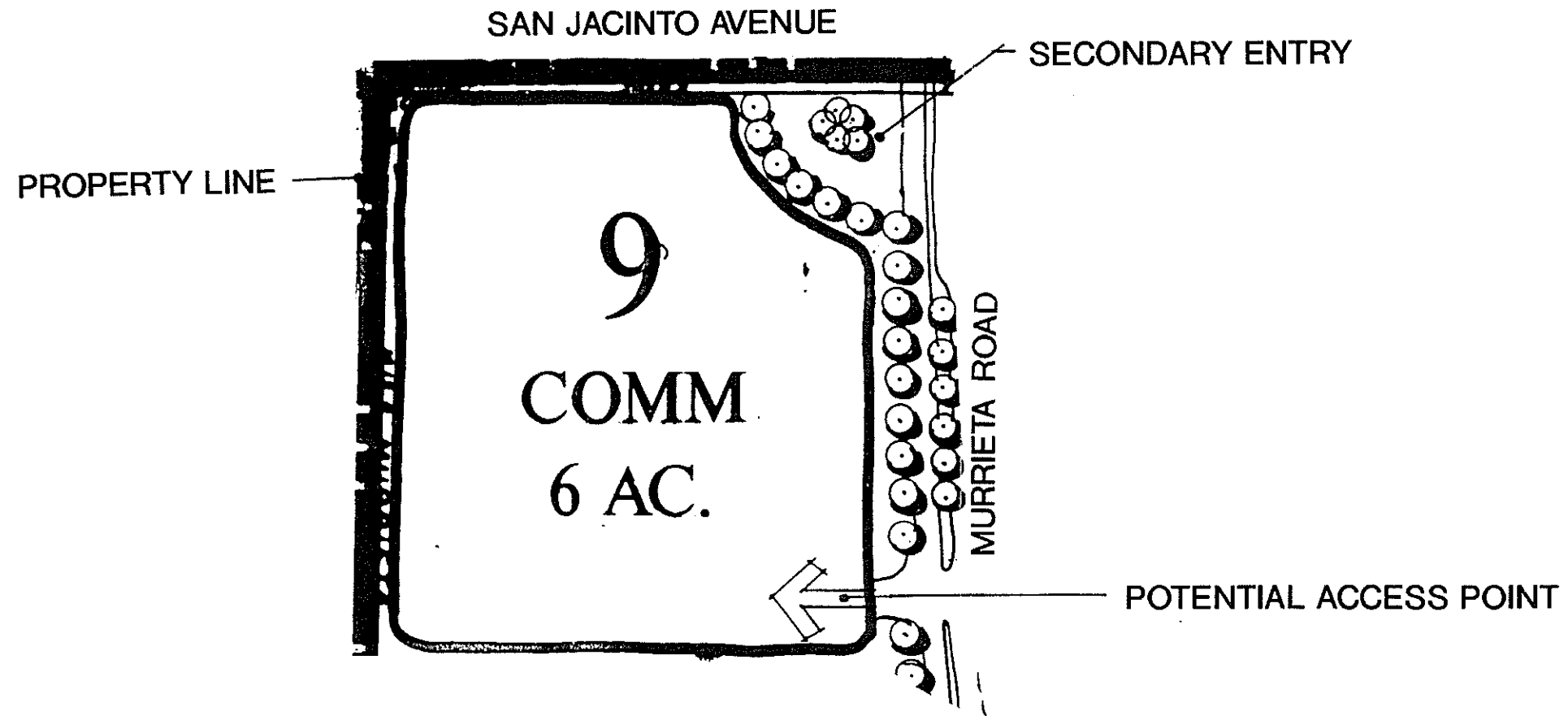
- 1) Private garages, parking structures, and/or subterranean parking
- 2) Accessory structures or uses which are customarily incidental or necessary to the permitted uses
- 3) Pedestrian and bicycle trails

D. Development Standards

- 1) **Maximum Structure Height:** Maximum height of buildings within Planning Area 9 shall be two (2) stories or 24 feet above grade.
- 2) **Maximum Building Coverage:** Maximum permissible building coverage for all structures within Planning Area 9 shall be 35 percent.
- 3) **Setbacks:**
 - a. **Streetside:** Building setbacks from public streets shall be a minimum of thirty-five (35) feet as measured from the face-of-curb, and fully landscaped with no building or structure encroachments permitted.
 - b. **Rear Yard:** Where the rear of a building is adjacent to a property zoned or has a land use other than manufacturing, industrial or commercial the rear yard setback shall be a minimum of fifty (50) feet, of which a minimum of 20 feet shall be landscaped adjacent to the property line. No building, structure or parking encroachments shall be permitted.
 - c. Where the adjoining property is similarly zoned, the rear set back shall be a minimum of eighteen (18) feet.
- 4) **Parking Requirements:**
 - a. The minimum number of parking spaces required shall be as identified within Section 5.2, Parking Regulations and Standards, of this Specific Plan.
 - b. All other requirements shall be as per the City of Perris parking codes.

- 5) Vehicular access to Planning Area 9 shall conform to the requirements of the City Engineer and Fire Department.
- 6) Proposed uses within Planning Area 9 shall be subject to development plan review by the City of Perris Planning and Community Development Department.

PLANNING AREA 9
COMMERCIAL
6 ACRES

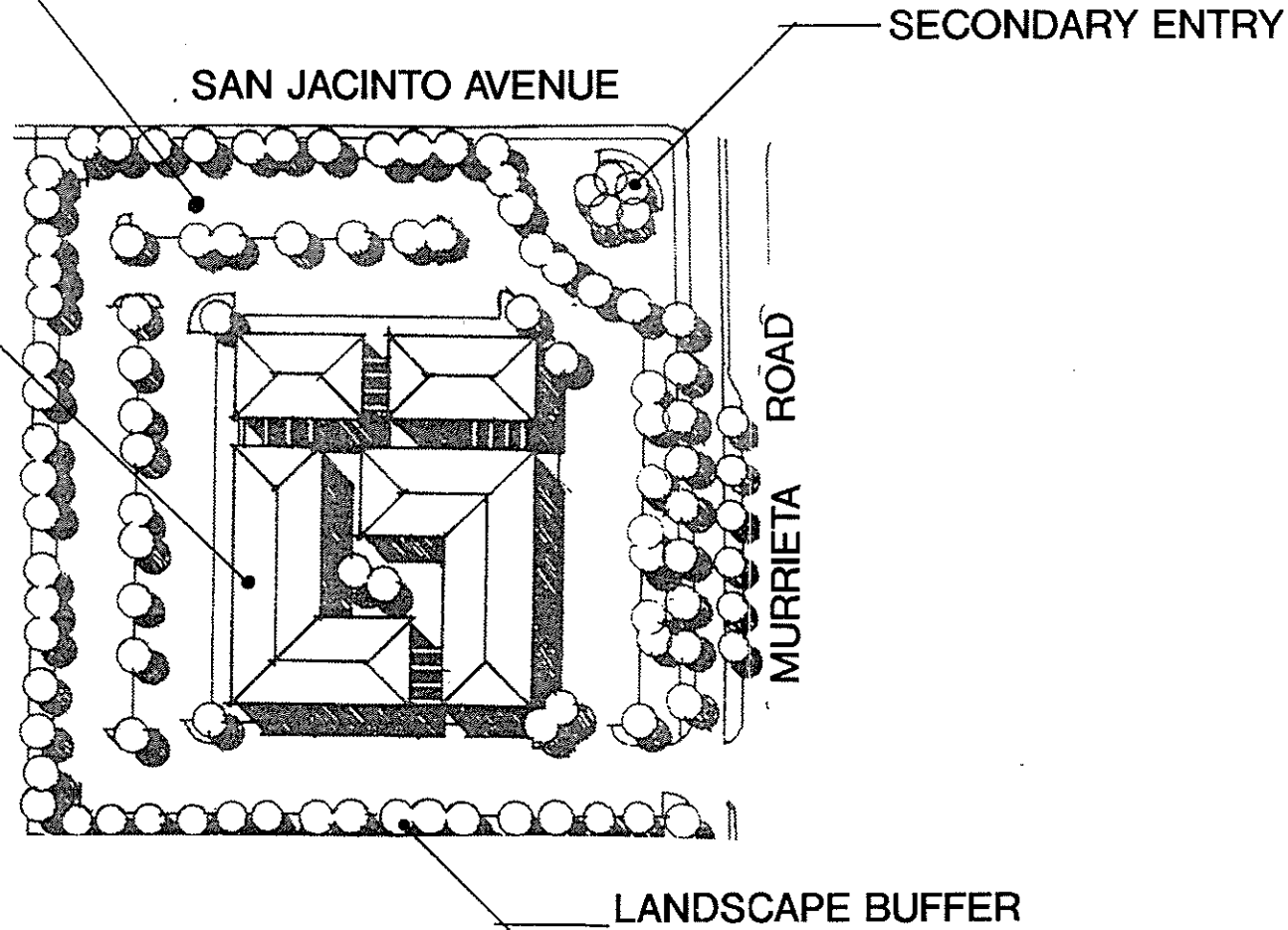


KEY MAP

FIGURE 25

OPEN LANDSCAPED
PARKING AREAS

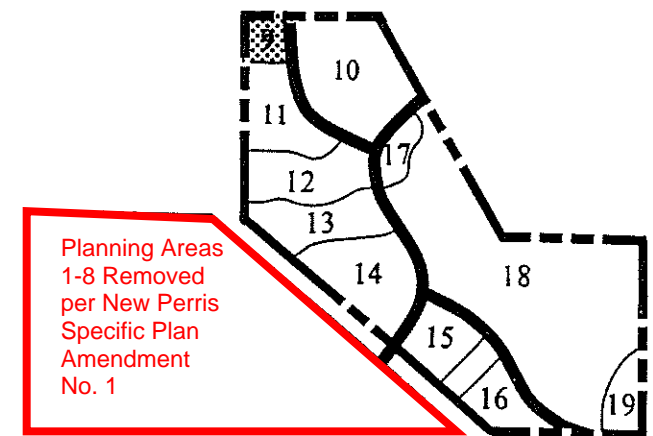
RETAIL COMMERCIAL
2 STORIES



PLANNING AREA 9

COMMERCIAL

6 ACRES



KEY MAP

FIGURE 26

4.2 Planning Area 10 – Research and Development

4.2.1 Land Use

Planning Area 10 is a 40 acre site that will consist of research and development land uses and located in the northern portion of the property, east of Murrieta Road and adjacent to San Jacinto Avenue and the Perris Valley Storm Drain channel as depicted in **Figure 27, Planning Area 10** and **Figure 28, Planning Area 10 Detail**.

4.2.2 Zoning Ordinance Regulations

A. Permitted Uses. The following general uses shall be permitted within Planning Area 10:

- 1) Accounting, auditing, and bookkeeping services
- 2) Administrative offices
- 3) Advertising services
- 4) Architectural, engineering, and planning services
- 5) Business and management consulting services
- 6) Data processing services
- 7) Dental labs
- 8) Duplicating, mailing, and stenographic services
- 9) Medical labs
- 10) Product development facilities
- 11) Product research facilities
- 12) Photographic service
- 13) Research and analysis services
- 14) Telephone answering

B. Other Uses. Other similar uses as deemed appropriate by the Director of Planning and Community development.

C. Accessory Uses. The following accessory building and uses customarily incident to any of the above shall be permitted within Planning Area 10:

- 1) Private garages, parking structures, and/or subterranean parking
- 2) Clubhouses
- 3) Accessory structures or uses which are customarily incidental or necessary to the permitted uses and bicycle trails
- 4) Auto-related products

D. Development Standards

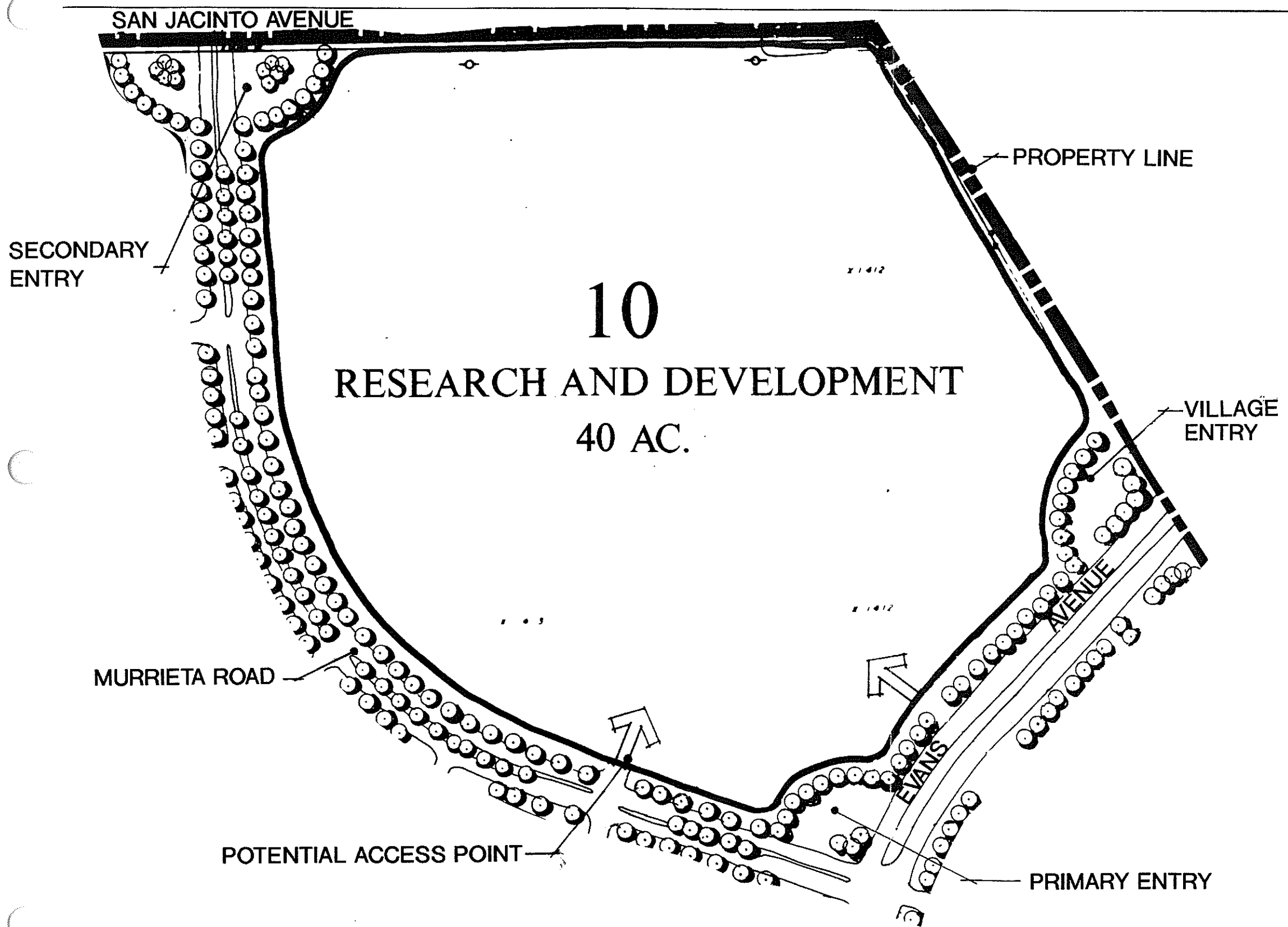
- 1) **Maximum Structure Height:** Maximum height for buildings within Planning Area 10 shall be two (2) stories or 24 feet above grade.
- 2) **Maximum Building Coverage:** Maximum permissible building coverage for all structures within Planning Area 10 shall be 35 percent.
- 3) **Setbacks:**
 - a. **Streetside:** Building setbacks from public streets shall be a minimum of thirty-five (35) feet as measured from the face-of-curb,

and fully landscaped with no building or structure encroachments permitted.

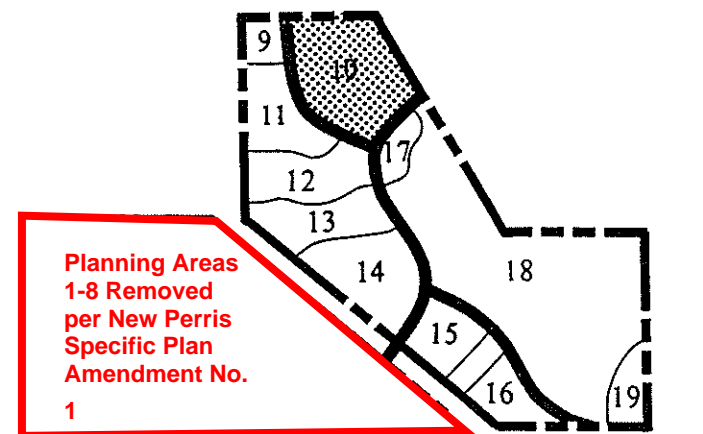
- b. Adjacent to the Perris Valley Storm Channel, the setback shall be 50 feet, of which a minimum of 20 feet shall be fully landscaped adjacent the channel. No building, structure or parking encroachments shall be permitted.

4) **Parking Requirements:**

- a. The minimum number of parking spaces required shall be as identified within Section 5.2, Parking Regulations and Standards, of this Specific Plan.
 - b. All other requirements shall be as per the City of Perris parking codes.
- 5) Vehicular access to Planning Area 10 shall conform to the requirements of the City Engineer and Fire Department.
 - 6) Proposed uses within Planning Area 10 shall be subject to development plan review by the City of Perris Planning and Community Development Department.



PLANNING AREA 10
RESEARCH AND DEVELOPMENT
40 AC



KEY MAP **FIGURE 27**

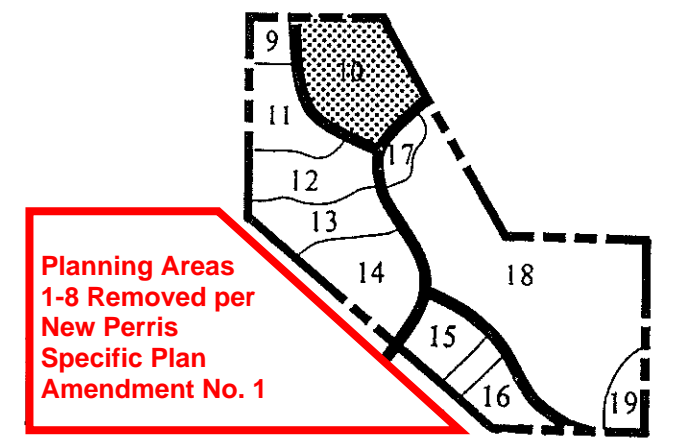
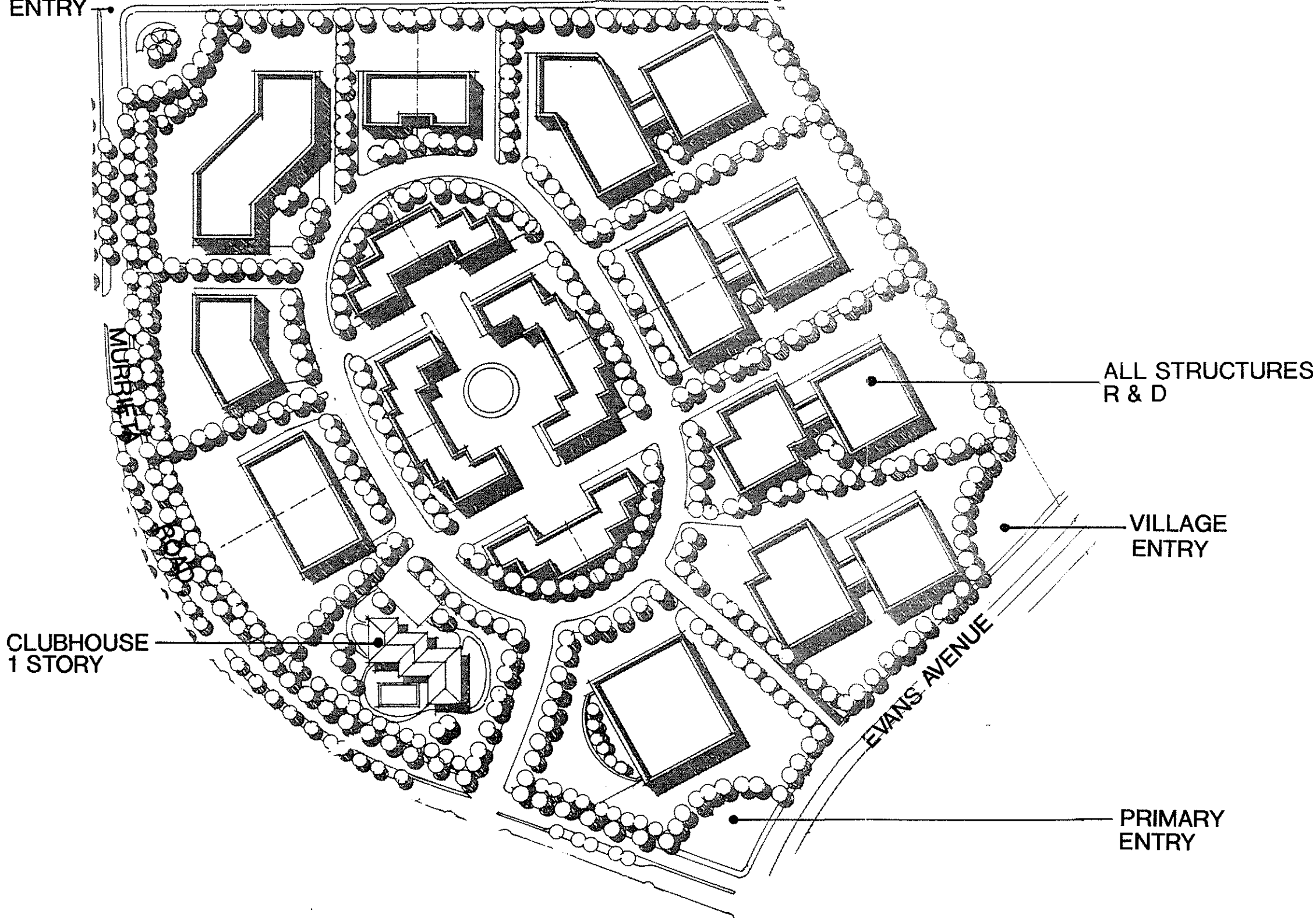


SECONDARY
ENTRY

SAN JACINTO AVENUE

PLANNING AREA 10

RESEARCH AND DEVELOPMENT 40 AC



KEY MAP
FIGURE 28



FOR GRAPHIC REPRESENTATION ONLY, NOT FOR ADOPTION PURPOSES

4.3 Planning Area 11 – Office/Commercial

4.3.1 Land Use

Planning Area 11 is a 24-acre site that will consist of office/commercial land uses and located in the northern portion of the property, west of Murrieta Road and adjacent to Evans Road on the east. Planning Area 11 will consist of office/commercial land uses intended to provide goods and services to the community as depicted in **Figure 29, Planning Area 11** and **Figure 30, Planning Area 11 Detail**.

4.3.2 Zoning Ordinance Regulations

A. Permitted Uses. The following general uses shall be permitted within Planning Area 11:

Office Related

- 1) Accounting, auditing, and bookkeeping services
- 2) Administrative offices
- 3) Advertising services
- 4) Architectural, engineering, and planning services
- 5) Business and management consulting services
- 6) Collection agencies
- 7) Credit Services
- 8) Data processing services
- 9) Dental clinic, labs, and offices
- 10) Detective and protective services
- 11) Duplicating, mailing, and stenographic services
- 12) Employment services
- 13) Financial institution
- 14) Insurance services
- 15) Investment services
- 16) Medical clinics, labs, and offices
- 17) Optometrist
- 18) Pharmacy
- 19) Photographic services
- 20) Public Utility offices
- 21) Real estate services
- 22) Research services
- 23) Security and commodity brokers, dealers, and exchanges
- 24) Tax consultation
- 25) Telephone answering

Retail Related

- 1) Apparel stores
- 2) Appliance stores
- 3) Art galleries, frame shop
- 4) Automotive accessories
- 5) Baker sales shops
- 6) Bicycle shops
- 7) Book stores

- 8) Camera stores
- 9) Candy, nut, and confectionary stores
- 10) Department stores
- 11) Drug stores
- 12) Florist shops
- 13) Gift stores
- 14) Grocery stores
- 15) Furniture stores
- 16) Hardware stores
- 17) Optical goods stores
- 18) Package liquor stores
- 19) Paint, glass and wallpaper stores
- 20) Pet shops (completely enclosed)
- 21) Radio, television and music sound system stores
- 22) Shoe shops
- 23) Specialty shops
- 24) Sporting goods stores
- 25) Stationery stores
- 26) Theaters, not including drive-ins
- 27) Toy stores
- 28) Variety stores

Service Related

- 1) Apparel repair, alteration, and cleaning
- 2) Barber and beauty shops
- 3) Banks and savings and loan institutions
- 4) Day care centers
- 5) TV stations
- 6) Instant printing and duplicating services
- 7) Public service facilities (government, civic and utility)
- 8) Shoe repair
- 9) Travel service

Food Related

- 1) Bars and cocktail lounges, provided such uses are a part of a bona fide restaurant
- 2) Delicatessens and dairy product sales
- 3) Restaurants and sidewalk cafes
- 4) Fast food and carry out restaurants with drive-thru's

Recreation Related

- 1) Health and athletic clubs
- 2) Recreational uses such as tennis courts, swimming pools, bowling alleys, ice and roller skating rinks, racquetball courts
- 3) Specialty recreation centers, such as family billiard facilities and other recreation uses conducted within completely enclosed buildings

Cultural Related

- 1) Amphitheater
- 2) Art cinema
- 3) Art museum
- 4) Concert halls

Auto Service Center (including car wash, gasoline, service pumps, lube oil and tune-up, and other similar minor automobile repairs) provided that:

- 1) The service center shall be setback at least 100 feet from the right-of-way lines
- 2) No more than two service centers shall be permitted in Planning Area 11
- 3) The proposed architecture and site location shall enhance the appearance and character
- 4) Feasibility of arranging ingress and egress without interference or hazard to circulation patterns
- 5) Car wash and service repair activities and operations shall be completely and attractively screened
- 6) Nuisances due to noise or other features associated with the activities and operation shall be sufficiently mitigated through physical location, design, or other measures

B. Other Uses. Other uses as deemed appropriate by the Director of Planning and Community Development.

C. Accessory Uses. The following accessory building and uses customarily incident to any of the above shall be permitted within Planning Area 11:

- 1) Private garages, parking structures, and/or subterranean parking
- 2) Accessory structures or uses which are customarily incidental or necessary to the permitted uses
- 3) Pedestrian and bicycle trails
- 4) Water features

D. Development Standards

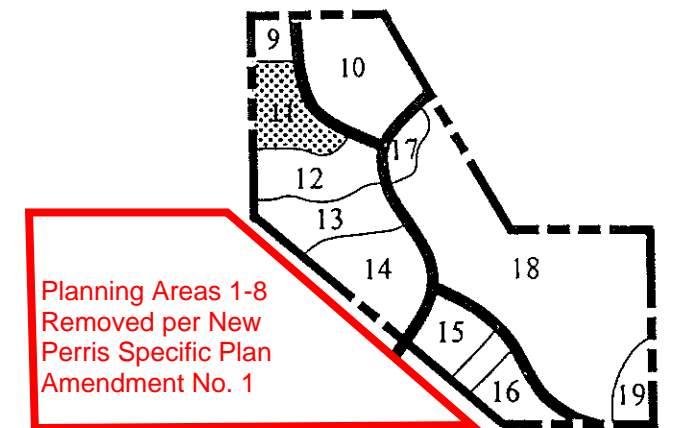
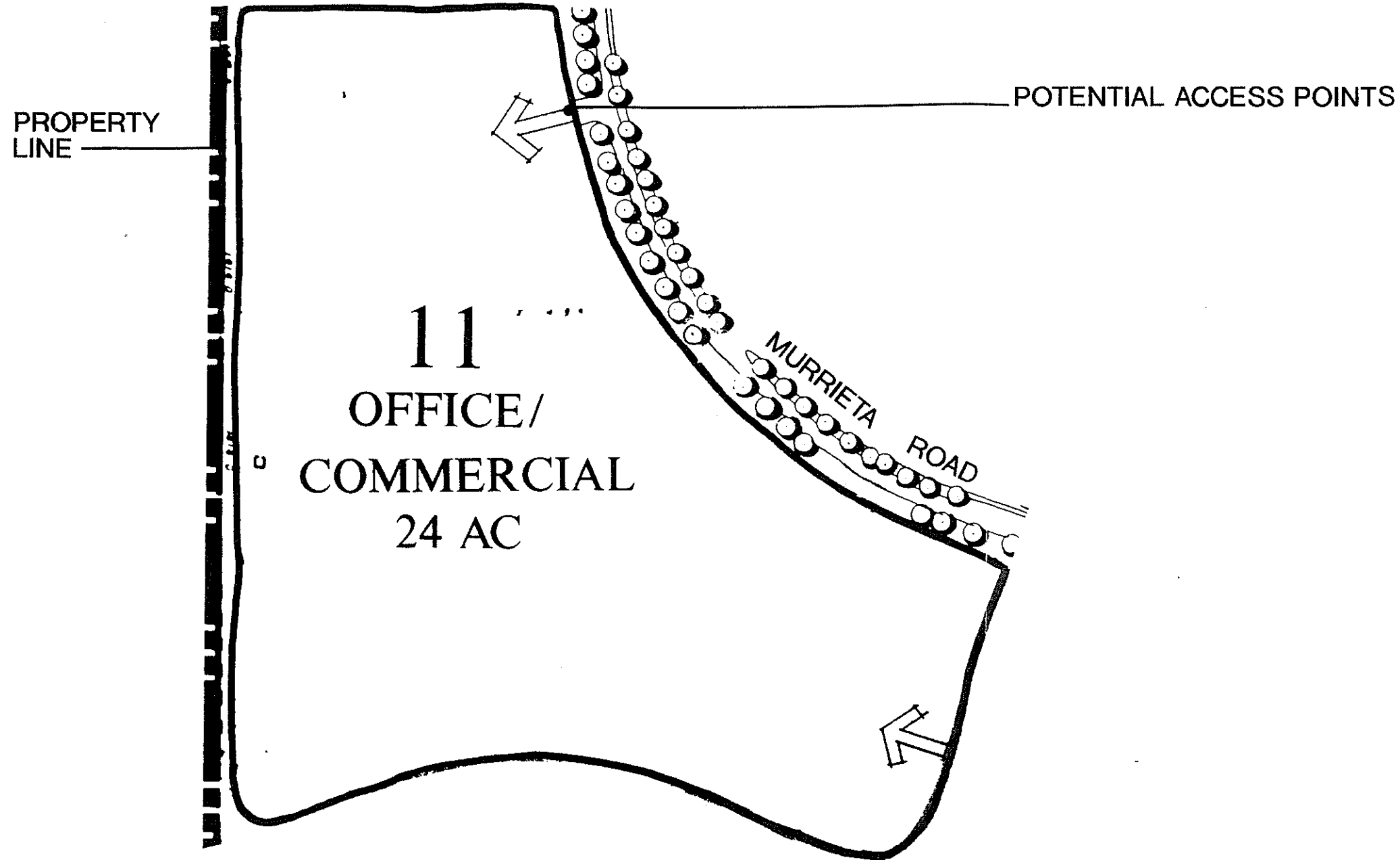
- 1) **Maximum Structure Height:** Maximum height for buildings within Planning Area 11 shall be two (2) stories or 24 feet above grade
- 2) **Maximum Building Coverage:** Maximum permissible building coverage for all structures within Planning Area 11 shall be 35 percent.
- 3) **Setbacks**
 - a. **Streetside:** Building setbacks from public streets shall be a minimum of thirty-five (35) feet as measured from the face-of-curb, and fully landscaped with no building or structure encroachments permitted.
 - b. When the side property line adjoins a property zoned, or land use other than manufacturing, industrial or commercial, the buildings shall be setback a minimum of fifty (50) feet, of which a minimum of twenty (20) feet shall be landscaped adjacent to the property line.
 - c. **Side and Rear Building Setback:** A minimum side and rear building setback shall be not less than ten (10) feet in depth as

measured from the property line or face-of-curb, except that open parking areas and parking structures may be located at the side and the rear property line provided the parking structures are constructed with adequate fire walls as defined by the latest edition of the Uniform Building Code adopted by the City. Setbacks shall be fully landscaped.

4) **Parking Requirements:**

- a. The minimum number of parking spaces required shall be as identified within Section 5.2, Parking Regulations and Standards, of this Specific Plan.
 - b. All other requirements shall be as per the City of Perris parking codes.
- 5) Vehicular access to Planning Area 11 shall conform to the requirements of the City Engineer and Fire Department.
- 6) Proposed uses within Planning Area 11 shall be subject to development plan review by the City of Perris Planning and Community Development Department.

PLANNING AREA 11
OFFICE/COMMERCIAL
24 AC



KEY MAP

FIGURE 29

PLANNING AREA 11
OFFICE/COMMERCIAL
24 ACRES

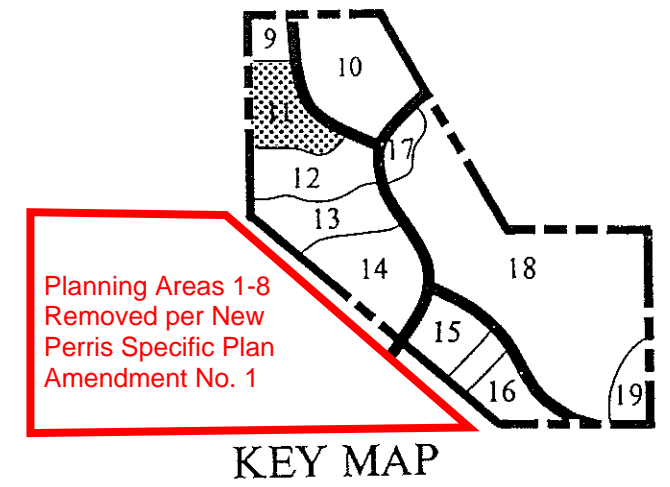
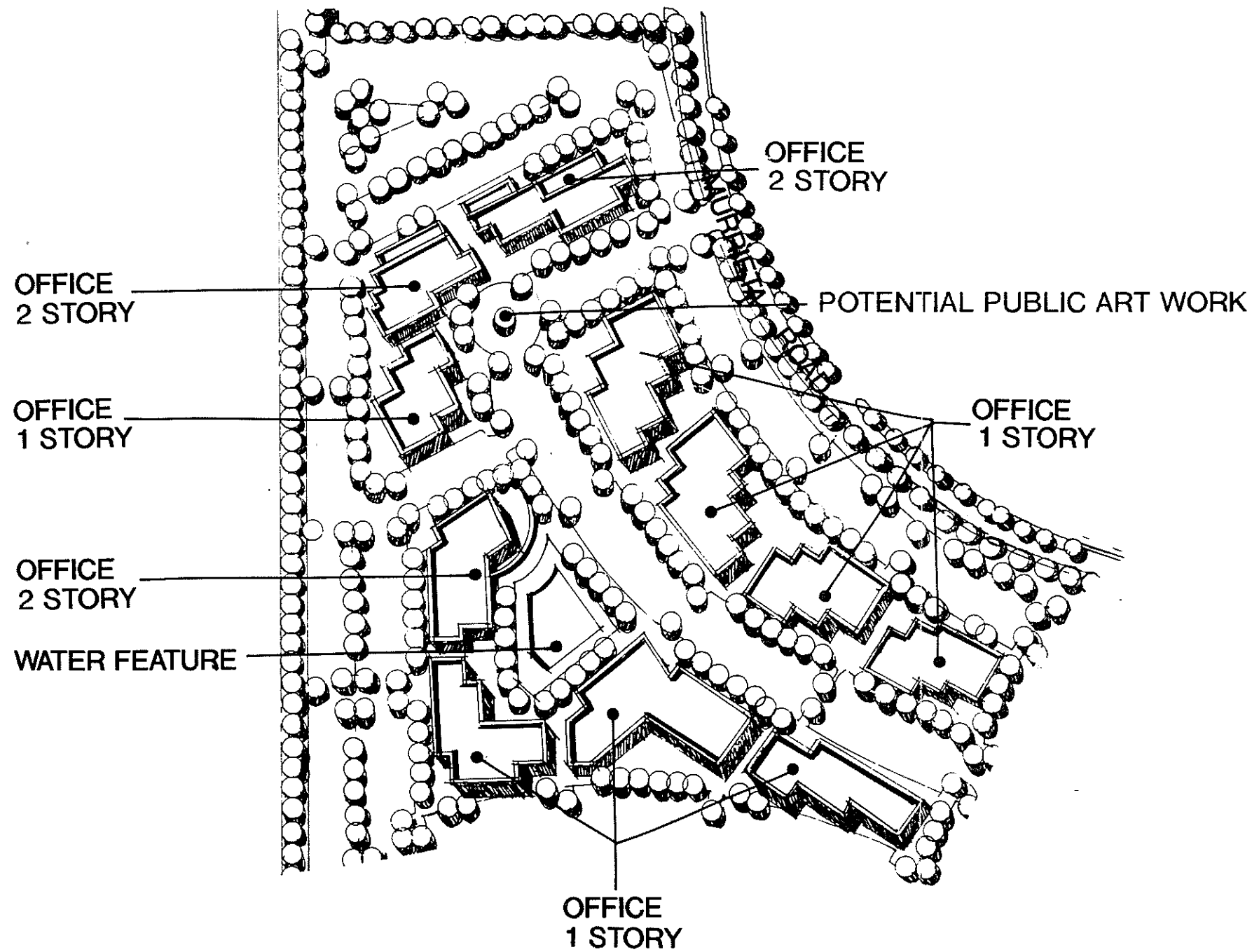


FIGURE 30

4.4 Planning Area 12 – Single-Family Detached Residential

4.4.1 Land Use

Planning Area 12 is a 25-acre site located in the central portion of the project site bordered by Evans Avenue to the east and Murrieta Road to the north. Planning Area 12 will consist of single-family detached residential land uses on minimum 7,200 square foot lots as depicted in **Figure 31, Planning Area 12** and **Figure 32, Planning Area 12 Detail**.

4.4.2 Zoning Ordinance Regulation

A. Permitted Uses. The following general uses shall be permitted within Planning Area 12:

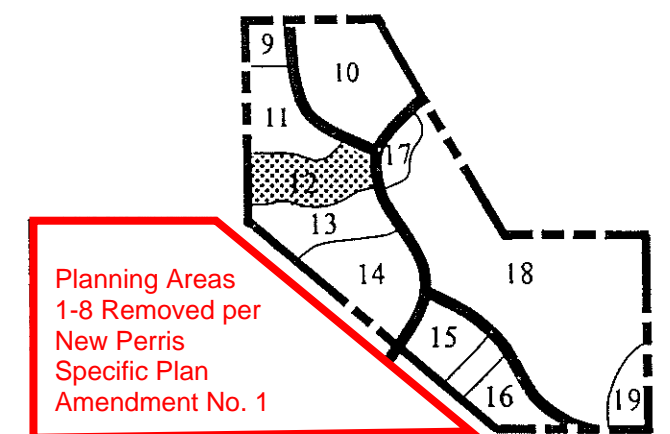
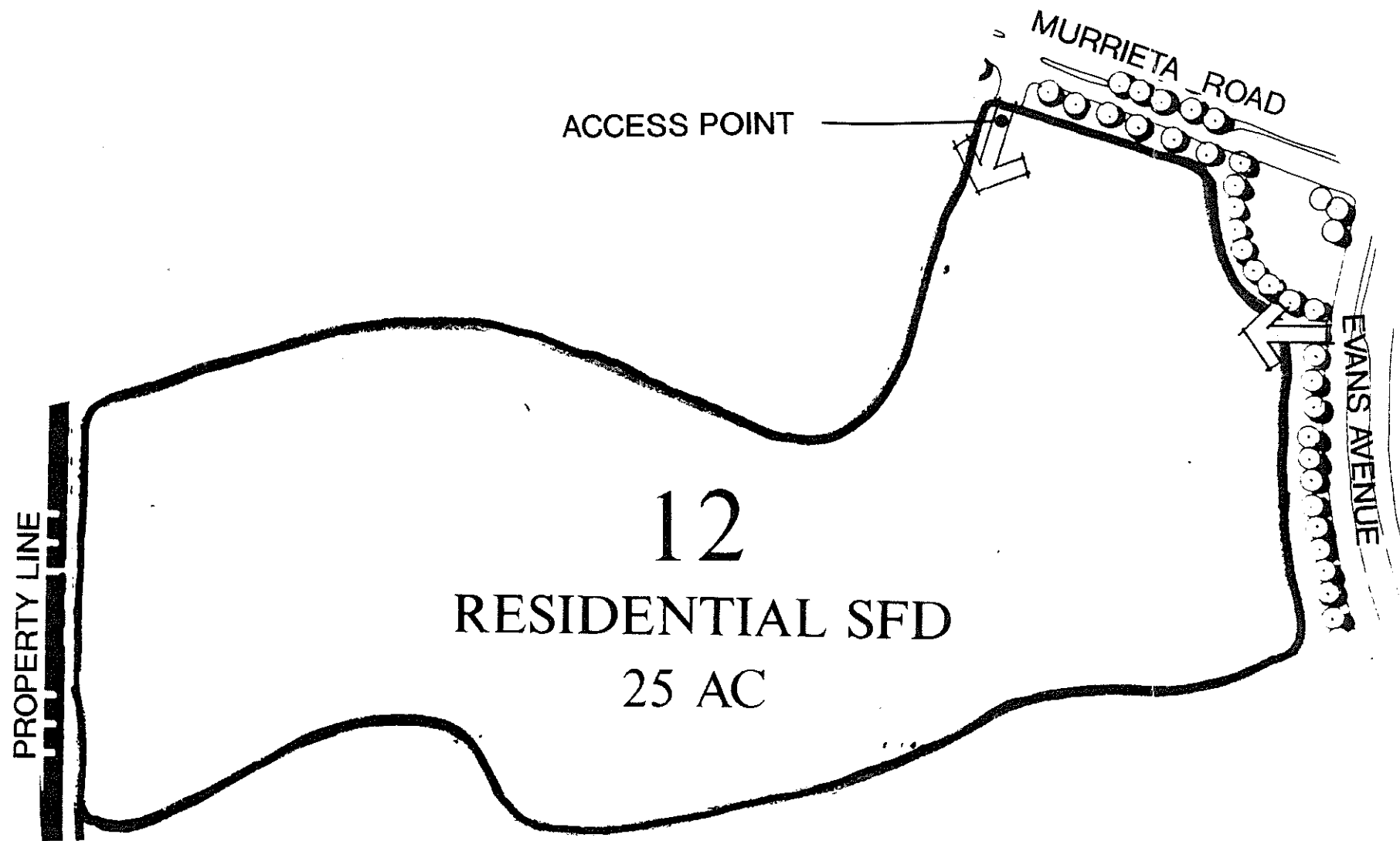
- 1) Single-Family dwellings of a permanent character, in a permanent location, and used by only one family

B. Accessory Uses

- 1) A private garage for the accommodation of not less than two automobiles;
- 2) Arbors, trellises, gazebos, and similar shade structures of open construction
- 3) Fences and walls
- 4) Patio covers
- 5) Swimming pools and spas
- 6) Pedestrian and bicycle trails
- 7) Tennis and racquet courts
- 8) Home occupations, including any vocation, trade professions, or childcare operation carried on solely by the occupant of the premise shall be permitted, provided that:
 - a. There is no alteration in the residential character of the premises in connection with such vocation or trade
 - b. All operations are carried on within the dwelling
 - c. Not more than 25 percent of the ground floor area of the dwelling shall be occupied or used
 - d. No motor other than electrically driven motors shall be used in connection there with
 - e. No merchandise or articles shall be displayed
 - f. No merchandise or articles pertaining to such home occupation shall be stored other than in the dwelling
 - g. No assistants shall be employed in connection therewith
 - h. The premises shall not be used as a place of business or point of sale of the products of such home occupation
 - i. All operations in connection with such home occupation shall be conducted so as to prevent the emanation of any dust, gas, smoke, noise, fumes, odors, vibrations, or electrical disturbances, which are or may be detrimental to the interests or welfare of the occupants of surrounding properties.
- 9) Neighborhood parks
- 10) Any other similar uses as deemed appropriate by the City of Perris Director of Planning and Community Development

- C. Temporary Uses.** The following temporary uses shall be permitted:
- 1) Temporary construction facility during construction
 - 2) Temporary real estate offices and model homes located within a subdivision, to be used only for and during the original sale of the subdivision
- D. Development Standards**
- 1) **Maximum Structure Height:** Maximum height for buildings within Planning Area 12 shall be two stories or 35 feet above grade.
 - 2) **Minimum Lot Size:** The minimum lot size within Planning Area 12 shall be not less than 7,200 square feet.
 - 3) **Maximum Building Coverage:** Maximum permissible building coverage for all structures within Planning Area 12 shall be 50 percent.
 - 4) **Setbacks:**
 - a. **Front yard:** Building setbacks for Planning Area 12 shall be a minimum of 25 feet as measured from the face-of-curb; garages shall be setback a minimum of 20 feet as measured from the face-of-curb.
 - b. **Rear yard:** Building setbacks for Planning Area 12 rear yards shall be a minimum 25 feet. Patio covers may extend to within 10 feet of the rear property line.
 - c. **Side yard:** Building setbacks for Planning Area 12 for side yards shall be a minimum of 5 feet from the nearest property line on interior and through lots. For side yards on corner and reverse corner lots shall be a minimum of 10 feet, excluding architectural projections of 2 feet maximum.
 - d. **Building to Building:** Minimum distance between buildings in Planning Area 12 shall be 10 feet, excluding architectural projections of 2 feet maximum, such as fireplace boxes and bay windows.
 - 5) **Minimum Lot Width:** The minimum width of a lot used as a building site area in Planning Area 12 shall be 60 feet.
 - 6) Landscaping shall buffer surrounding uses to enhance view impacts.
 - 7) Proposed uses within Planning Area 12 shall be subject to development plan review by the City of Perris Planning and Community Development Department and the Fire Department.
- E. Density Regulations**
- 1) The density within Planning Area 12 shall not exceed 3.2 dwelling units per gross acres.

PLANNING AREA 12
RESIDENTIAL SFD
25 AC

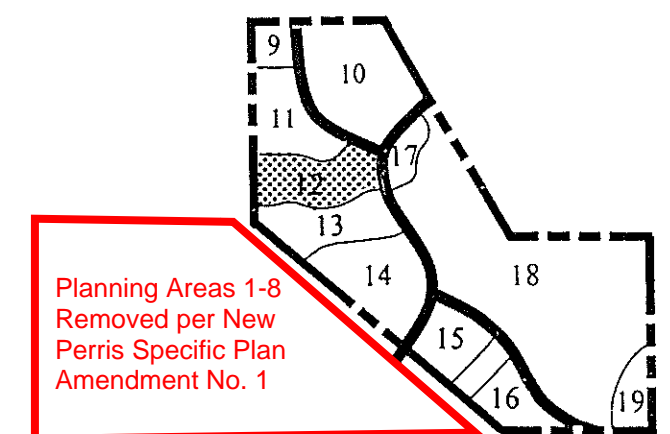
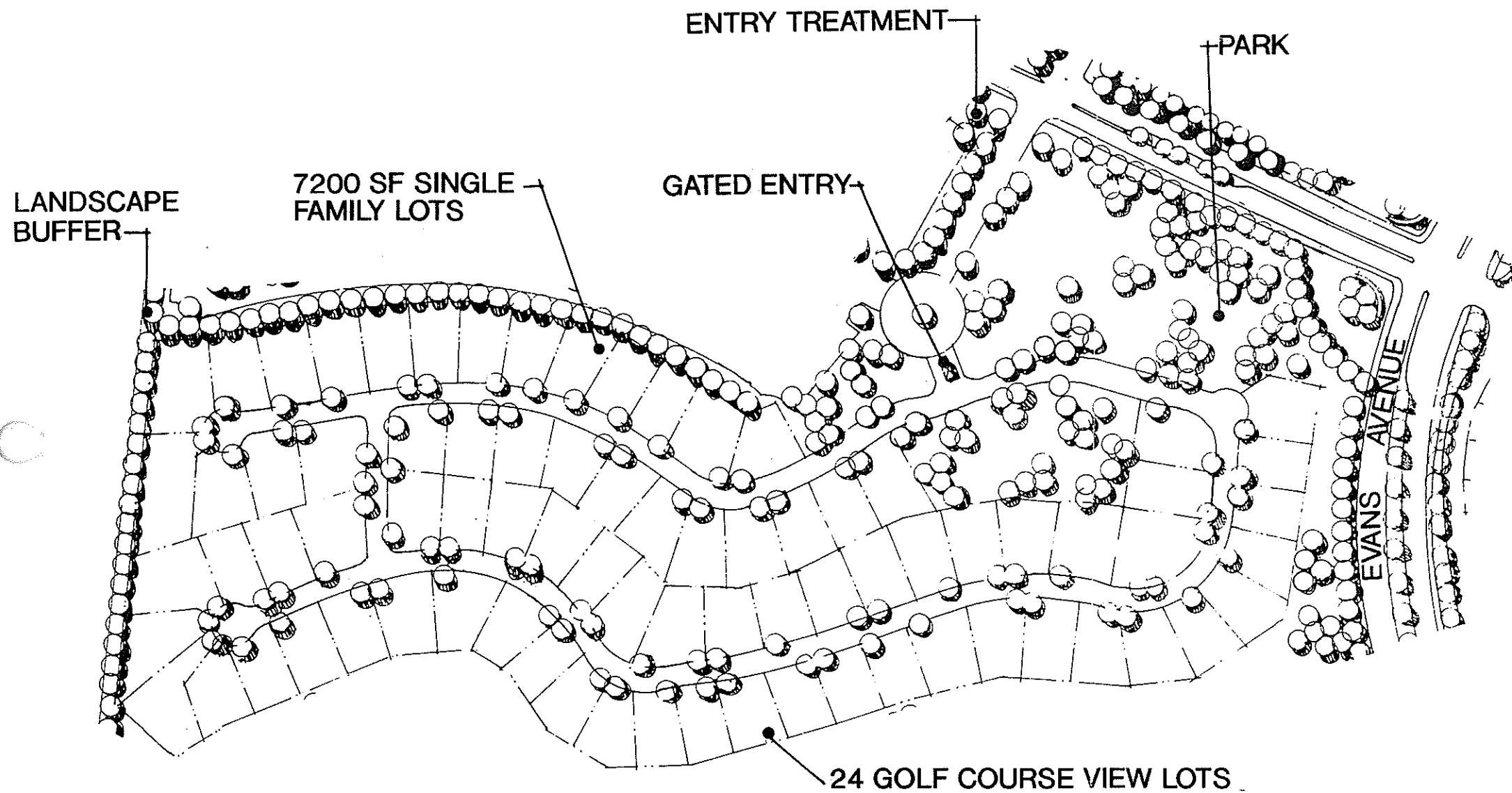


KEY MAP

FIGURE 31

PLANNING AREA 12

RESIDENTIAL SFD
25 AC
79 UNITS
3.2 DU/AC



Planning Areas 1-8
Removed per New
Perris Specific Plan
Amendment No. 1

KEY MAP

FIGURE 32

4.5 Planning Area 13 – Golf Course

4.5.1 Land Use

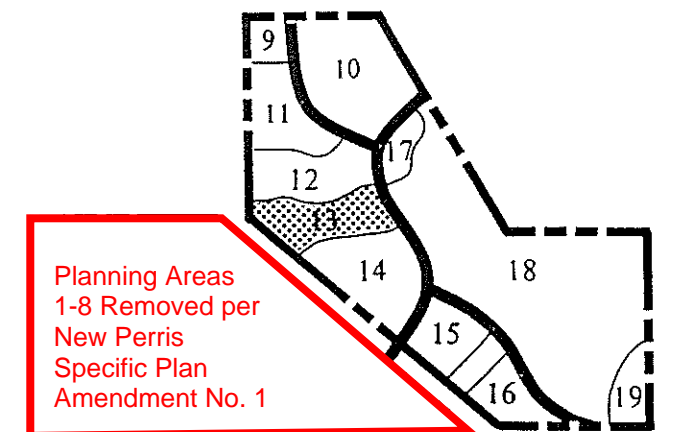
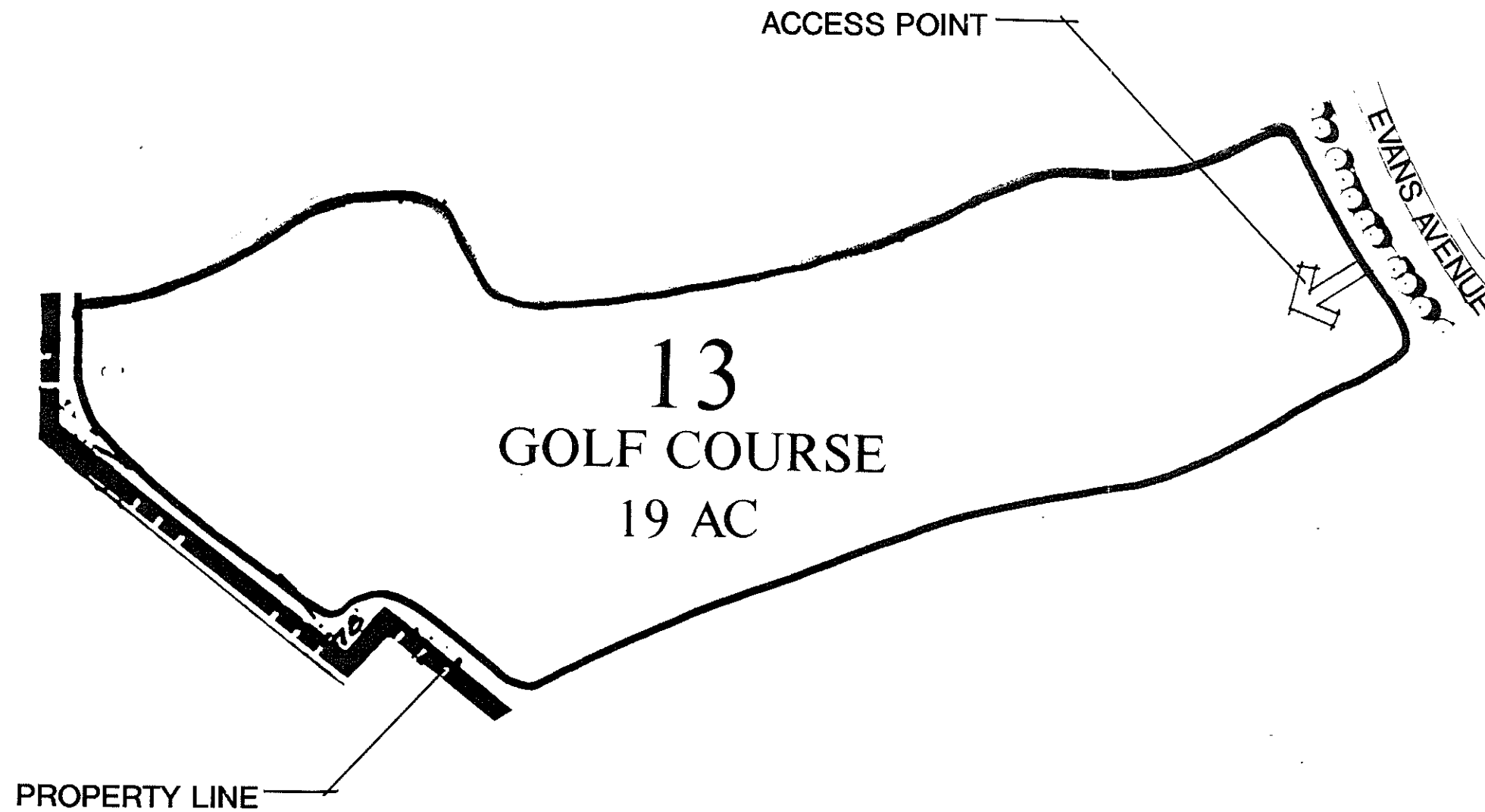
Planning Area 13 is a 19-acre located in the central portion of the project site bordered by Evans Road to the east and I-215 to the west. Planning Area 13 will consist of recreational open space uses, which includes a portion the golf course proposed within the project site as depicted in **Figure 33, Planning Area 13** and **Figure 34, Planning Area 13 Detail**.

4.5.2 Zoning Ordinance Regulations

- A. Permitted Uses.** The following general uses shall be permitted within Planning Area 13:
- 1) Golf course and any other use related to a golf course operation
 - 2) Lakes and ponds
 - 3) Detention and retention basin
- B. Accessory Uses.** The following accessory building and uses customarily incident to any of the above shall be permitted within Planning Area 13:
- 1) Accessory buildings including but limited to guard houses, trash enclosures, storage buildings, and maintenance build-up
 - 2) Pedestrian and bicycle trails
- C. Temporary Uses.** The following uses shall be permitted:
- 1) Parking for special events
 - 2) Temporary construction facilities during construction
 - 3) Fairs and/or carnivals
 - 4) Parking only during a special event
- D. Development Standards**
- 1) **Maximum structure height:** Maximum height for buildings within Planning Area 13 shall be one (1) story or 25 feet above grade.
 - 2) **Maximum Building Coverage:** Maximum permissible building coverage for all structures within Planning Area 13 shall be 20 percent.
 - 3) **Setbacks:**
 - a. **Streetside:** Building setbacks shall be a minimum of thirty-five (35) feet as measured from the face-of-curb, and fully landscaped with no building or structure encroachments permitted.
 - 4) Planning Area 13 shall have an open space nature.
 - 5) Landscaping shall buffer surrounding uses to enhance view impacts and preserve the open space nature.
 - 6) Proposed uses within Planning Area 13 shall be subject to development plan review by the City of Perris Planning and Community Development Department and Fire Department.

PLANNING AREA 13

GOLF COURSE
19 AC

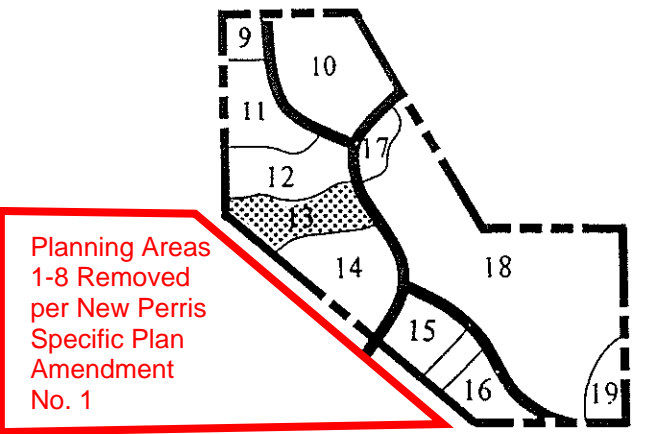
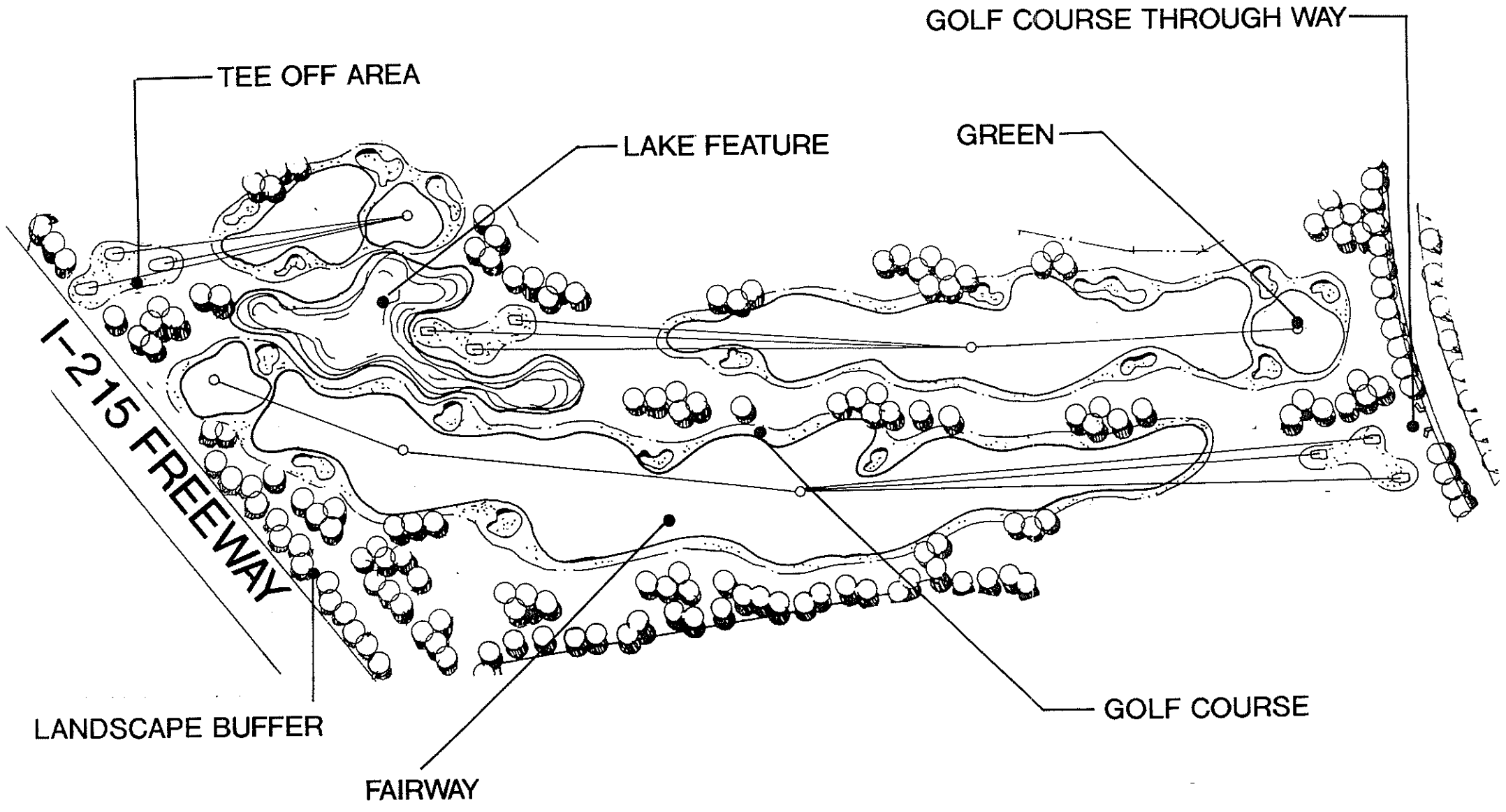


KEY MAP

FIGURE 33

PLANNING AREA 13

GOLF COURSE 19 AC



Planning Areas
1-8 Removed
per New Perris
Specific Plan
Amendment
No. 1

KEY MAP

FIGURE 34

4.6 Planning Area 14 – Office/Commercial

4.6.1 Land Use

Planning Area 14 is a 31-acre site that will be comprised of office/commercial land uses. Planning Area 14 is located adjacent to Ellis Avenue and Evans Avenue on the easterly portion of the project site as depicted in **Figure 35, Planning Area 14** and **Figure 36, Planning Area 14 Detail**.

4.6.2 Zoning Ordinance Regulations

A. Permitted Uses. The following general uses shall be permitted within Planning Area 14:

Office Related

- 1) Accounting, auditing, and bookkeeping services
- 2) Administrative offices
- 3) Advertising services
- 4) Architectural, engineering, and planning services
- 5) Business and management consulting services
- 6) Collection agencies
- 7) Credit services
- 8) Data processing services
- 9) Dental clinic, labs, and offices
- 10) Detective and protective services
- 11) Duplicating, mailing, and stenographic services
- 12) Employment services
- 13) Financial institutions
- 14) Insurance services
- 15) Investment services
- 16) Medical clinics, labs, and offices
- 17) Optometrist
- 18) Pharmacy
- 19) Photographic services
- 20) Public Utility offices
- 21) Real estate services
- 22) Research services
- 23) Security and commodity brokers, dealers, and exchanges
- 24) Tax consultation
- 25) Telephone answering

Retail Related

- 1) Apparel stores
- 2) Appliance stores
- 3) Art galleries, frame shops
- 4) Automotive accessories
- 5) Baker sales shops
- 6) Bicycle shops
- 7) Book stores
- 8) Camera stores
- 9) Candy, nut, and confectionery stores

- 10) Department stores
- 11) Drug stores
- 12) Florist shops
- 13) Gift stores
- 14) Grocery stores
- 15) Furniture stores
- 16) Hardware stores
- 17) Optical goods stores
- 18) Package liquor stores
- 19) Paint, glass and wallpaper stores
- 20) Pet shops (completely enclosed)
- 21) Radio, television and music sound system stores
- 22) Shoe shops
- 23) Specialty shops
- 24) Sporting goods stores
- 25) Stationery stores
- 26) Theaters, not including drive-ins
- 27) Toy stores
- 28) Variety stores

Service Related

- 1) Apparel repair, alteration, and cleaning
- 2) Barber and beauty shops
- 3) Banks and savings and loan institutions
- 4) Day care centers
- 5) TV stations
- 6) Instant printing and duplicating services
- 7) Public service facilities (government, civic, and utility)
- 8) Shoe repair
- 9) Travel service

Food Related

- 1) Bars and cocktail lounges, provided such uses are a part of a bona fide restaurant
- 2) Delicatessens and dairy product sales
- 3) Restaurants and sidewalk cafes
- 4) Fast food and carry-out restaurants with drive-thrus

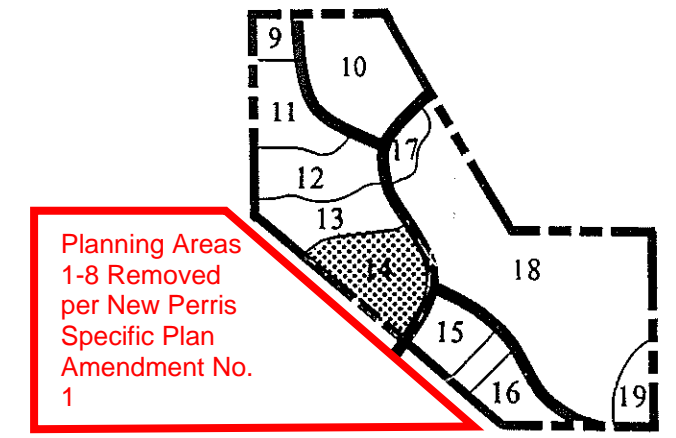
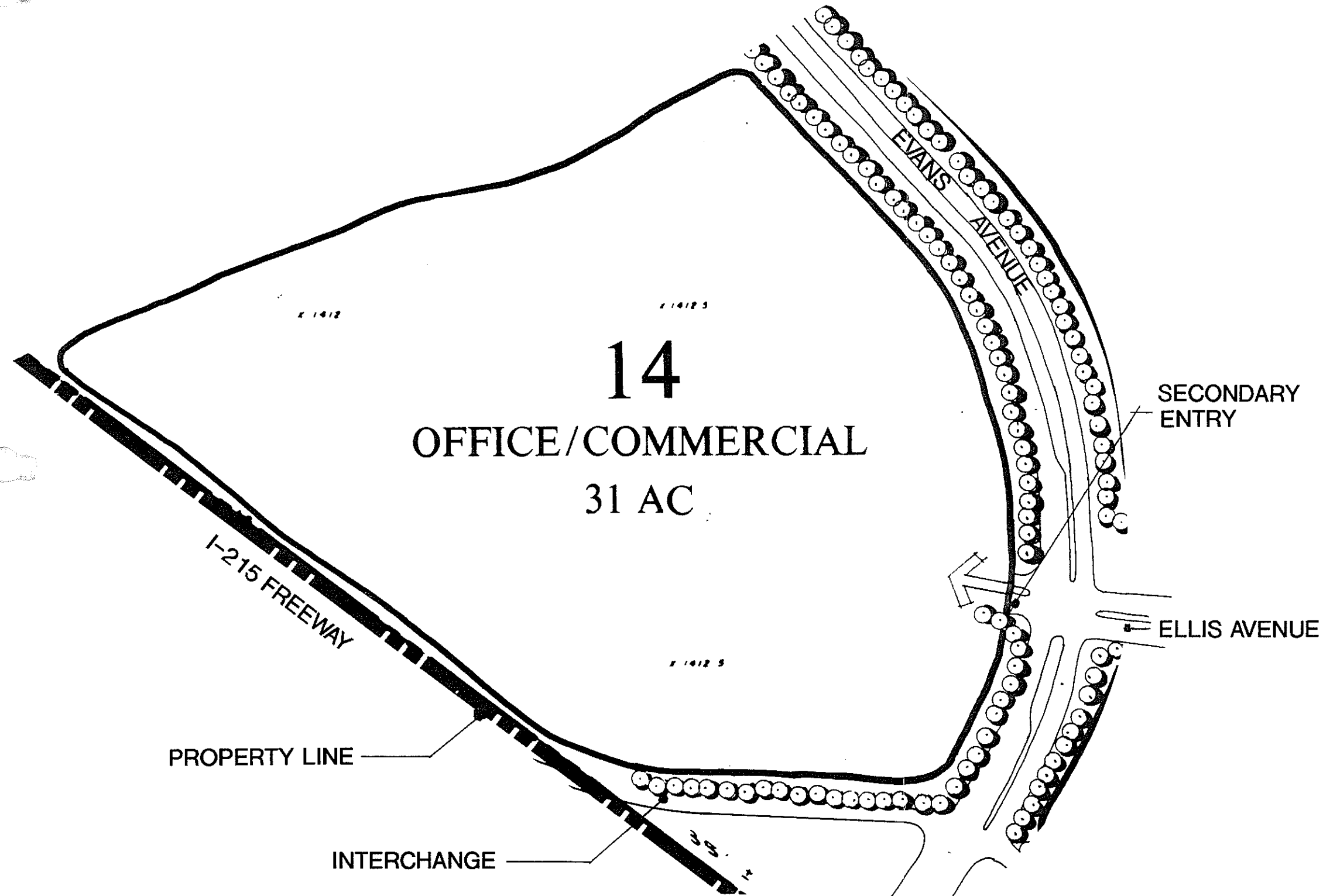
Recreation Related

- 1) Health and athletic clubs
- 2) Recreational uses such as tennis courts, swimming pools, bowling alleys, ice and roller skating rinks, racquetball courts
- 3) Specialty recreation centers, such as family billiard facilities and other recreation uses conducted within completely enclosed buildings

- B. Other Uses.** Other uses as deemed appropriate by the Director of Planning and Community Development.

- C. Accessory Uses.** The following accessory building and uses customarily incident to any of the above shall be permitted within Planning Area 14:
- 1) Private garages, parking structures, and/or subterranean parking
 - 2) Accessory structures or uses which are customarily incidental or necessary to the permitted uses
 - 3) Pedestrian and bicycle trails
- D. Development Standards**
- 1) **Maximum Structure Height:** Maximum height for buildings within Planning Area 14 shall be seven (7) stories or 84 feet above grade.
 - 2) **Maximum Building Coverage:** Maximum permissible building coverage for all structures within Planning Area 14 shall be 50 percent if parking structures are built and shall be 40 percent if no parking structures are built.
 - 3) **Setbacks:**
 - a. **Streetside:** Building setbacks from public streets shall be a minimum of thirty-five (35) feet as measured from the face-of-curb, and fully landscaped with no building or structure encroachments permitted.
 - b. **I-215:** Building setbacks from I-215 shall be a minimum of 50 feet measured from the right-of-way line. Of the total building setback, a minimum of 20 feet shall be fully landscaped.
 - 4) **Parking Requirements:**
 - a. The minimum number of parking spaces required shall be as identified within Section 5.2, Parking Regulations and Standards, of this Specific Plan.
 - b. All other requirements shall be as per the City of Perris parking codes.
 - 5) Vehicular access to Planning Area 14 shall conform to the requirements of the City Engineer and the Fire Department
 - 6) Proposed uses within Planning Area 14 shall be subject to development plan review by the City of Perris Planning and Community Development Department.

PLANNING AREA 14
OFFICE/ COMMERCIAL
31 AC

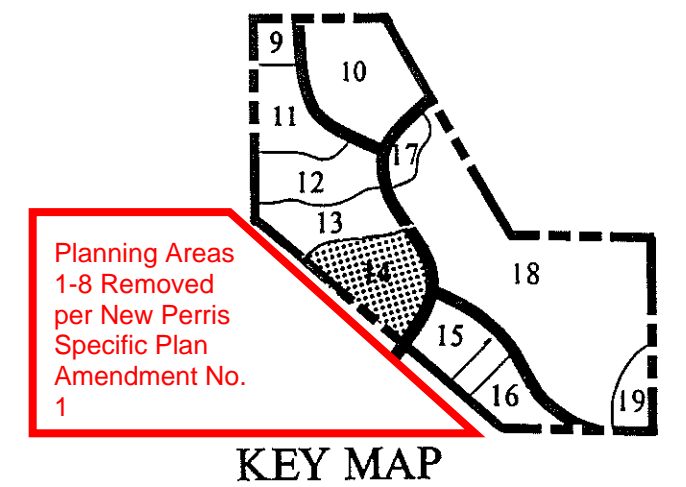
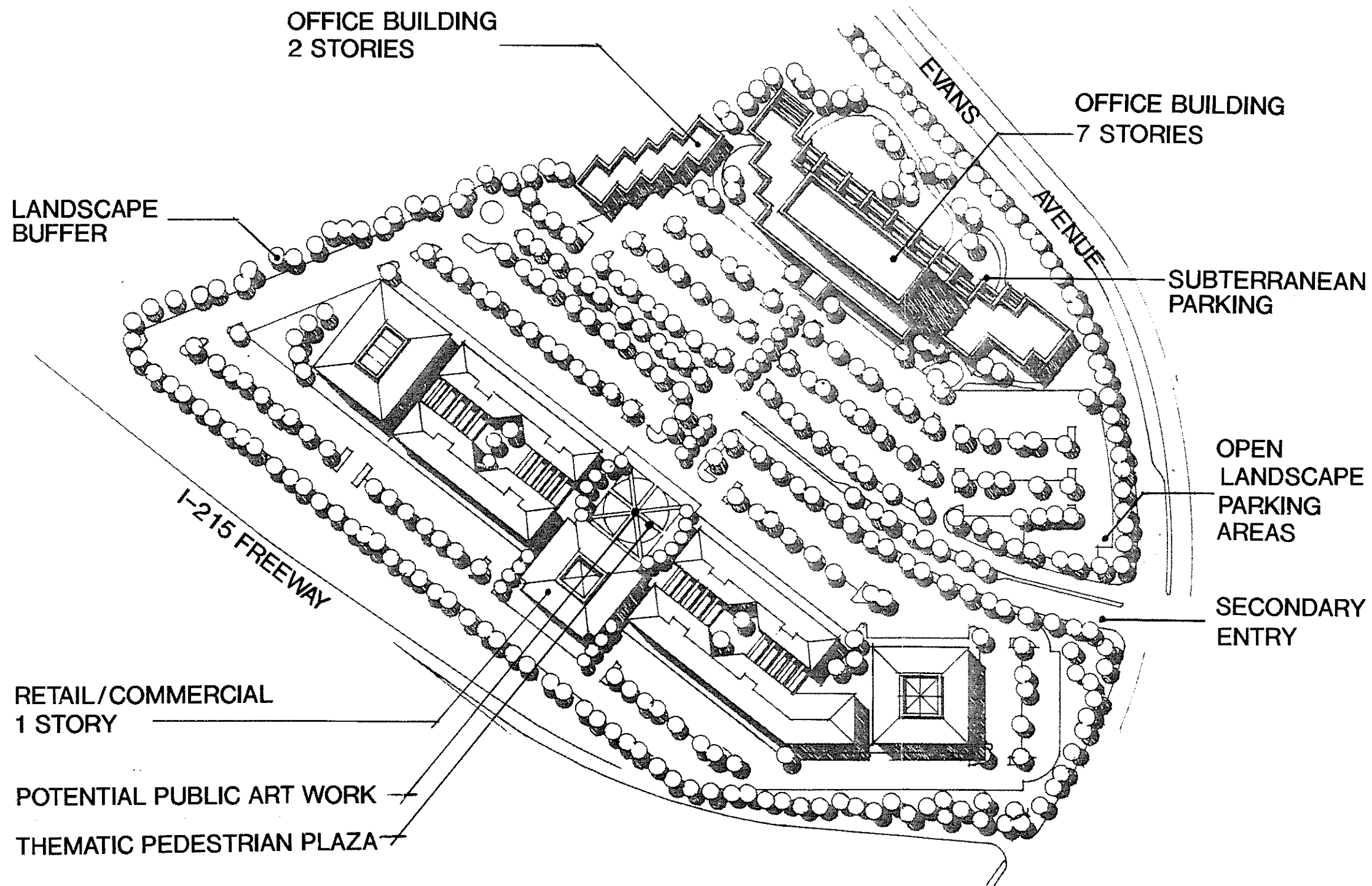


KEY MAP

FIGURE 35



**PLANNING AREA 14
OFFICE/COMMERCIAL
31 ACRES**



KEY MAP
FIGURE 36

4.7 Planning Area 15 – Hotel

4.7.1 Land Use

Planning Area 15 is a 12-acre site located at the northerly portion on the west side of I-215. It will consist of a high-rise hotel and hotel-oriented support service land uses and be visible from I-215 as depicted in **Figure 37, Planning Area 15** and **Figure 38, Planning Area 15 Detail**.

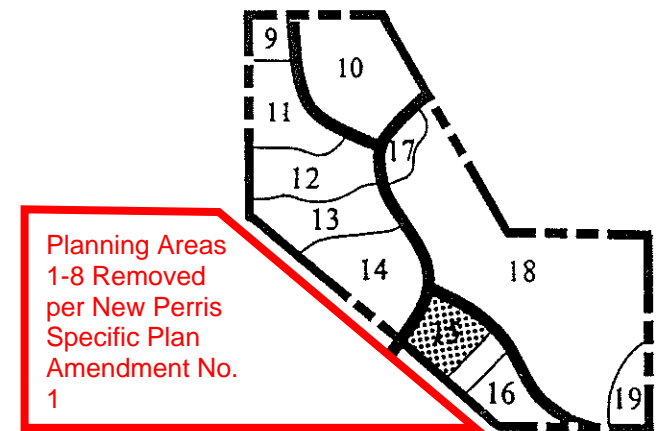
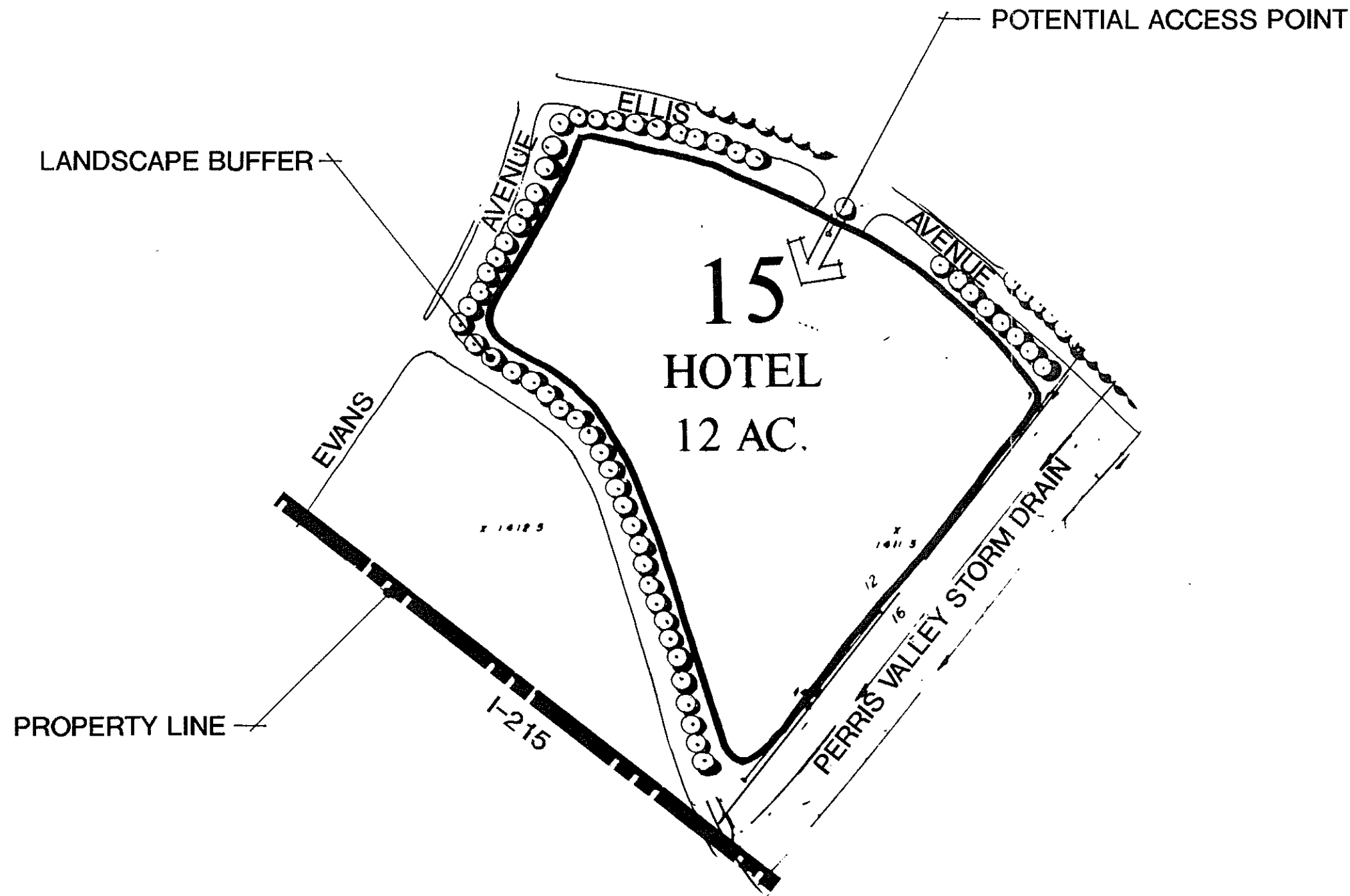
4.7.2 Zoning Ordinance Regulations

- A. Permitted Uses.** The following general uses shall be permitted within Planning Area 15:
- 1) Hotels
- B. Accessory Uses.** The following accessory building and uses customarily incident to any of the above shall be permitted within Planning Area 15:
- 1) Retail uses such as restaurants, coffee shops, gift shops and services such as barber shops, beauty salons, health centers, spas, cocktail lounges, travel and ticket agencies, art galleries, or any use where the principal service is to the guests of the hotel
 - 2) Private garages, parking structures, and/or subterranean parking
 - 3) Swimming pools and spas, tennis and racquet courts, and other recreational facilities
- C. Temporary Uses.** The following temporary uses are permitted in Planning Area 15:
- 1) Temporary construction facility
- D. Development Standards**
- 1) **Maximum Structure Height:** Maximum height for buildings within Planning Area 15 shall be eight (8) stories or 96 feet above grade.
 - 2) **Maximum Building Coverage:** Maximum permissible building coverage for all structures within Planning Area 15 shall be 65 percent.
 - 3) **Setbacks:**
 - a. **Streetside:** Building setbacks from public streets shall be a minimum of thirty-five (35) feet for each building as measured from the face-of-curb and fully landscaped with no building or structure encroachments permitted.
 - b. Adjacent to the Perris Valley Storm Channel, the minimum setback shall be 50 feet of which a minimum of 20 feet shall be landscaped adjacent the channel. No building, structure or parking encroachments shall be permitted.
 - 4) **Parking Requirements:**
 - a. The minimum number of parking spaces required shall be as identified within Section 5.2, Parking Regulations and Standards, of this Specific Plan.
 - b. All other requirements shall be as per the City of Perris parking codes.

- 5) Vehicular access to Planning Area 15 shall conform to the requirements of the City Engineer and Fire Department.
- 6) Proposed uses within Planning Area 15 shall be subject to development plan review by the City of Perris Planning and Community Development Department.

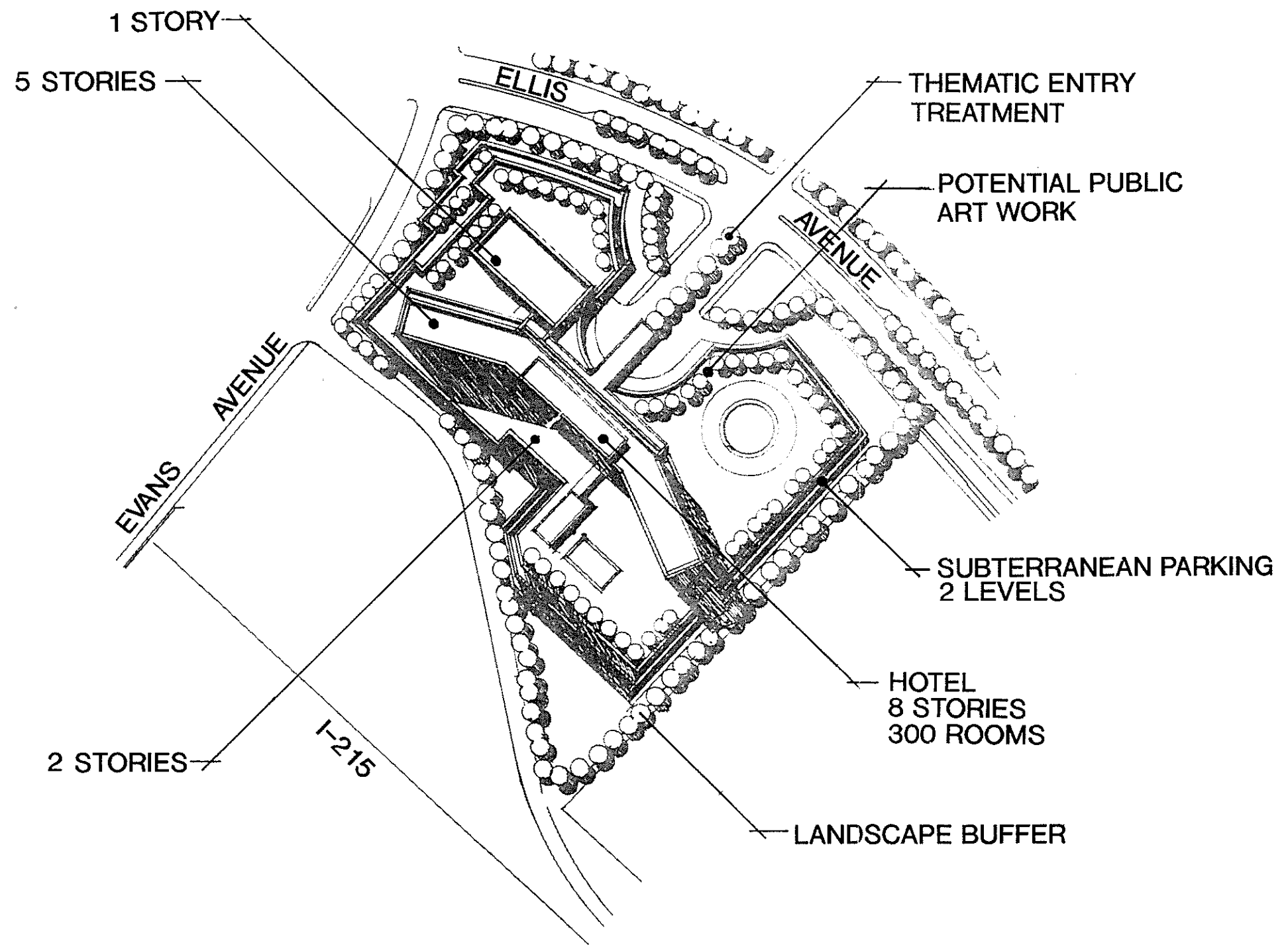
PLANNING AREA 15

HOTEL
12 ACRES

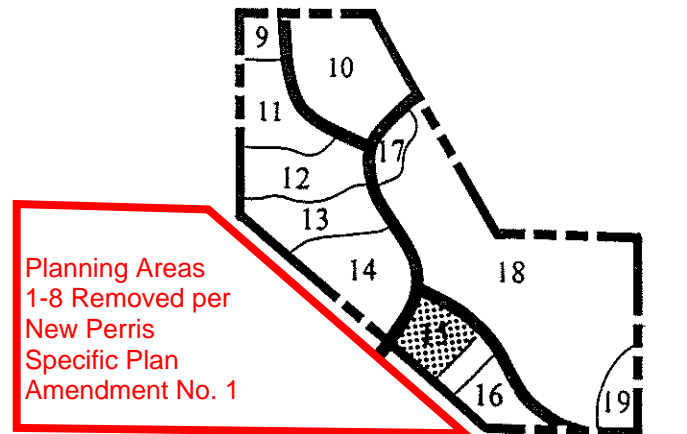


KEY MAP

FIGURE 37



PLANNING AREA 15
HOTEL
12 ACRES



KEY MAP

FIGURE 38



FOR GRAPHIC REPRESENTATION ONLY, NOT FOR ADOPTION PURPOSES

4.8 Planning Area 16 – Retail Commercial

4.8.1 Land Use

Planning Area 16 is a 15-acre site located in the southern portion of the property and bounded by I-215, the Perris Valley Storm Drain, and Ellis Avenue. Planning Area 16 will consist of retail commercial land uses intended to provide goods and services to the local community as depicted in **Figure 39, Planning Area 16** and **Figure 40, Planning Area 16 Detail**.

4.8.2 Zoning Ordinance Regulations

A. Permitted Uses. The following general uses shall be permitted within Planning Area 16:

Retail Related

- 1) Apparel stores
- 2) Appliance stores
- 3) Art galleries, frame shops
- 4) Automotive accessories
- 5) Baker sales shops
- 6) Bicycle shops
- 7) Book stores
- 8) Camera stores
- 9) Candy, nut, and confectionery stores
- 10) Department stores
- 11) Drug stores
- 12) Florist shops
- 13) Gift stores
- 14) Grocery stores
- 15) Furniture stores
- 16) Hardware stores
- 17) Optical goods stores
- 18) Package liquor stores
- 19) Paint, glass and wallpaper stores
- 20) Pet shops (completely enclosed)
- 21) Radio, television and music sound system stores
- 22) Shoe repairs
- 23) Specialty shops
- 24) Sporting goods stores
- 25) Stationary stores
- 26) Theaters, not including drive-ins
- 27) Toy stores
- 28) Variety stores

Service Related

- 1) Apparel repair, alteration, and cleaning
- 2) Barber and beauty shops
- 3) Banks and savings and loan institutions
- 4) Day care centers
- 5) TV stations

- 6) Instant printing and duplicating services
- 7) Shoe repair
- 8) Travel service

Auto Service Center (including car wash, gasoline, service pumps, lube oil and tune-up, and other similar minor automobile repairs) provided that:

- 1) The service center shall be setback at least 100 feet from the right-of-way lines.
- 2) No more than two service centers shall be permitted in Planning Area 16.
- 3) The proposed architecture and site location shall enhance the appearance and character.
- 4) Feasibility of arranging ingress and egress without interference or hazard to circulation patterns.
- 5) Car wash and service repair activities and operations shall be completely and attractively screened.
- 6) Nuisances due to noise or other features associated with the activities and operation shall be sufficiently mitigated through physical location, design, or other measures.

Food Related

- 1) Bars and cocktail lounges, provided such uses are a part of a bona fide restaurant
- 2) Delicatessens and dairy product sales
- 3) Restaurants and sidewalk cafes
- 4) Fast food and carry out restaurants with drive-thru's

B. Other Uses. Other uses as deemed appropriate by the Director of Planning and Community Development.

C. Accessory Uses. The following accessory building and uses customarily incident to any of the above shall be permitted within Planning Area 16:

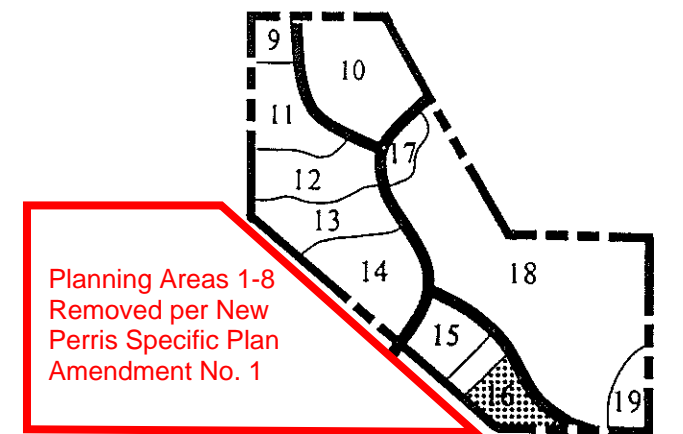
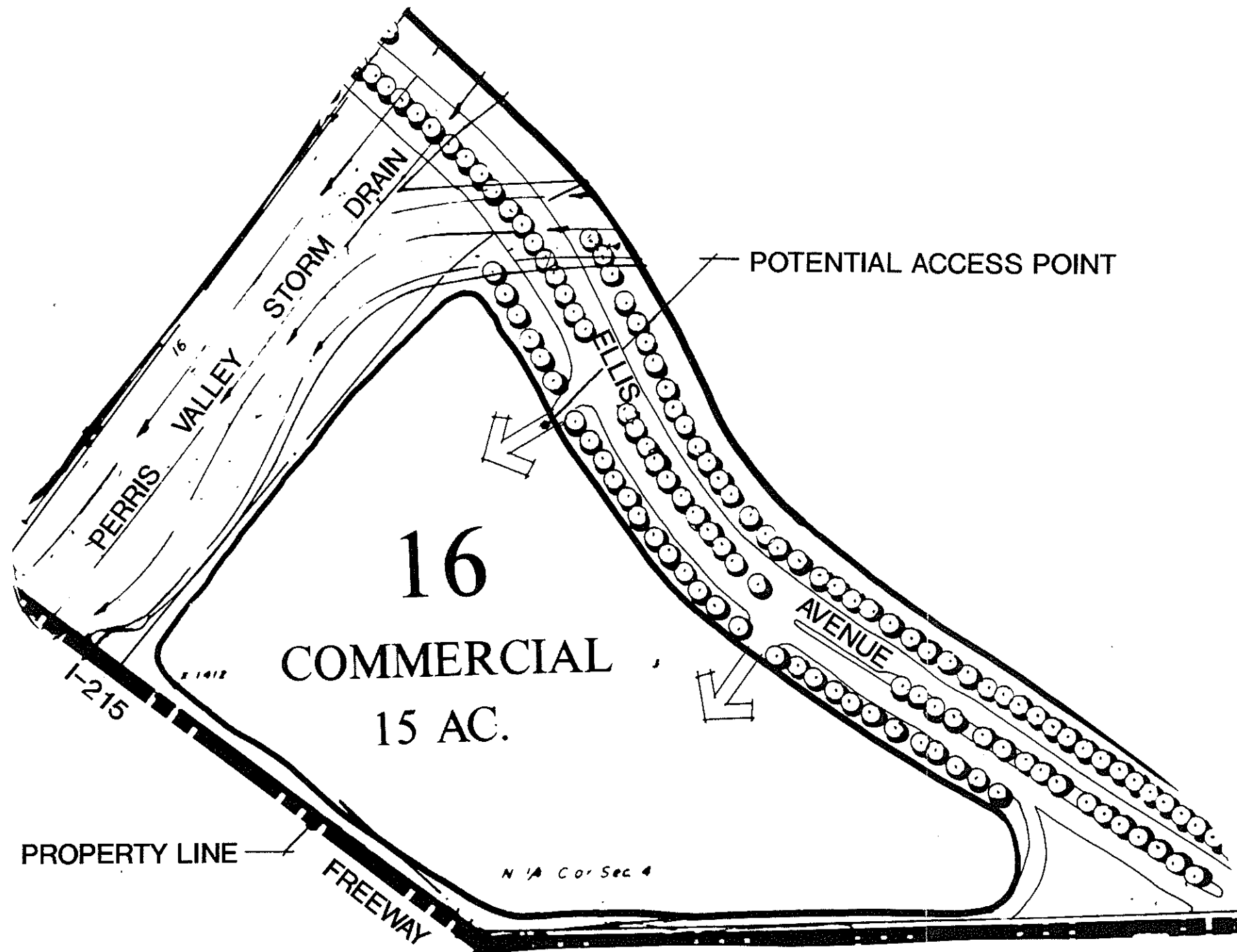
- 1) Private garages, parking structures, and/or subterranean parking
- 2) Accessory structures or uses which are customarily incidental or necessary to the permitted uses
- 3) Pedestrian and bicycle trails

D. Development Standards

- 1) **Maximum Structure Height:** Maximum height for buildings within Planning Area 16 shall be two (2) stories or 24 feet above grade.
- 2) **Maximum Building Coverage:** Maximum permissible building coverage for all structures within Planning Area 16 shall be 30 percent.
- 3) **Setbacks:**
 - a. **Streetside:** Building setbacks from public streets shall be a minimum of thirty-five (35) feet as measured from the face-of-curb, and fully landscaped with no building or structure encroachments permitted.
 - b. Setbacks from the Perris Valley Storm Drain Channel shall be 35 feet, of which 15 feet shall be fully landscaped.

- c. Adjacent to the San Jacinto River, the minimum setback shall be 50 feet of which a minimum of 20 feet shall be landscaped adjacent the property line.
 - d. **I-215:** Building setbacks from I-215 shall be a minimum of 50 feet measured from the right-of-way line. Of the total building setback, a minimum of 20- feet shall be landscaped.
- 4) Parking Requirements:**
- a. The minimum number of parking spaces required shall be as identified within Section 5.2, Parking Regulations and Standards, of this Specific Plan.
 - b. All other requirements shall be as per the City of Perris parking codes.
- 5) Vehicular access to Planning Area 16 shall conform to the requirements of the City Engineer and The Fire Department.
- 6) Proposed uses within Planning Area 16 shall be subject to development plan review by the City of Perris Planning and Community Development Department.

PLANNING AREA 16
COMMERCIAL
15 ACRES



KEY MAP

FIGURE 39

PLANNING AREA 16
COMMERCIAL
15 ACRES

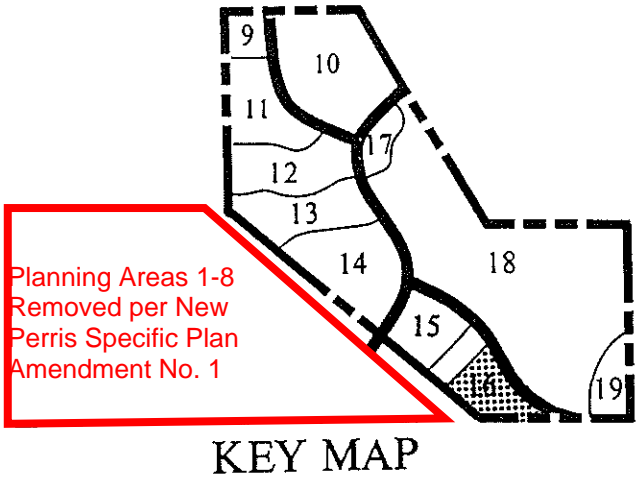
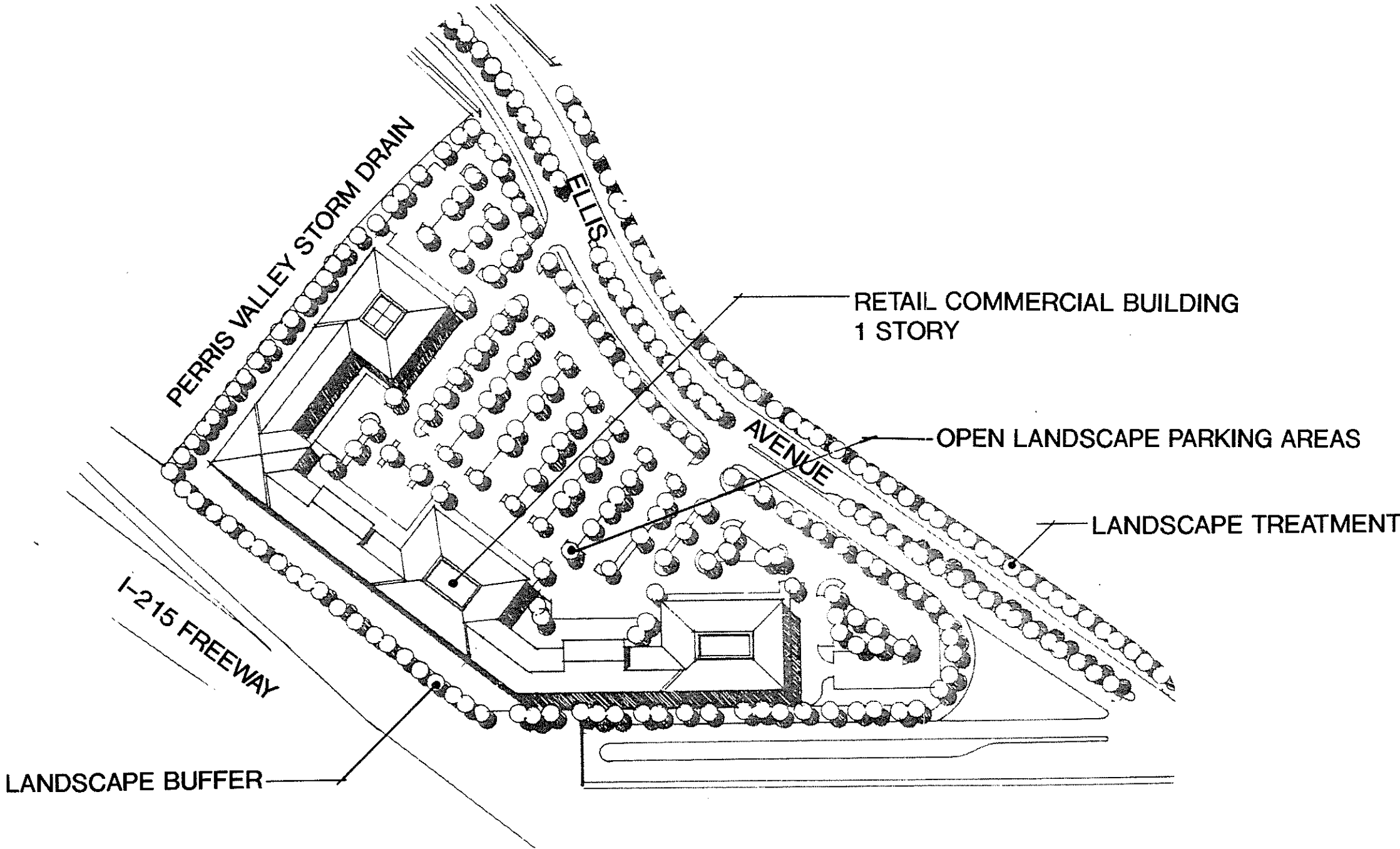


FIGURE 40



GRAPHIC REPRESENTATION ONLY, NOT FOR ADOPTION PURPOSES

4.9 Planning Area 17 – Single-Family Detached Residential

4.9.1 Land Use

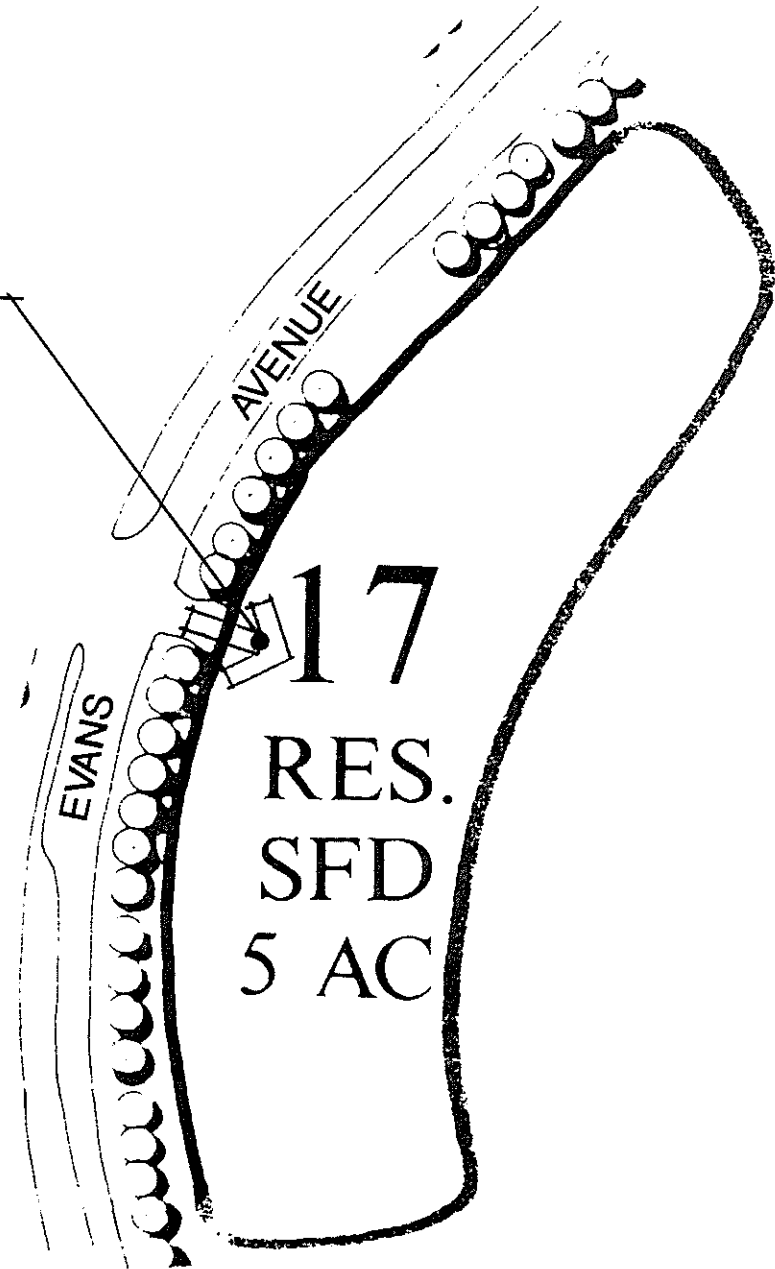
Planning Area 17 is located in the northeast portion of the project site bordered by Evans Road to the west. Planning Area 17 will consist of single-family detached residential land uses on 7,200 square foot lots as depicted in **Figure 41, Planning Area 17** and **Figure 42, Planning Area 17 Detail**.

4.9.2 Zoning Ordinance Regulations

- A. Permitted Uses.** The following general uses shall be permitted within Planning Area 17:
- 1) Single-Family dwellings of a permanent character, in a permanent location, and used by only one family
- B. Accessory Uses.** The following accessory building and uses customarily incident to any of the above shall be permitted within Planning Area 16:
- 1) A private garage for the accommodation of not less than two automobiles
 - 2) Arbors, trellises, gazebos, and similar shade structures of open construction
 - 3) Fences and walls
 - 4) Patio covers
 - 5) Swimming pools and spas
 - 6) Pedestrian and bicycle trails
 - 7) Tennis and racquet courts
 - 8) Home occupations, including any vocation, trade, professions, or childcare operation carried on solely by the occupant of the premise shall be permitted, provided that:
 - a. There is no alteration in the residential character of the premises in connection with such vocation or trade
 - b. All operations are carried on within the dwelling
 - c. Not more than 25 percent of the ground floor area of the dwelling shall be occupied or used
 - d. No motor other than electrically driven motors shall be used in connection there with
 - e. No merchandise or articles shall be displayed
 - f. No merchandise or articles pertaining to such home occupation shall be stored other than in the dwelling
 - g. No assistants shall be employed in connection there with
 - h. The premises shall not be used as a place of business or point of sale of the products of such home occupation
 - i. All operations in connection with such home occupation shall be conducted so as to prevent the emanation of any dust, gas, smoke, noise, fumes, odors, vibrations, or electrical disturbances, which are or may be detrimental to the interests or welfare of the occupants of surrounding properties
 - 9) Neighborhood parks
 - 10) Any other similar uses as deemed appropriate by the City of Perris Director of Planning and Community Development.

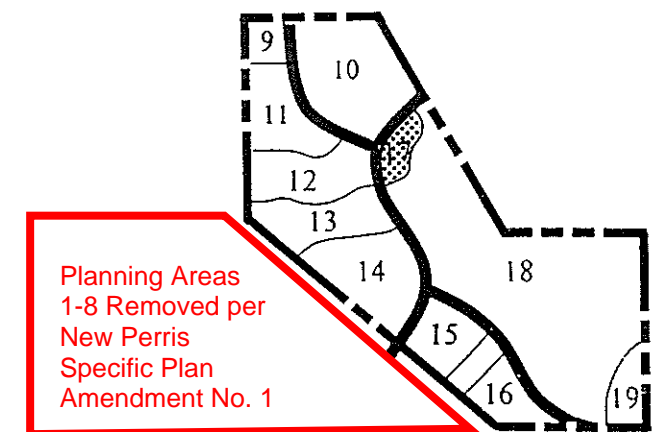
- C. Temporary Uses.** The following temporary uses shall be permitted:
- 1) Temporary construction facility
 - 2) Temporary real estate offices and model homes located within a subdivision, to be used only for and during the original sale of the subdivision
- D. Development Standards**
- 1) **Maximum Structure Height:** Maximum height for buildings within Planning Area 17 shall be two stories or 35 feet above grade.
 - 2) **Minimum Lot Size:** The minimum lot size within Planning Area 17 shall be not less than 7,200 square feet.
 - 3) **Maximum Building Coverage:** Maximum permissible building coverage by lot for all structures within Planning Area 17 shall be 50 percent.
 - 4) **Setbacks:**
 - a. **Front yard:** Building setbacks for Planning Area 17 shall be a minimum of 25 feet as measured from the face-of-curb; garages shall be setback a minimum of 20 feet as measured from the face-of-curb.
 - b. **Rear yard:** Building setbacks for Planning Area 17 rear yards shall be a minimum 25 feet. Patio covers may extend to within 10 feet of the rear property line.
 - c. **Side yard:** Building setbacks for Planning Area 17 for side yards shall be a minimum of 5 feet from the nearest property line on interior and through lots. For side yards on corner and reverse corner lots, setbacks shall be a minimum of 10 feet, excluding architectural projections of 2 feet maximum.
 - d. **Building to Building:** Minimum distance between buildings in Planning Area 17 shall be 10 feet, excluding architectural projections of 2 feet maximum, such as fireplace boxes and bay windows.
 - 5) **Minimum Lot Width:** The minimum width of a lot used as a building site area in Planning Area 17 shall be 60 feet.
 - 6) Landscaping shall buffer surrounding uses to enhance view impacts.
 - 7) Proposed uses within Planning Area 17 shall be subject to development plan review by the City of Perris Planning and Community Development Department and the Fire Department.
- E. Density Regulations**
- 1) The density within Planning Area 17 shall not exceed 3.6 dwelling units per gross acres.

ACCESS POINT



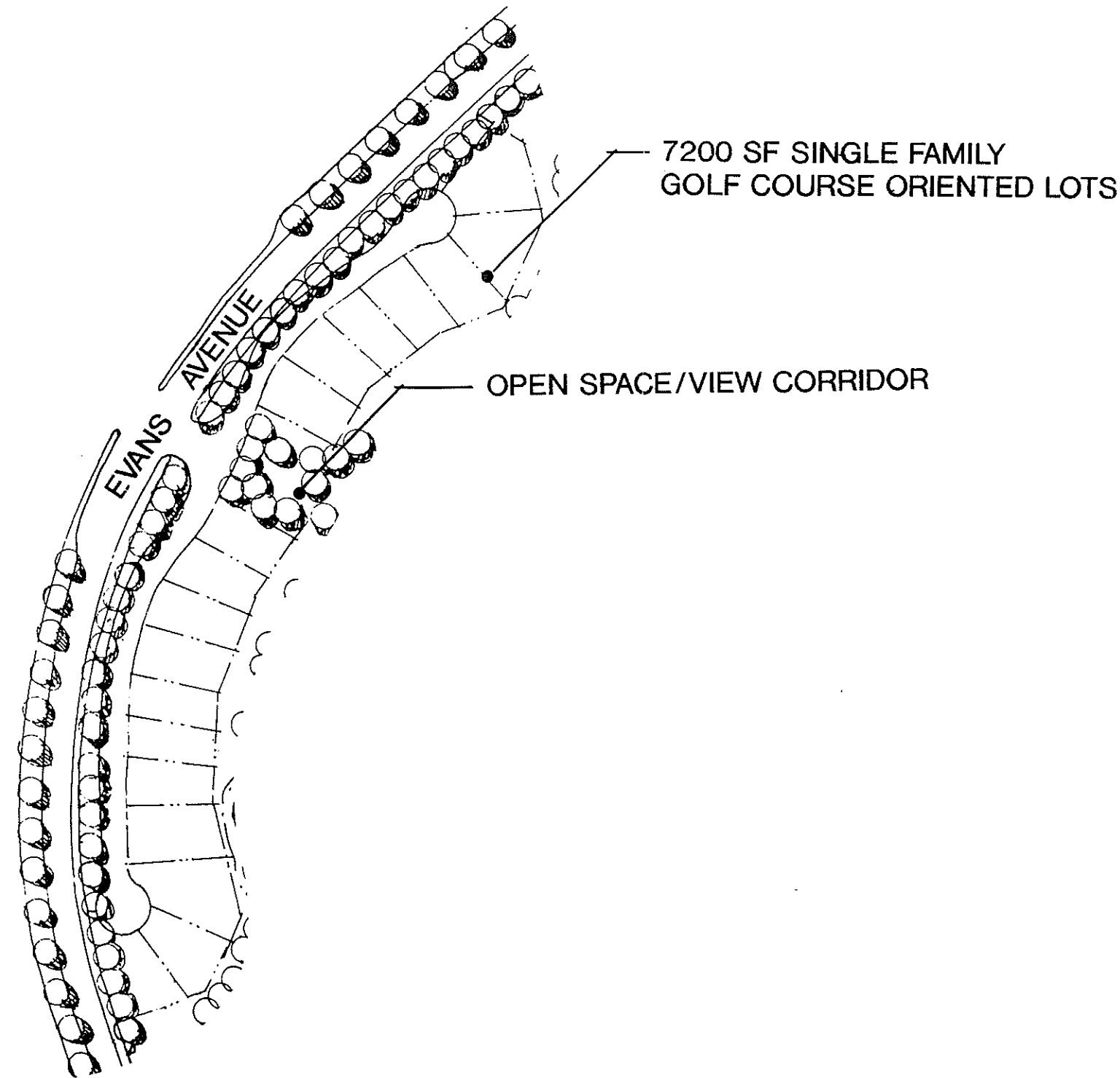
PLANNING AREA 17

RESIDENTIAL SFD
5 AC



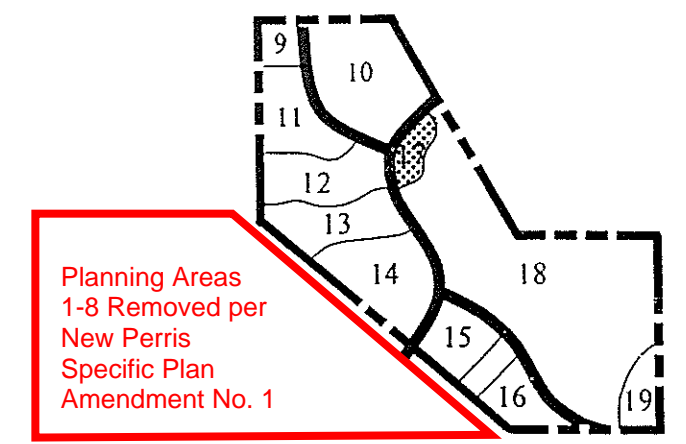
KEY MAP

FIGURE 41



PLANNING AREA 17

RESIDENTIAL SFD
5 AC
18 UNITS
3.6 DU/AC



Planning Areas
1-8 Removed per
New Perris
Specific Plan
Amendment No. 1

KEY MAP

FIGURE 42

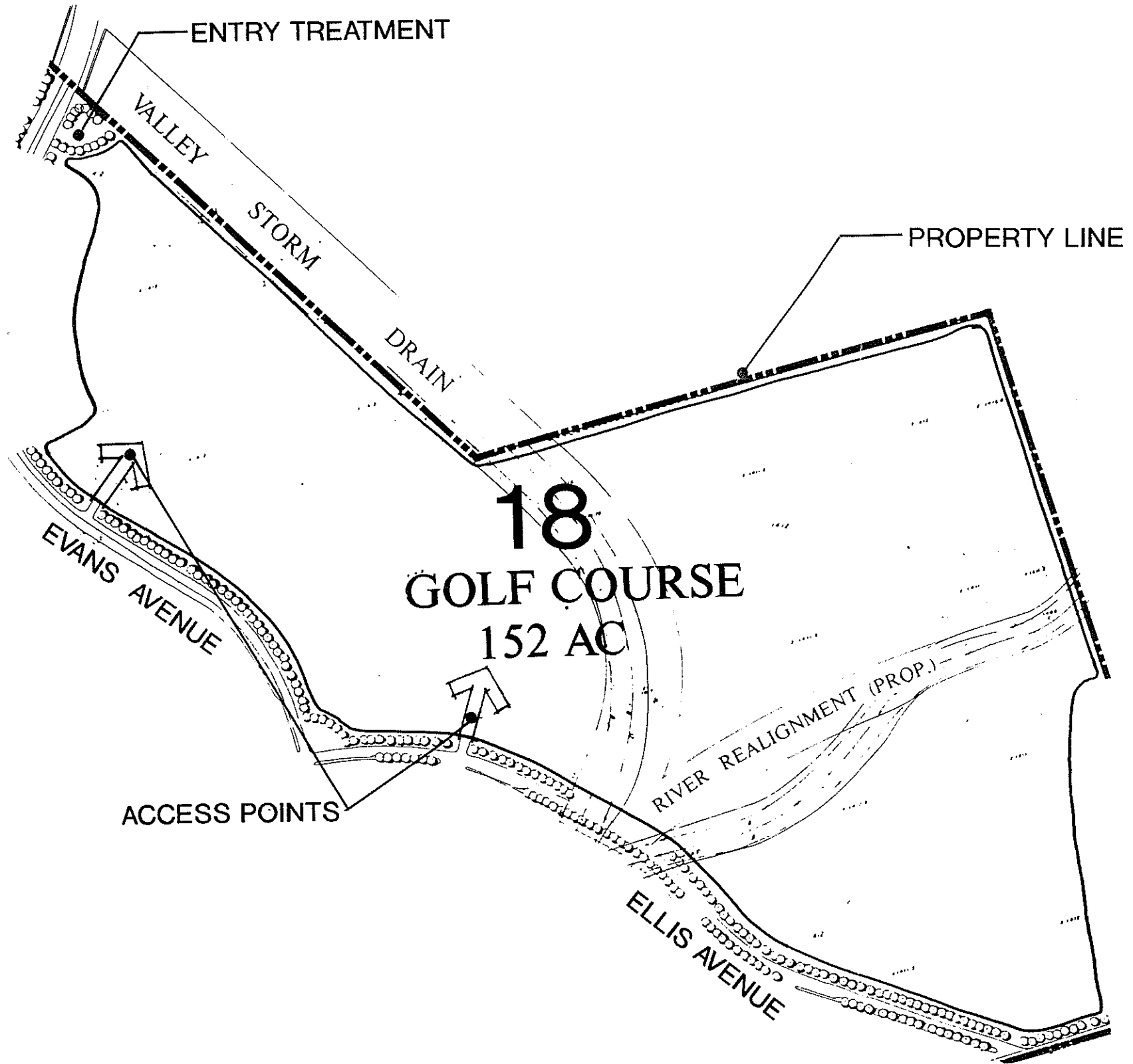
4.10 Planning Area 18 – Golf Course

4.10.1 Land Use

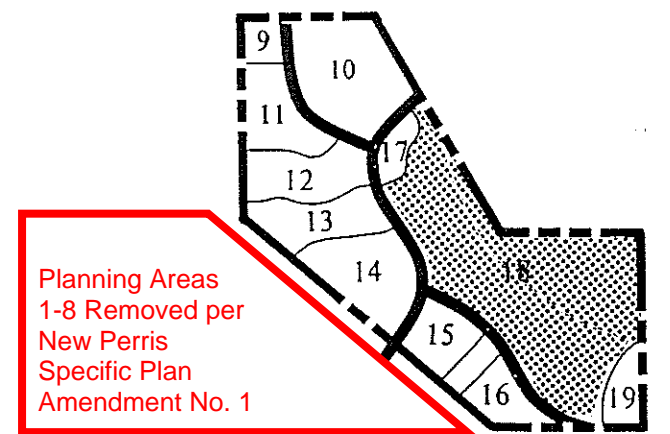
Planning Area 18 consists of 152 acres and is located in the central portion of the project site bordered by Evans Road to the east and I-215 to the west. Planning Area 18 proposes a complete golf course operation with a driving range and clubhouse facilities as depicted in **Figure 43, Planning Area 18** and **Figure 44, Planning Area 18 Detail**.

4.10.2 Zoning Ordinance Regulations

- A. Permitted Uses.** The following general uses shall be permitted within Planning Area 18:
- 1) Golf course, clubhouse, driving range and any other use related to a complete golf course operation
 - 2) Lakes and Ponds
 - 3) Tennis courts
 - 4) Public use and public utility buildings, structures, and facilities
 - 5) Detention and retention basin
 - 6) Any similar recreational uses as deemed appropriate by the City of Perris Director of Planning and Community Development
- B. Accessory Uses.** The following accessory building and uses customarily incident to any of the above shall be permitted within Planning Area 18:
- 1) Accessory buildings including but limited to guard houses, trash enclosures, storage buildings, and maintenance buildings
 - 2) Equestrian, pedestrian, and bicycle trails
- C. Temporary Uses.** The following temporary uses shall be permitted:
- 1) Parking for special events
 - 2) Temporary construction facilities
 - 3) Fairs and/or carnivals
- D. Development Standards**
- 1) **Maximum Structure Height:** Maximum height for buildings within Planning Area 18 shall be one (1) story or 25 feet above grade.
 - 2) **Maximum Building Coverage:** Maximum permissible building coverage for all structures within Planning Area 18 shall be 20 percent.
 - 3) **Setbacks**
 - a. **Streetside:** Building setbacks from public streets shall be a minimum of 50 feet as measured from the face- of-curb and shall be fully landscaped.
 - 4) Planning Area 18 shall have an open space nature.
 - 5) Landscaping shall buffer surrounding uses to enhance view impacts and preserve the open space nature.
 - 6) Proposed uses within Planning Area 18 shall be subject to development plan review by the City of Perris Planning and Community Development Department and the Fire Department.



PLANNING AREA 18
GOLF COURSE
152 AC



Planning Areas
 1-8 Removed per
 New Perris
 Specific Plan
 Amendment No. 1

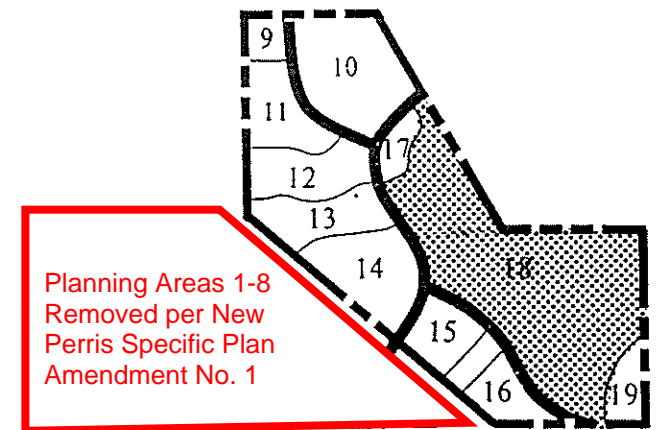
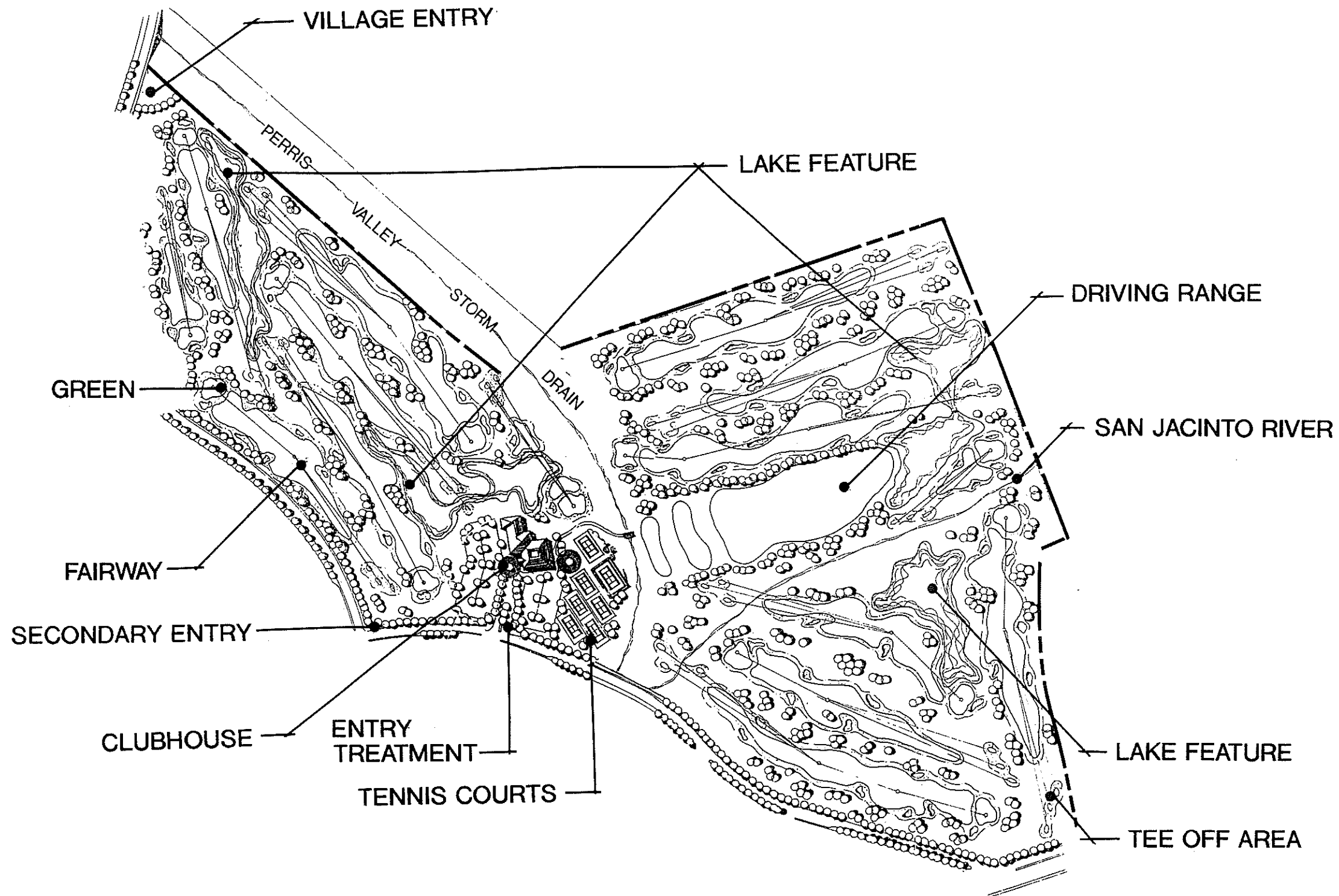
KEY MAP

FIGURE 43



PLANNING AREA 18

GOLF COURSE
152 AC



KEY MAP

FIGURE 44

4.11 Planning Area 19 – Single-Family Detached Residential (10,000 SF)

4.11.1 Land Use

Planning Area 19 is 9 acres and is located in the central portion of the project site bordered by Evans Road to the east, Murrieta to the north. Planning Area 19 will consist of Single-Family Detached residential land uses on 10,000 square foot lots as depicted in **Figure 45, Planning Area 19** and **Figure 46, Planning Area 19 Detail**.

4.11.2 Zoning Ordinance Regulations

- A. Permitted Uses.** The following general uses shall be permitted within Planning Area 19:
- 1) Single-Family dwellings of a permanent character, in a permanent location, and used by only one family
- B. Accessory Uses.** The following accessory building and uses customarily incident to any of the above shall be permitted within Planning Area 19:
- 1) A private garage for the accommodation of not less than two automobiles
 - 2) Arbors, trellises, gazebos, and similar shade structures of open construction
 - 3) Fences and walls
 - 4) Patio covers
 - 5) Swimming pools and spas
 - 6) Pedestrian and bicycle trails
 - 7) Tennis and racquet courts
 - 8) Home occupations, including any vocation, trade, professions, or childcare operation carried on solely by the occupant of the premise shall be permitted, provided that:
 - a. There is no alteration in the residential character of the premises in connection with such vocation or trade
 - b. All operations are carried on within the dwelling
 - c. Not more than 25 percent of the ground floor area of the dwelling shall be occupied or used
 - d. No motor other than electrically driven motors shall be used in connection therewith
 - e. No merchandise or articles shall be displayed
 - f. No merchandise or articles pertaining to such home occupation shall be stored other than in the dwelling
 - g. No assistants shall be employed in connection therewith
 - h. The premises shall not be used as a place of business or point of sale of the products of such home occupation
 - i. All operations in connection with such home occupation shall be conducted so as to prevent the emanation of any dust, gas, smoke, noise, fumes, odors, vibrations, or electrical disturbances, which are or may be detrimental to the interests or welfare of the occupants of surrounding properties
 - 9) Neighborhood parks

10) Any similar residential uses as deemed appropriate by the City of Perris Director of Planning and Community Development.

C. Temporary Uses. The following temporary uses shall be permitted:

- 1) Temporary construction facility
- 2) Temporary real estate offices and model homes located within a subdivision, to be used only for and during the original sale of the subdivision

D. Development Standards

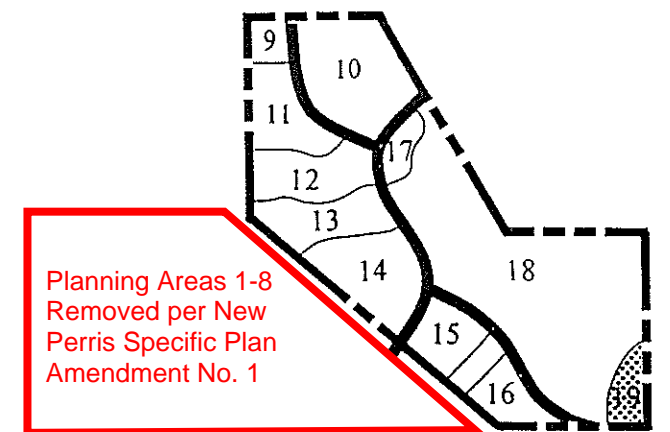
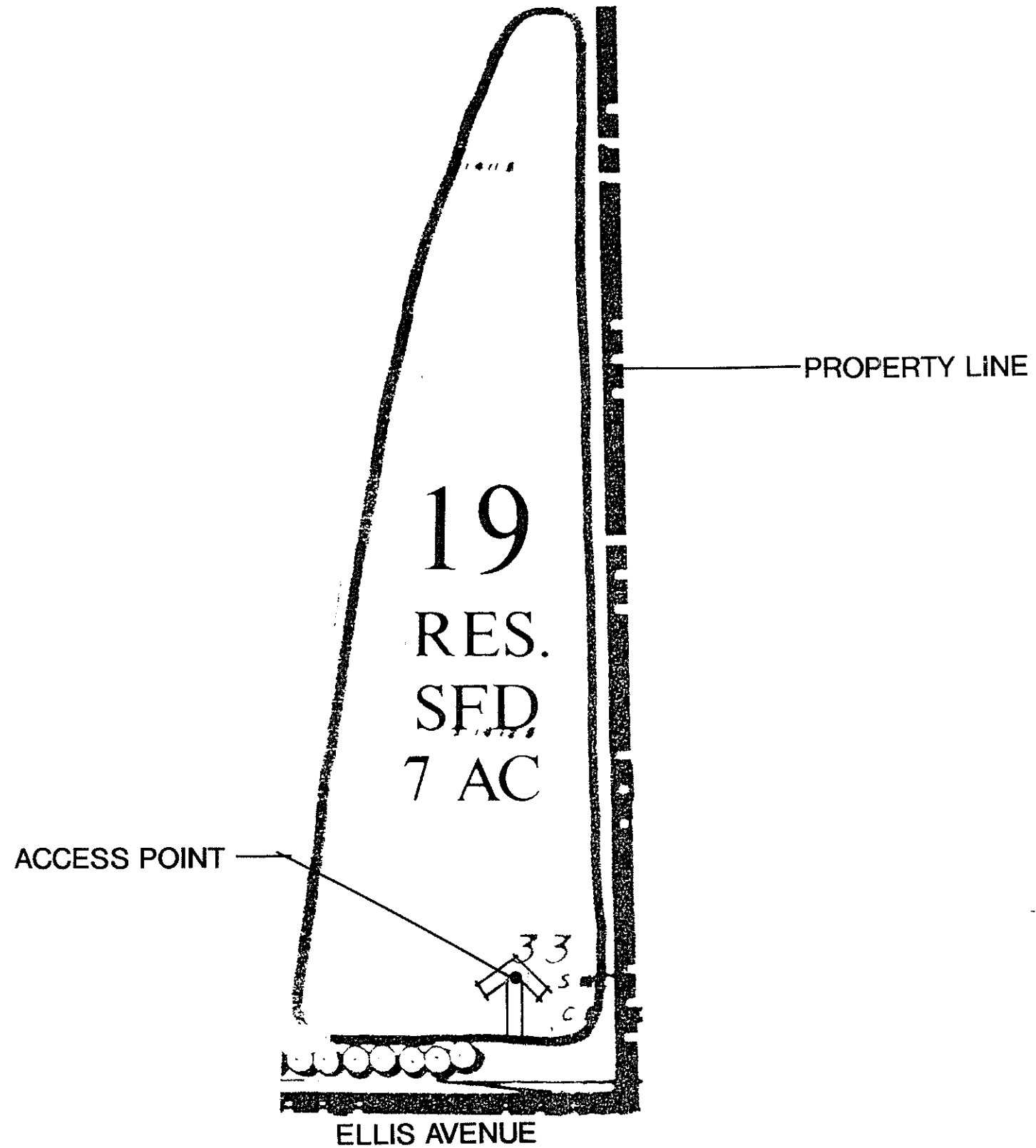
- 1) **Maximum Structure Height:** Maximum height for buildings within Planning Area 19 shall be two stories or 35 feet above grade.
- 2) **Minimum Lot Size:** The minimum lot size within Planning Area 19 shall be not less than 10,000 square feet.
- 3) **Maximum Building Coverage:** Maximum permissible building coverage by lot for all structures within Planning Area 19 shall be 50 percent.
- 4) **Setbacks:**
 - a. **Front yard:** Building setbacks for Planning Area 19 shall be a minimum of 25 feet as measured from the face-of-curb; garages shall be setback a minimum of 20 feet as measured from the face-of-curb.
 - b. **Rear yard:** Building setbacks for Planning Area 19 rear yards shall be a minimum 25 feet. Patio covers may extend to within 10 feet of the rear property line.
 - c. **Side yard:** Building setbacks for Planning Area 19 for side yards shall be a minimum of 5 feet from the nearest property line on interior and through lots. For side yards on corner and reverse corner lots shall be a minimum of 10 feet, excluding architectural projections of 2 feet maximum.
 - d. **Building to Building:** Minimum distance between buildings in Planning Area 19 shall be 10 feet, excluding architectural projections of 2 feet maximum, such as fireplace boxes and bay windows.
- 5) **Width of lots:** The minimum width of a lot used as a building site area in Planning Area 19 shall be 80 feet.
- 6) Landscaping shall buffer surrounding uses to enhance view impacts.
- 7) Proposed uses within Planning Area 19 shall be subject to development plan review by the City of Perris Planning and Community Development Department.

E. Density Regulations

- 1) The density within Planning Area 19 shall not exceed 2.4 dwelling units per gross acre.

PLANNING AREA 19

RESIDENTIAL SFD
7 AC



KEY MAP

FIGURE 45

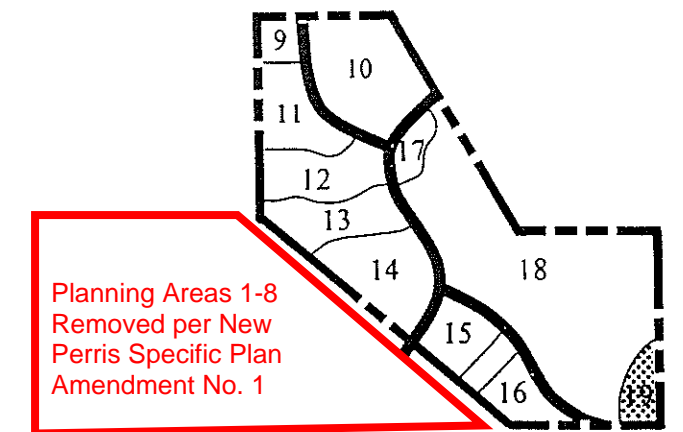
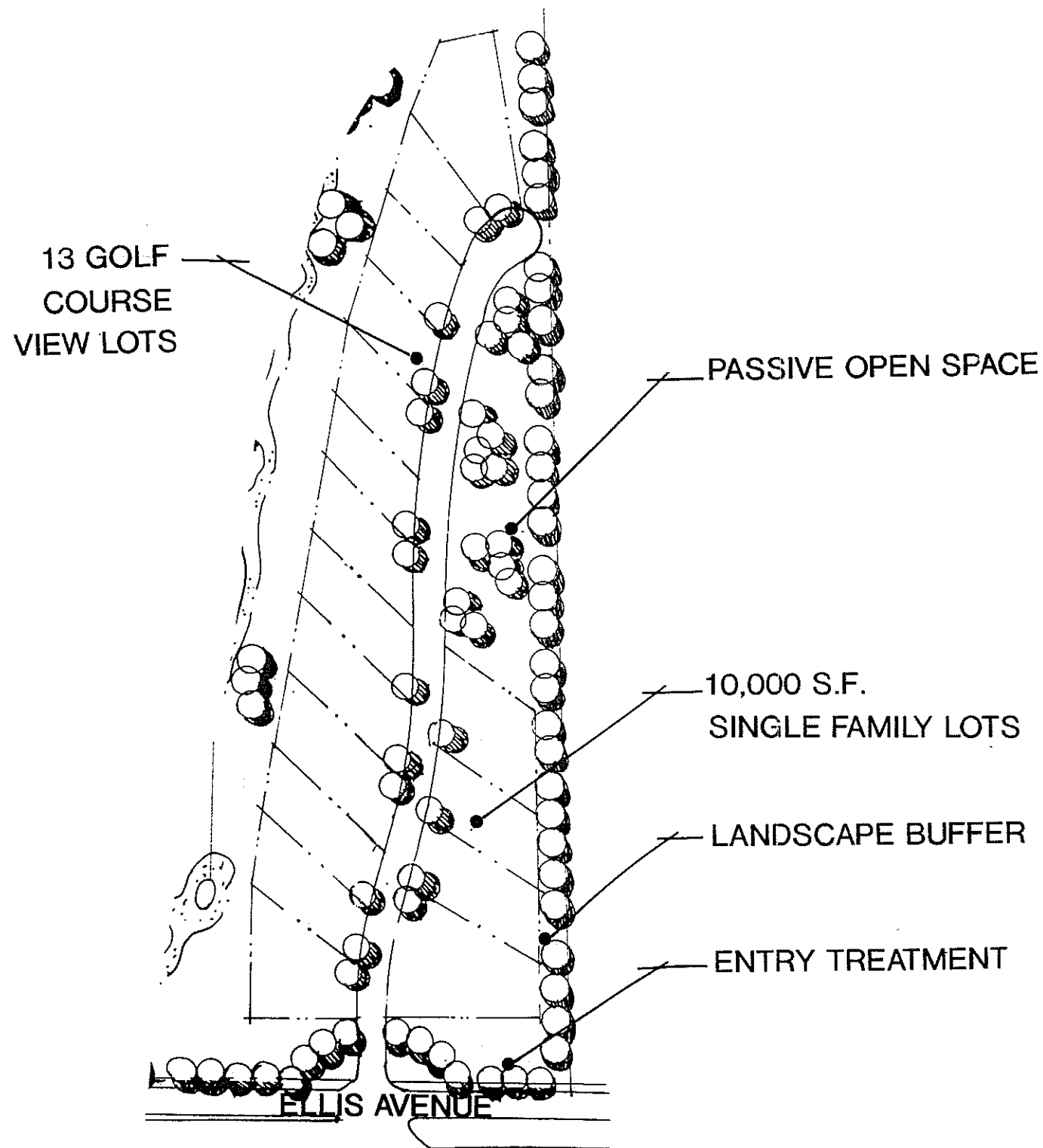
PLANNING AREA 19

RESIDENTIAL SFD

7 AC

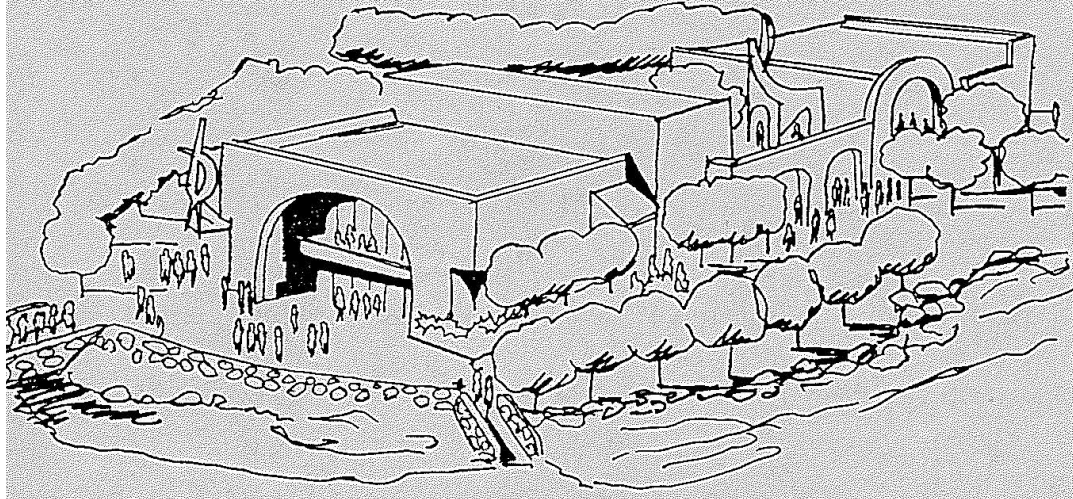
17 UNITS

2.4 DU/AC



KEY MAP

FIGURE 46



5.0 GENERAL DEVELOPMENT STANDARDS

5.0 GENERAL DEVELOPMENT STANDARDS

5.1 Site Planning and Architectural Guidelines

5.1.1 Architecture Guidelines

To promote general design quality and consistency throughout the New Perris development area, architectural design should reflect the following design elements:

- A. Architectural style should be contemporary, straightforward and consistent.
- B. Building massing should consist of strong, simple, integrated geometric forms.
- C. Exterior building materials are to be of a contemporary nature that expresses the high quality of development intended for New Perris, such as smooth concrete, smooth metal panels and glass. New technology materials are acceptable if they accomplish a contemporary style consistent with the New Perris design guidelines.
- D. All colors, textures and materials on exterior elevations are to be coordinated to achieve continuity of design.
- E. View corridors into the project site from I-215 and adjacent streets shall be enhanced whenever possible.
- F. Buildings will be oriented in such a way to promote variation of architectural elements so that off-site views of the City are enhanced.

5.1.2 Office

- A. The overall plan shall be cohesive, embracing land, landscaping, the environment, open spaces, the arts and their interrelationships. The design review committee shall take into account intelligent selection of form and details, sensitive development of massing, consistency of site planning, landscaping, and architecture, logical, sensitive and creative use of materials.
- B. No harsh contrast of colors, large massing illogical or inappropriate combination of scale, poorly designed, or inappropriate location of details, excessive use of design elements or extreme interpretations of style shall be permitted.
- C. The site plan shall provide for adequate pedestrian and vehicular circulation and off-street parking and shall encourage the use of bicycles, carpools, and ride-share programs.
- D. Circulation capacity of the plan shall not constitute a disruptive element to the site and/or community.

- E. Architectural design, including signage shall be thematic and cohesive throughout the development inclusive of all types of uses.
- F. Building facades adjacent to streets shall be integrated with the building design and shall not have excessive bulk or shading, or appearance thereof.
- G. Buildings clustered around a pedestrian area, such as a plaza or courtyard shall be designed to minimize shading while maximizing light exposure.
- H. The physical orientation of buildings shall be given careful attention so as not to create the appearance of a tunnel effect.
- I. Interior and exterior spaces shall be linked together by window areas, courtyards, and breezeways to create a visual link with exterior settings. The project will be designed to be physically and socially cohesive both within and outside the immediate area and will be self-contained within the environment.
- J. The use of solar energy techniques shall be encouraged. Solar collectors shall be oriented away from public view or unified with the architecture of the building.
- K. Roof flashing, rain gutters and down spouts, vents and other roof protrusions shall be screened from view or finished to match adjacent materials and or colors.
- L. Walls and/or fences shall be used to screen utility and maintenance structures/facilities, storage, parking etc. These surfaces shall match the exterior finish of any structure with which they are in contact.
- M. Site design shall emphasize visual corridors.

5.1.3 Commercial

- A. The commercial areas shall be pedestrian oriented including appropriate pedestrian amenities.
- B. The plan shall provide for adequate circulation and parking. The building structures and facilities and accessory uses shall be well integrated. The use of bicycles, carpools, vans and ride-share programs will be encouraged. Public transit stops will be distributed throughout the project site so encourage the use of public transportation.
- C. Landscaping along fronting streets shall be designed to provide views of the commercial buildings.

- D. The approach to the commercial center from the street should serve to emphasize and reinforce the building interrelationship with the streetscene.
- E. For neighborhood commercial areas, buildings may be clustered together around pedestrian plazas and parking areas.
- F. If large anchor tenants or a variety of different size shops and service uses are planned for a commercial center, then the large anchor stores should be oriented toward the adjacent major streets and highways. These anchors stores will serve to draw consumers into the anchor stores and their ability to attract high volume consumer traffic.
- G. All parking required for employees, clients, customers or any others related to an enterprise shall be provided on-site and in close proximity of buildings where the parking is served. Large expanses of parking should be buffered from adjacent arterial streets and highways.
- H. Buildings and structures shall be designed with a low, horizontal profile whenever possible. Building and structures intended for garden office uses may be designed as a series of units incorporating such things as courtyards, plazas, open space, and parking areas.
- I. Outdoor storage of material is permitted, provided the storage area is completely enclosed by walls, fences, buildings, or a combination thereof. Walls or fences shall be of a solid material unless setback far enough where it is not visible from the street. No merchandise, material or equipment is stored to a height greater than any adjacent wall, fence building, or landscape screening. Outdoor storage shall also be screened from above if adjacent to a building with a higher elevation.
- J. Building facades abutting streets shall not have the appearance of excessive massing or shading.
- K. The juxtaposition and configuration of building forms shall be given careful attention so as not to create a venturi or wind funnel effect.
- L. Orientation, configuration and location of building shall emphasize visual corridors.
- M. Special consideration shall be given to emphasize pedestrian areas architecturally such as entryways, walkways, and courtyards/plazas (e.g., concrete trellis, low parapet walls, extended roof or patio overhangs).
- N. Solar collectors shall be oriented away from public view or made as an integral part of the roof structure. In addition, roof flashing, rain gutters and downspouts, vents and other roof protrusions shall be screened from view or finished to match adjacent materials and/or colors.

- O. The utilization of glass areas shall be encouraged in order to extend interior space to the outside, and to create a visual link with the exterior setting of court or plaza areas.

5.1.4 Single Family Detached

- A. Local and curvilinear arterial and collector streets layouts shall be encouraged to enhance the streetscape visual character.
- B. Use of unbroken or geometric "grid" layouts shall be discouraged.
- C. Designs shall emphasize the idea of creating cohesive neighborhoods.
- D. Articulation of streetscape themes shall be encouraged through variations in unit footprints, massing, roof form diversity, and architectural detailing; through variation in orientation of garages and entries; and through variable setback dimensions.
- E. The construction of single story homes at corner locations shall be encouraged to provide aesthetic relief from building mass along the street.
- F. Rooftop and ground-mounted mechanical equipment shall be located and screened to minimize the visual intrusion of the equipment into the public view. Rooftop equipment shall be painted to match the roof and shall be located below the ridge of pitched roofs or the parapet of flat roofs. As an alternative for flat roofs, mechanical equipment may be clustered or located behind a screen wall that architecturally complements the building elevation. Such screen walls shall be setback from the building edge at a ratio of two (2) feet horizontal for every one (1) foot of vertical screen. The screens shall be considered as part of the minimum height limitations.
- G. Ground-mounted equipment shall be screened from the public view with building walls, free-standing solid walls and gates, and overhead trellises complementary with the project's architectural elements is required.

5.2 Parking Regulation and Standards

5.2.1 Intent and Purpose

The purpose of these regulations is to ensure that adequate off-street parking is provided to meet the parking needs of uses located within the New Perris project. All parking areas shall be designed and situated to ensure their usefulness; to protect the public safety; and where appropriate, to mitigate potential adverse impacts on adjacent uses. It shall be the responsibility of the developer, owner or operator of a specific use area to provide and maintain adequate off-street parking for that area.

5.2.2 Applicability of Regulations

- A. Both vehicle and bicycle off-street parking facilities shall be provided for any new building constructed; for any new use established; and for any change in use of an existing building that would require additional parking spaces.
- B. Additions, enlargements or change of use of any existing building or use area, and any change of occupancy or change in manner of operation that would increase the number of parking spaces required, the additional parking spaces shall be required only for such addition, enlargement, or change, and not for the entire building or use, unless required as a condition of approval of development permit.

5.2.3 Vehicle Off-Street Parking Requirements

Table 5-1, Vehicle Off-Street Parking Requirements, below, contains the minimum vehicle off-street parking requirements and the maximum amount of compact spaces allowed:

Table 5-1, Vehicle Off-Street Parking Requirements	
RESIDENTIAL	
Single-Family Detached	2.0 covered spaces/unit
OFFICES	
Low-Rise	4.0 spaces/1,000 square feet of gross floor area; maximum of 40 percent compact spaces allowed.
Mid and High-Rise Tower	4.0 spaces/1,000 square feet of gross floor area; maximum of 40 percent compact spaces allowed.
COMMERCIAL	
General Commercial	4.0 spaces/1,000 square feet of gross floor area; maximum of 25 percent compact spaces allowed.

Table 5-1, Vehicle Off-Street Parking Requirements	
Banks; Savings and Loan	4.0 spaces/1,000 square feet of gross floor area; maximum of 25 percent compact spaces allowed.
Hotel	1.0 space/room plus banquet/convention parking at 1.0 space/5 seats or 5.0 spaces/35 square feet of gross floor area, whichever is the greatest number of spaces.
Freestanding Restaurants and Dinner House	1.0 space/100 square feet of gross floor area under 6,000 gross square feet plus 1.0 space/55 gross square feet of floor area over 6,000 gross square feet; maximum 25 percent compact spaces allowed.
Freestanding Carry-Out Restaurants	1.0 space/100 square feet gross floor area; maximum 25 percent compact spaces allowed; special design requirement if there is a drive-thru facility proposed (see Section 5.2.12)
Theaters	1.0 space/2.5 seats plus 5 spaces for employees; maximum 25 percent compact spaces allowed.
Amphitheater	1.0 space/3 seats or 1 space 35 square feet of gross floor area where there are no fixed seats; maximum 25 percent compact spaces allowed
Auto Sales and Auto Repair	1.0 space/400 square feet of gross floor area; maximum 25 percent compact spaces allowed.
Golf Course	6.0 spaces/hole.
INDUSTRIAL	
Research and Development	1.0 space/250 square feet of gross floor area; maximum 50 percent compact spaces allowed.

5.2.4 **Motorcycle Parking Requirements**

Motorcycle parking shall be provided for all uses, at the following rate:

- A. One motorcycle space, dimensioned at 5 feet by 7 feet, shall be required for each 25 automobile spaces. Uses with more than 100 automobile parking spaces shall provide motorcycle parking areas at the rate of 1 motorcycle parking area for every 100 automobile parking spaces provided.

5.2.5 Handicapped Parking Requirements

Handicapped parking requirements are established by the State of California. The parking standards contained in the section are identical to those established by the State at the time of adoption of this Specific Plan. Any future changes in the State handicapped parking standards would preempt the requirements given in this section.

- A. Handicapped parking spaces shall be provided for multi-family and industrial areas as identified in **Table 5-2, Handicapped Parking Spaces**.

Table 5-2, Handicapped Parking Spaces	
Number of Automobile Spaces Provided	Number of Handicapped Spaces Required
1-40	1
41-80	2
81-120	3
121-160	4
161-300	6
301-400	6
401-500	7
Over 500	7 + 1 for each 200 additional automobile spaces provided

- B. Handicapped parking for all other uses shall be provided at a rate of 1.0 space per 20 automobile spaces up to 200 automobile spaces; 1.0 spaces for each additional 100 automobile spaces thereafter.
- C. Handicapped parking spaces required by this section shall count towards fulfilling automobile parking requirements.
- D. Each handicapped space shall be 14 feet wide, lined to provide a 9-foot wide parking area and a 5-foot wide long area, and 20 feet in depth. If handicapped spaces are adjacent to each other. They may share the 5-foot wide loading area, resulting a 23-foot width for both spaces.

5.2.6 Bicycle Parking Requirements

Bicycle use is encouraged as an alternate means of transportation to the New Perris project. Parking for bicycles will be provided as indicated in **Table 5-3, Bicycle Parking Requirements**, below:

Table 5-3, Bicycle Parking Requirements	
Use	Bicycle Parking Required
Office (over 100,000 square feet)	5 spaces
Commercial Centers with more than 500,000 square feet of gross floor area	1 space/33 automobile parking spaces required
Freestanding Fast Food Restaurants	10 spaces
Banks	2 spaces
Recreational Uses	1 space/10 automobile parking spaces required

5.2.7 Uses Not Listed

The parking requirement for uses not specifically listed above shall be determined by the approval body for the proposed use on the basis of requirements for similar uses, and on any traffic engineering and planning data that is appropriate to the establishment of a minimum requirement.

5.2.8 Dimensions of Parking Spaces

A. Automobile, Residential

- 1) Covered Spaces: Each covered parking space shall be in a garage or carport. Such a space shall be a minimum of 9 feet in width and 20 feet in depth.
- 2) Uncovered Space: Each uncovered space shall be a minimum of 9 feet in width and 19 feet in depth.

B. Automobile, All Other Uses

- 1) Full-Sized Space; Each full-sized space shall be a minimum of 10 feet in width and 19 feet in depth.
- 2) Compact Space; Each compact space shall be a minimum of 9 feet in width and 16 feet in depth.
- 3) Each compact space shall be individually designed as a compact space.
- 4) All compact spaces shall be conveniently located and dispensed throughout the required parking areas.
- 5) Parallel Space: Each parallel parking space shall have a minimum dimension of 9 feet in width and 22 feet in length.

5.2.9 Design Parking Standards for Parking Areas

The standards presented are the minimum requirements for parking areas:

- A. Vehicles may not intrude into or onto a public sidewalk or public right-of-way.

- B. A vehicle may overhang a landscaped planter area a maximum of two feet provided the landscape planter has minimum width of five feet measured from the inside curb face.
 - 1) Curbs and “wheel stops” shall be located in such a position as to:
 - 2) Stop the parked vehicle from encroachment into landscaped or pedestrian areas.
 - 3) Protect the adjacent property from damage by vehicle.
 - 4) Must be at least 6’ in height.
- C. All properties shall provide attractive, direct and safe pedestrian access to parking. The layout of the parking areas shall relate to building entrances or important architectural elements.
- D. All parking lots shall be designed to provide for internal circulation so that each parking space is accessible to all other parking spaces without using a public street.
- E. Parking lot landscaping shall be located as to discourage pedestrians from having to cross any landscaped areas to reach building entrances from parked cars. This can be achieved through proper orientation of the landscaped fingers and islands.
- F. The minimum width of driveways shall be twenty-four (24) feet where ingress and egress are the same. If ingress and egress are separate drives, then the minimum width shall be fourteen (14) feet.
- G. Lights illuminating a parking space shall be arranged and screened to reflect light away from adjoining residences and streets.

5.2.10 Parking Area Landscaping

- A. There shall be a minimum of fifteen percent (15 percent) parking lot landscaping, with one tree planted for every seven (7) parking stalls. The 15 percent landscaping requirement shall be calculated based upon the parking area only. The parking area is defined as the 9-foot by 20-foot parking stall and the access to the parking stall. The landscaping shall include live plant material, including but not limited to, trees, shrubs and groundcover. All landscaped areas shall be provided with permanent, automatic irrigation facilities. All landscaping plans shall be prepared by a registered landscape architect.

5.2.11 Off-Street Loading and Unloading Areas

- A. **Loading Spaces Required**
 All commercial/industrial uses shall provide loading space in accordance with **Table 5-4, Loading Space Requirements**, below. This space shall be exclusive of the required parking, access or maneuvering areas.

Table 5-4, Loading Space Requirements	
Use (Gross Floor Area)	Loading Space Required
Commercial/Research and Development	
0-15,000	1
15,001-40,000	2
40,000-90,000	3
90,000-150,000	4
150,000 and over	5
Hotels, Motels, and Restaurants	1

B. General Provisions

- 1) The size of loading spaces with less than 15,000 square feet of gross floor area shall be 12 feet by 24 feet. Uses with over 15,000 square feet shall be at least 12 feet in width, 40 feet in length, and with 14 feet in vertical clearance.
- 2) When the lot upon which loading spaces are located abuts an alley such loading spaces shall have access from the alley. When such loading area is parallel with the alley and the lot is 50 feet or less on width, the loading area shall extend across the full width of the lot.
- 3) Loading spaces shall be located so trucks shall not back into a public street or alley.
- 4) Research and development sites shall be designed with the following;
 - a. At least one driveway approach capable of accommodating a 48-foot wheel tract turning radius with at least one on-site maneuvering area which provides a 48-foot wheel tract turning radius through the parking area.
 - b. Loading doors shall not be visible from a public street.

C. Research and Development Service

- 1) Loading facilities shall be located either on the rear or side of research and development structures to alleviate the unsightly appearances often created by loading facilities. Where this is not feasible, loading docks and doors shall be screened from the street by landscaping or walls.
- 2) Each industrial site shall be self-contained and capable of handling its own truck maneuvering and docking requirements. The use of public streets for parking, truck staging and/or maneuvering is prohibited.

5.2.12 Drive-Thru Facilities

Drive-thru facilities require special consideration as their design can significantly impact the vehicular circulation on a site. Pedestrian access shall not cross the drive-thru land, whenever feasible. The following requirements apply to any use with drive-thru facilities:

- A. Each drive-thru lane shall be separated from the circulation routes necessary for ingress or egress from the property, or access to any parking space.
- B. Each drive-thru lane shall be striped, marked, or otherwise delineated.
- C. The vehicle stacking capacity for uses containing drive-thru facilities shall be as follows in **Table 5-5, Stacking Requirements**, below:

Table 5-5, Stacking Requirements	
Use	Stacking Requirements
Fast Food Restaurants	Stacking for three cars between the order board and the pick-up window and stacking for five cars behind the order board
Bank Drive-Thru Windows	Stacking for five cars for each window

5.2.13 Off-Site Parking Facilities

Required parking for a development may be located off the site under certain circumstances. Off-site parking must meet the following requirements:

- A. Off-site parking shall be located so that it will adequately serve the use for which it is intended.
- B. There shall be no hazardous traffic safety conditions for pedestrians utilizing the off-site parking facilities.
- C. A written agreement shall be drawn and executed to the satisfaction of all concerned parties, assuring the continued availability of the off-site parking facilities for the use they are intended to serve. The agreement between the affected property owners, providing for maintenance and use shall be entered into and recorded prior to approval of occupancy.

5.2.14 Shared Parking

Parking facilities may be used jointly for uses with significantly different peak hours of operation. An agreement between affected property owners shall be entered into and recorded prior to approval of occupancy.

5.2.15 Parking Structures

Parking structures require unique design considerations to reduce the mass scale and building bulk that they often create on a site. The following requirements apply only to parking located within above-grade parking structures:

A. Landscaping

The exterior elevation of parking structures shall be designed to minimize the use of blank concrete facades. This can be accomplished through the use of textured concrete, planters, terraces, or trellises or other architectural treatments.

- 1) The perimeter of the parking structure shall be landscaped at ground level.
- 2) No parking lot landscaping shall be required for parking spaces located in parking structures.
- 3) Setbacks shall be provided for each level of the parking structure to add landscaping at each level.
- 4) The parking structure shall be landscaped at the top level.

5.3 Transportation Management Standards

5.3.1 Intent and Purpose

The transportation management standards for New Perris are intended to promote a balanced transportation system which will support growth and be consistent with the overall development goals of the City of Perris. These standards also intended to reduce the impacts of traffic within the project and region by reducing the number of vehicle trips that might otherwise be generated by the community. It is intended that the transportation management standards thereby contribute to the cumulative reduction of vehicular emissions, energy usage and ambient noise levels.

5.3.2 Objectives

- Develop an efficient parking supply which supports citywide, regional and county development goals.
- Recognize that access needs differ in congested and uncongested areas.
- Reduce the need for parking, especially long term parking serving low occupancy commuter trips.
- Provide transportation incentives structured to alleviate congestion.
- Encourage the improvement of public transit service and the increase in transit ridership.
- Encourage trips in shared vehicles, on bicycles, and by walking.
- Reduce through traffic, as well as circling traffic, on congested streets, and minimize traffic congestion.
- Reduce reliance on automobile travel, especially during peak hours.
- Reduce conflicts between transit, pedestrians, bicycles, and automobile traffic.
- Meet Federal and State air quality standards.

5.3.3 Parking

- A. In commercial areas, now parking for non-residential use should include sufficient short-term spaces and at a minimum, meet the project's needs. As a goal, at least 20 percent of the parking spaces should be designed for short-term parking.
- B. Parking allocated for tenant use (in non-residential developments) should be allocated to carpools at a first priority. Property owners, or tenants designated as property managers, should maintain access and inspection programs to ensure appropriate allocation and maintain separate lists for carpools and non-carpools, with spaces given to carpool groups before any are given to non-carpooling drivers.
- C. Compact car parking spaces are to be places at entrances or other prime locations in order to avoid unnecessary use of large spaces by compact cars.
- D. Parking should be marked, priced and operated in a manner which encourages its use for short term parking and discourages its use for long term parking.

- E. Garages classified as accessory parking facilities should be oriented to minimize conflict with pedestrian movements, and amenities, resulting from the placement of driveways and ramps, the breaking of continuity of shopping facilities along sidewalks, and the drawing of traffic through areas of heavy pedestrian concentration.

These garages should also minimize conflict with transit operations, and pedestrian loading areas, resulting from the location of driveways, ramps and vehicle queuing areas.

5.3.4 **Transportation Management Programs**

A project wide Transportation Management Program is strongly encouraged, whereby a coordination group is formed to manage and organize such programs as listed below.

Transportation Management Programs are intended to reduce the overall rate of travel by single occupancy automobiles to worksites within the New Perris development. At employers regularly employing 200 or more employees at one worksite, and all operators of worksites with 200 or more employees should have an average employee to vehicle ratio of 1.25 to 1.50.

- A. (These goals are subject to change as the average employee to vehicle ratio increases for the City of Perris overall.)
- B. The average employee to vehicle ratio shall be calculated by dividing a worksite's total employee population scheduled to start work between 7:00 a.m. and 9:00 a.m. by the total number of vehicles which are driven by these employees from home to the parking facilities serving the worksite, including off-site facilities. Transit vehicles serving multiple worksites and bicycles are excluded from the vehicle count.
- C. Where feasible, the landowner/developer should coordinate an annual plan, or have one coordinated by a certified transportation brokerage service that:
 - o designates a transportation coordinator responsible for development, implementation and administration of the plan;
 - o provides an inventory of current strategies used to increase the average employee to vehicle ratio;
 - o provides a verifiable estimate of the existing average employee to vehicle ratio; and
 - o lists specific incentives the employer or operator commits to undertake which can reasonably be expected to lead to the achievement of the target ratio.

The list of incentives may include:

- o establishment of carpool, buspool, taxipool, or vanpool programs;
- o preferential parking access or egress for vehicles used for ridesharing;
- o financial incentives to employees and employers encouraging the use of ridesharing or public transit facilities;
- o provision of transit information and on-site pass sales;

- flexible or modified work hours for employees;
- use of fleet vehicles for ridesharing; or
- additional bike facilities, such as bike racks and bathroom showers.

5.3.5 Pedestrian Circulation

New developments or redevelopments shall provide adequate pedestrian access to, within, and across the site. Sidewalks shall be constructed around the street perimeter of the site. They shall also be constructed from the perimeter walkway to the main entrances of the principle buildings within the project site. They shall also be constructed between buildings.

Where possible, the shortest route shall be used to connect the main building entrance with adjacent transit stops.

Sufficient lighting shall be provided for sidewalks, and for bus stops used during non-daylight hours. The lighting system shall be designed to eliminate light spill and unnecessary glare on adjacent residential properties. The system design shall insure turn-off times by the site operator when lights are not needed.

Land should be dedicated to allow for the installation of bus shelters. Any developer may submit for approval a design other than that provided by the City of Perris in order to accelerate installation. The City of Perris or other appropriate District or Agency will approve and maintain any shelter which conforms to its minimum design standards. A developer may also provide and maintain shelter amenities beyond these standards.

5.4 Signage

5.4.1 Intent

Signs are to provide drivers and pedestrians with sufficient information to circulate safely throughout the New Perris Project. They are also required to have a high level of design which is consistent and complements the major design themes of the project (such as architecture and landscaping).

The sign standards established in this Specific Plan are intended to set guidelines for public signage in the New Perris development. The sign standards below establish guidelines for the New Perris identity signs and project specific identity signs only. All other signage shall conform to the City of Perris sign ordinance, and all signage will require City of Perris approval. Signs will be approved per the current City ordinance or as part of a detailed sign program.

5.4.2 Sign Standards

The location and ape of sign shall be determined by the nature of information being displayed. The following standards shall apply.

- A. New Perris Identity Signs
 - 1) Ground signs
 - Limited to the display of the “New Perris” logo
 - Ground signs will be located at major project entries
 - Ground sign shall not exceed 300 square feet in sign area
 - 2) Wall signs
 - Limited to the display of the “New Perris” logo
 - Wall signs will be incorporated into the landscape walls (or planned project walls)

- B. Project Specific Identity Signs
 - 1) Ground Signs
 - Industrial building or tenant name on a monument sign
 - One project identity sign will be located at the entry from a street to a building group
 - Ground signs shall be not exceed 250 square feet in sign area. Letter height shall be suitable to the display of information and shall not exceed 60 percent of the total sign area unless otherwise approved through the design review process
 - 2) On-Building Signs
 - Individual building or tenant name attached to a building wall surface
 - On-building signs will have either internally illuminated or non-illuminated lettering and graphics. On-building signs will have individual lettering mantled directly onto the building facade. Colors and letters of graphics shall be considered with the overall New Perris theme
 - The size and location of each sign shall be designed to be complimentary and proportional to each building

5.4.3

Sign Materials

A. Permitted sign materials (chosen for quality, durability, costs and minimum maintenance factors):

- 1) Thick gauge aluminum monument sign cabinets with attached or routed through graphics.
- 2) Natural or sandblasted concrete monument sign bases.
- 3) Cast or fabricated aluminum, individual letters on low wall mounted signs and on non-illuminated on-building signs located twelve feet or less above grade level.
- 4) High-density polystyrene or aluminum for use on non-illuminated on-building signs located in excess of 12 feet above grade level.
- 5) Laser or die-cut adhesive vinyl graphics on smaller monument signs or on glass facades.
- 6) White Plexiglas may be used in internally illuminated aluminum monument signs providing it is used solely to back up the routed-through graphic message and may not be used as the entire sign face.
- 7) White or bronze Plexiglas may be used only on the faces of internally illuminated on-building signs. The sides (returns) of the letters are to be fabricated from aluminum and are to be finished to match the predominant building trim color.
- 8) Illumination: White fluorescent tubes in routed-through aluminum sign cabinets; white fluorescent fixtures, shielded from view, to flood light monument or low wall signs; white neon tube within to illuminate individual on-building letters.
- 9) All attachment and fastening hardware to be stainless steel to enable disassembly and to prevent rust staining of architectural surfaces.
- 10) Sign finish materials: Kynar 500 or Linear Polyurethane will be used, because of their durability, particularly on ground mounted signs.

B. Prohibited Sign Materials: These materials will not be allowed because they are not consistent with the quality objectives of New Perris, are improper for the described uses, or have poor longevity.

- 1) Plastic faced monument signs
- 2) Colored lights
- 3) Sheet or other ferrous metals
- 4) Automotive, acrylic, or similar finishes
- 5) Colored plastic or Plexiglas other than white or bronze as described
- 6) above in General Sign Requirements (Color and Contrast)
- 7) Bold or inharmonious colors or combinations
- 8) "Can" or "box" type signs or hand-painted graphics

5.4.4

Prohibited Signs

These signs are specifically prohibited for use in the New Perris project:

A. Blinking, flashing, or animated signs

- B. Advertising displayed on vehicles to draw attention to a specific business or “sale”
- C. Signs located or sized in a manner as to impede views of neighboring signs, enterprises, traffic, or pedestrians

5.4.5 Temporary Signage

- A. Temporary signs are intended to display information related to property for sale or lease and to identify future facilities.
- B. Temporary signs shall not exceed 6-feet in width, 6-inches in depth, or 10 feet in height.

5.5

Outdoor Storage Areas and Reuse Collection Areas

Outdoor storage areas, trash enclosures, and refuse collection areas shall be buffered from primary visual impact areas (e.g. streets, vehicular and pedestrian entry areas and drives, recreational amenities). The following guidelines and standards have been developed for compliance:

- A. No materials, supplies, or equipment, including trucks or motor vehicles, shall be stored upon a site except inside a closed building or behind a visually aesthetic barrier that will screen materials, supplies or vehicles from neighboring properties and streets.
- B. No outdoor storage areas may be located within any required front yard of any residential areas.
- C. In all portions of the project area, storage areas should be located in inconspicuous areas, such as the side or rear portion of a site.
- D. Outdoor storage areas shall be visually screened by using landscape plantings, walls or fences, earth berms, or any combination thereof.
- E. Refuse collection areas or trash enclosures shall be screened from off-site views by structures, solid walls or fences. Refuse collection areas shall be screened from views from any public street or adjacent residential area.
- F. Refuse collection areas or trash enclosures shall be conveniently accessible to the units/buildings they are designed to serve.
- G. Refuse collection areas and trash enclosures shall be located in such a manner as to minimize noise and odor nuisance.
- H. Refuse collection areas in industrial, commercial, and open space areas shall be screened by walls in such a manner as not to be visible thorn any public street or adjacent residential dwelling.
- I. Refuse collection areas in all areas shall be screened with a 6-foot high solid fence or wall and have an opaque gate.
- J. Refuse collection areas shall be screened by landscaping or architectural features in such a manner as not to be visible from a public street.
- K. Refuse collection bays (enclosures) within view of second story or multi-story buildings shall, additionally, be covered with a fire proof and open trellis.
- L. Provisions for refuse collection locations and angle of placement shall be determined such that service vehicles shall not have to perform a backing maneuver to approach the collection bins. Refuse collection areas shall not be located within the setback areas.

- M. All uses shall be conducted within a building except that outdoor storage and manufacturing is permitted subject to the following:
- 1) Outside storage or incidental service areas shall be obscured from view by structures which are architecturally blended with the main building, or by an equivalent screening density of live landscaping.
 - 2) Outside storage, of materials or equipment, service areas or manufacturing incidental to a permitted use shall be allowed, provided that not more than ten (10) percent of the site is used for that purpose.
 - 3) Outside storage shall not be permitted to extend above the perimeter walls.
 - 4) Outdoor storage or incidental manufacturing, performed outside, shall be conducted in such a location so as not to be visible from adjacent streets.
- N. Exceptions to the above conditions may be permitted upon approval of a Conditional Use Permit.

5.6

Mechanical Equipment Screening

The following provisions and restrictions shall apply to all exterior mechanical equipment in the New Perris Specific Plan area:

- A. No exterior mechanical equipment or devices shall be permitted within any required front yard or setback area.
- B. Exterior roof-mounted components of plumbing, processing, heating, cooling and ventilation systems (including but not limited to piping, tanks, stacks, collectors, heating, cooling and ventilating equipment fans, blowers, ductwork, vents louvers, meters, compressors, motors, incinerators, ovens, satellite dish antennas, and solar collectors, etc.) shall not extend above the top of the parapet or fencing, unless otherwise approved by the City of Perris.
- C. All roof-mounted equipment shall be screened from view at ground level by parapets or other building forms. If building parapets do not provide the required screening, mechanical equipment shall be screened by fencing or other architectural elements. Screening and shielding elements should be integrated with building design - including form and materials.
- D. Any exterior components of plumbing, processing, heating, and cooling and ventilating systems and their screening devices, that will be visible from upper floors of adjacent buildings shall be kept to a visible minimum, shall be installed in a neat and compact fashion, and shall be painted a color as to allow blending with the surrounding area.
- E. Exterior components of plumbing, processing, heating and cooling systems, and ventilating systems located near or at ground level shall be screened from views from adjacent or adjoining lots, buildings, or streets whenever possible, by heavy landscape plantings, walls or fences, earth berms, or any combination thereof.

5.7 Energy Conservation Guidelines

5.7.1 Water

- A. The use of a xeriscape or drought tolerant landscape system is strongly encouraged. This includes, but is not limited to, the use of drought resistant plants, limiting turf areas, efficient irrigation, soil improvements, mulch planting beds, proper maintenance and a well-planned design by a certified landscape architect.
- B. A Water Management Program should be implemented to ensure water conservation. This includes, but is not limited to, the proper irrigation of vegetation during the different seasons, planting vegetation during the appropriate time of year and an automatic irrigation system.
- C. Residential units should be provided with water saving washing machines and dishwashers, small reserve tanks in toilets and flow restrictions in bathrooms and kitchens.
- D. The irrigation of landscape shall use reclaimed water where available. An irrigation system using reclaimed water will be required for parkways, parking lots, public parks, etc.

5.7.2 Energy

- A. Architecture and landscape features should be used to reduce summer heat and increase winter heat where possible.
- B. Energy efficient appliances should be installed in all residential units.
- C. Florescent lighting should be used where desirable.
- D. Programmable thermostats should be installed in all residential and commercial units.
- E. The use of skylights, light wells, atriums, interior courts or similar architectural forms is strongly recommended.
- F. Insulation and/or extra insulation shall be used where possible.
- G. Water pipes shall be insulated.
- H. Solar energy systems should be installed where feasible.
- I. setting zones should be required for offices and suggested for residential units.
- J. Gas appliances shall not have continuous burning pilot lights.

5.8 Public Arts Program

5.8.1 Intent

The New Perris project intends to establish the highest standards of aesthetic quality and economic vitality. The Public Arts Program is an integral component in attaining goals. In support of a commitment to the arts, and the concern for the quality of life and environment of the New Perris project, strongly encouraged to participate in the Arts in Public Places Program. The New Perris project will create a visual image and identity which will heighten civic pride with the aid of public artworks. Each work of art has the potential to create physical and intellectual energy, which establishes an atmosphere that draws residents, business and tourists to enjoy the development as a whole. A major function of such a program is to manifest, uplift and nurture the civic spirit.

5.8.2 The Program

Selected parcels have been solicited for participation in the Art in Public Places Program (see Figure 67). The developer/user will be encouraged to participate in the program. The selected locations are based on visual prominence from a view access area or major public street end-point.

A guideline of one percent of project hard development costs (i.e., not including land) for each designated parcel is suggested to direct funds towards the New Perris Arts Council for the installation of art works within the New Perris project. This guideline is in accordance with the National Endowment for the Arts and standards set forth by such progressive cities as Seattle, Philadelphia and San Francisco. For those parcels not designated under the Art in Public Places Program, the developer/user is, nonetheless, encouraged to participate at the same one percent rate

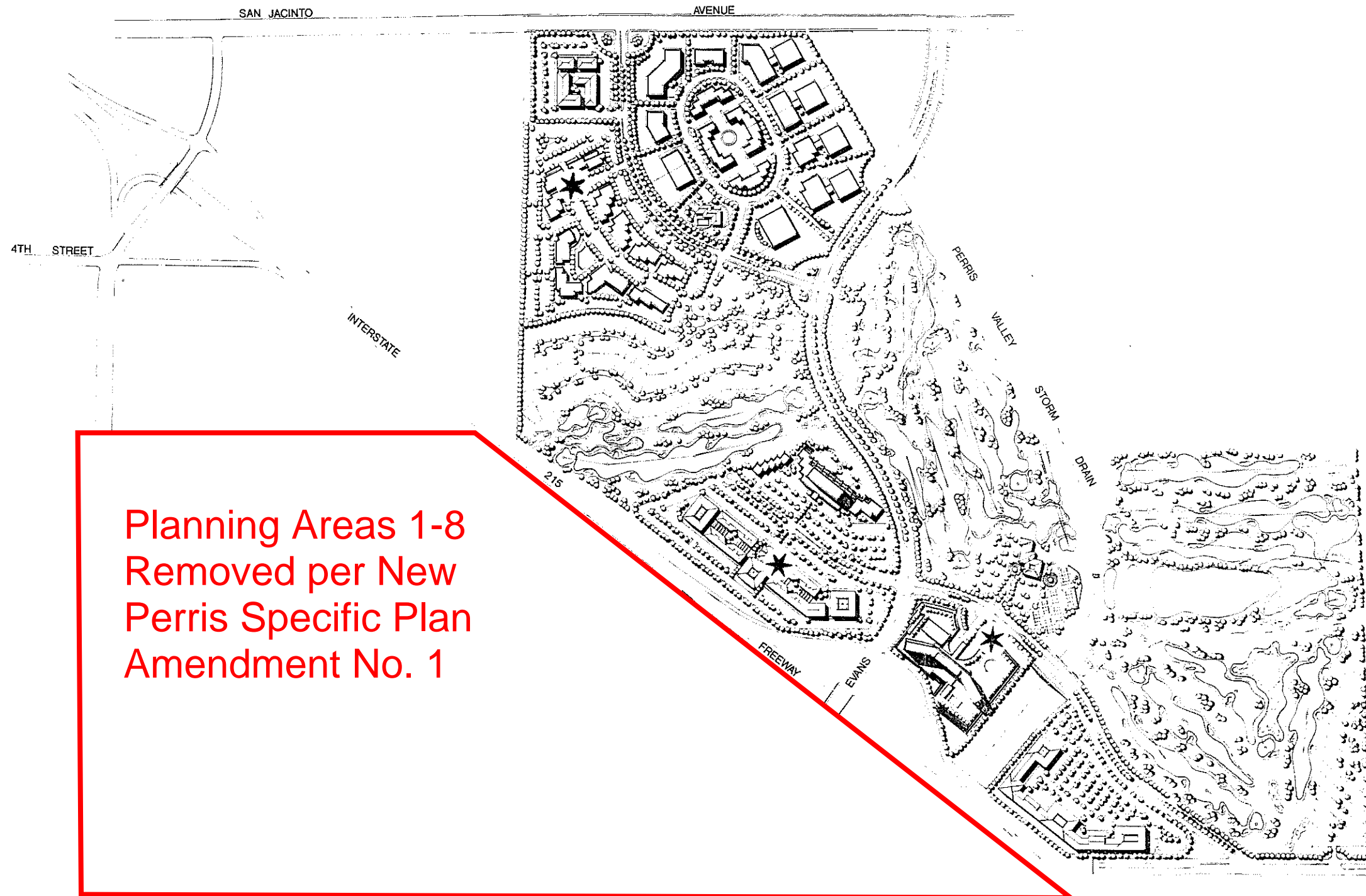
5.8.3 Guidelines

- A. **Location:** The location of each piece of art identified in **Figure 47, Art in Public Places**, is conceptual. Public artworks are to be at either the terminus of main public streets, or in visually prominent locations such as entry courtyards. Developer/user is encouraged to respect these locations during the design development of each parcel. The work of art shall be in a specific location designated for such purpose and approved by New Perris Art Council and the City of Perris Art in Public Places Administration.
- B. **Theme and Concept:** The selection of each work of art will be site specific and be selected based on the character and aesthetics of the landscape and architecture of that site. While the thematic intent is left to the owner/user/artist, artwork shall not display degenerative gestures or statements which would be offensive to the general public.

ART IN PUBLIC PLACES

LEGEND

 POTENTIAL PUBLIC ART LOCATIONS



Planning Areas 1-8
Removed per New
Perris Specific Plan
Amendment No. 1

FIGURE 47

C. Artistic Requirements: (1) The commission for all works of art budgeted at and under shall be awarded to a resident of Riverside County or the State of California; (2) Each artist shall submit documentation illustrating his/her experience and knowledge before being contracted to construct and to complete the proposed work of art; and (3) Developer/user of each parcel will encourage the creation of art that displays a wide range in theme or concept when related to other artworks within the project. In order to encourage this philosophy, no more than two pieces of work at the center shall be completed by the same artist.

D. Material: (1) Material shall be of the type and scale which is appropriate to the theme or concept of the art piece and should respect the surrounding natural and built environment; (2) Material shall be permanent in nature and require low maintenance; and (3) Accessories accompanying the artwork, unless the elements are integrally designed components. All accessories shall be durable, require low maintenance, and shall not create glare or passing drivers or pedestrians.

E. Maintenance: Each developer/user is responsible for maintaining the artwork in a manner which is acceptable to the New Perris Arts Council. Each artist shall provide guidelines that establish procedures for the ongoing maintenance after installation. These guidelines shall cover the following areas:

- 1) Identification of material in the work;
- 2) Methods of maintaining work;
- 3) Frequency of maintenance.

A review between the Artist and the Art Council shall take place after installation of the art piece to go over maintenance procedures.

F. Processing: Prior to development, the developer/user of any planning area should contact the New Perris Arts Council to provide further direction and answer any questions which may arise pertaining to the program.

In compliance with the program, each developer/user will submit to the New Perris Arts Council a list providing the following information:

- 1) Lot or parcel number and address
- 2) Project Name
- 3) Owner/Developer/User
- 4) Artist and Resume
- 5) Photographs and or drawing of the proposed work which illustrates and documents dimensions and materials
- 6) Site plan of appropriate scale to show location and intent
- 7) Owner-User-Artist statement of philosophy

The New Perris Art Council is a subcommittee of the New Perris development established by the Covenants, Conditions, and Restrictions for New Perris.

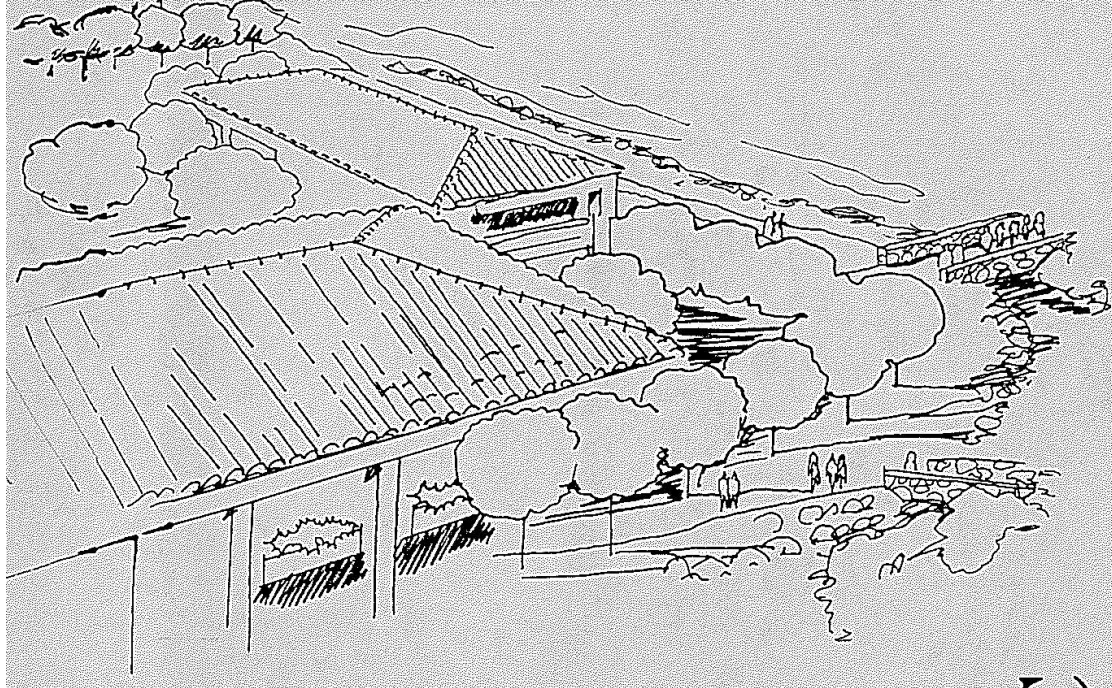
The Art Council shall review all art proposals. The council is responsible for maintaining the quality and diversity of artwork that is constructed within the New Perris project.

All council members should have an art orientation and be qualified either through education or experience. The council will be comprised of the following members:

- Two Representatives of the New Perris Developers.
- One Representative of the Riverside Arts Foundation.
- Two Representatives who are artists or art educators experience in Environmental Sculpture as approved by a majority of the other members of the council.

Upon submission of an art proposal, the Art Council will respond in writing to the developer/user within thirty (30) days. If the Art Council finds the proposal to be in conflict with the criteria of the Art in Public Places guidelines a written notification shall be forwarded to the parties submitting the proposal, identifying the areas and issues of objections or non-compliance.

At the time that objections or issues of non-compliance arise, a meeting shall be arranged with the developer/user to discuss the items of contention and to determine what modifications are necessary to bring the work into compliance with the guidelines.



6.0 SPECIFIC PLAN IMPLEMENTATION

6.0 SPECIFIC PLAN IMPLEMENTATION

6.1 Intent

The intent of implementation procedures set forth in this section is to assure the development of the New Perris project is in accordance with the design and development standards contained within this Specific Plan and other applicable City of Perris development and performance standards. Conformance of development on the project site can be assured through a two-step review process involving (a) the New Perris landowner; and (b) the City of Perris Planning and Community Development Department.

6.2 Site Plan and Building Design Review Procedures

Prior to submittal, all site and building plans may be submitted to the City of Perris for approval by the New Perris landowner in accordance with the submittal requirements and procedures contained in the declaration of covenants, conditions and restrictions for the property. This process is outlined briefly below. It is the landowner's intent to ensure that the architectural integrity and design standards are preserved throughout the entire build out process. In following this process through, the landowner may form a team of design experts to meet this objective. The team may be comprised of land planners, design professionals and architects. Therefore, prior to beginning any Schematic Design Studies, there may be a Pre-Design Meeting with the New Perris landowner, any other developer/user, and any appropriate design or planning professionals. This meeting will focus on understanding the intent and use of the Development Standards and on resolving the goals and objectives for the project. Following this meeting, the architect can then proceed to the Schematic Design Phase with a more positive understanding of the direction that will be dealt with in subsequent reviews.

At the time the landowner, any other developer/user and the design and planning professionals have developed the proposed design to a schematic design state, there may be a required schematic design review. The schematic design review will be an evaluation of the proposed development based upon the design guidelines contained within this Specific Plan. At this time, the development design or site plan may be initially submitted to the City of Perris.

All proposed structures within the New Perris project shall be subject to development plan review by the City of Perris. The purpose of this review is to assure that all individual building designs and site plans are in conformance with the design and development standards contained in the New Perris Specific Plan and other applicable City of Perris development and performance standards. This review process shall be in accordance with the following procedures:

Prior to the issuance of any building permits, an application for Development Plan Review shall be approved per standard City review and approval processes. An application for Development Plan Review shall be filed along with the following types of materials:

- Application Form
- Application Fees
- Site Plan

- Floor Plans
- Architectural Elevations
- Landscaping Plans
- Environmental Information Form (where applicable)

Issuance of building permits will also require improvement plan, record map, etc.

6.3 Subdivision Procedures

The City of Perris Subdivision Code shall regulate and control all divisions of land within the New Perris project.

6.4 Specific Plan Amendment Procedures

Amendments to the New Perris project shall be approved in accordance with California Planning, Zoning, and Development Laws 1988 (Government Code Section 65453) which states, “Specific plans shall be prepared, adopted, and amended in the same manner as general plans, except that a specific plan may be adopted by resolution or by ordinance and may be amended as often as deemed necessary by the legislative body.”